

## **Appendix B – Consultation**

ORGANIZATION	FIRST NAME	LAST NAME	POSITION	ADDRESS	CITY/PROVINCE	POSTAL CODE
<b>FEDERAL AGENCIES</b>						
Aboriginal Affairs and Northern Development Canada			Environmental Assessment Coordination Unit	25 St. Clair Avenue East, 8th Floor	Toronto, ON	M4T 1M2
Aboriginal Affairs and Northern Development Canada			Consultation and Accommodation Unit	300 Sparks Street	Ottawa, ON	K1A 0H4
Aboriginal Affairs and Northern Development Canada			EA Coordinator	25 St. Clair Avenue East, 8th Floor	Toronto, ON	M4T 1M2
Canadian Environmental Assessment Agency			Regional Director, Ontario Region	55 St. Clair Avenue East, 9th Floor	Toronto, ON	M4T 1M2
Canadian National Railway	Raymond	Beshro	Senior Officer, Community Planning and Development	935 de La Gauchetière Street West	Montreal, QC	H3B 2M9
Canadian National Railway	Stefan	Linder	Manager, Public Works, Design and Construction	1 Administration Road, Station 515	Concord, ON	L4K 1B9
Canadian National Railway	Dave A.	Reynolds	Manager, Engineering and Environmental Services	1 Administration Road, P.O. Box 1000	Concord, ON	L4K 1B9
Canadian Transportation Agency					Ottawa, ON	K1A 0N9
COSEWIC Secretariat	Carolina	Caceres	Manager, Canadian Wildlife Services, Environment Canada	351 St. Joseph Boulevard, 16th Floor	Gatineau, QC	K1A 0N9
Environment Canada	Sheila	Allen	Senior EA Officer	Box 5050: 867 Lakeshore Road	Burlington, ON	L7R 4A6
Environment Canada	Rob	Dobos	Manager, Environmental Assessment Section	Box 5050: 867 Lakeshore Road	Burlington, ON	L7R 4A6
Environment Canada - Ontario Region	Robert	Gilmore		4905 Dufferin Street	Downsview, ON	M3H 5T4
Fisheries and Oceans Canada			EA Coordinator, Habitat Management and Enhancement Division	200 Kent Street, 13th Floor, Station 13E228	Ottawa, ON	K1A 0E6
Fisheries and Oceans Canada			Habitat Management Leader	867 Lakeshore Road	Burlington ON	L7S 1A1
Fisheries and Oceans Canada / Coast Guard	Colleen	O'Meara	Communications Manager, Central and Arctic Region	Box 5050: 867 Lakeshore Road	Burlington, ON	L7R 4A6
Fisheries and Oceans Canada OGLA	Brent	Valere	Senior Habitat Biologist, Southern Ontario District	3027 Harvester Road, Suite 304	Burlington, ON	L7N 3G7
<b>PROVINCIAL MINISTRIES</b>						
Infrastructure Ontario	Neil	D'Souza	Manager, Asset Management	900 Bay Street, M1-34i	Toronto, ON	M7A 1N3
Ministry of Aboriginal Affairs	Wendy	Cornet	Manager, Consultation Unit, Aboriginal Relations and Ministry Partnership Division	160 Bloor Street East, 9th Floor	Toronto, ON	M7A 2E6
Ministry of Aboriginal Affairs	David	Pickles	Team Lead for EAs	9 <sup>th</sup> Floor, 160 Bloor Street East	Toronto ON	M7A 2E6
Ministry of Aboriginal Affairs			EA Coordinator	4 <sup>th</sup> Floor, 160 Bloor Street East	Toronto ON	M7A 2E6
Ministry of Agriculture, Food and Rural Affairs	David	Cooper	Manager, Environmental and Land Use Policy	Ontario Government Building 3rd Floor, 1 Stone Road West	Guelph, ON	N1G 4Y2
Ministry of Community Safety & Correctional Services	Ali	Veshkini	Director	George Drew Building, 25 Grosvenor Street, 17th Floor	Toronto, ON	M7A 2G8
Ministry of Infrastructure	Andrew	Theoharis	Manager, Ontario Growth Secretariat	777 Bay Street, 4th Floor, Suite 425	Toronto, ON	M5G 2E5
Ministry of Municipal Affairs and Housing	Barbara	Konyi	Manager, Greater Horseshoe Greenbelt Section	College Park, 14th Floor, 777 Bay Street	Toronto, ON	M5G 2E5
Ministry of Municipal Affairs and Housing	Mark	Christie	Manager, Community Planning and Development, Central Municipal Services Office	777 Bay Street, 2nd Floor	Toronto, ON	M5G 2E5
Ministry of Municipal Affairs and Housing			EA Coordinator	College Park, 17th Floor, 777 Bay Street	Toronto, ON	M5G 2E5
Ministry of Natural Resources and Forestry	Jason	Borwick	Area Biologist	50 Bloomington Road, West	Aurora ON	L4G 3G8
Ministry of Natural Resources and Forestry	Steven	Strong	District Planner, Aurora District	50 Bloomington Road West, R.R.#2	Aurora, ON	L4G 3G8
Ministry of the Environment			Environmental Assessment and Approvals Branch	2 St. Clair Avenue West, Flood 12A	Toronto ON	M4V 1L5
Ministry of the Environment and Climate Change	Dan	Delaquis	Supervisor, Air, Pesticides and Environmental Planning	5775 Yonge Street, 9th Floor	Toronto, ON	M2M 4J1
Ministry of the Environment and Climate Change	Emilee	O'Leary	Environmental Resource Planner & EA Coordinator	5775 Yonge Street	Toronto, ON	M2M4J1
Ministry of the Environment and Climate Change	Paul	Martin				
Ministry of Tourism, Culture and Sport	Karla	Barboza	Heritage Advisor, Cultural Services Unit, Programs and Service Branch	401 Bay Street, Suite 1700	Toronto, ON	M7A 0A8
Ministry of Tourism, Culture and Sport	Paula	Kulpa	Team Lead-Heritage Land Use Planning, Cultural Services Unit, Programs and Services Branch	401 Bay Street, Suite 1700	Toronto, ON	M7A 0A8
Ministry of Tourism, Culture and Sport	Dan	Minkin	Heritage Planner, Cultural Services Unit	401 Bay Street, Suite 1700	Toronto, ON	M7A 0A7
Ministry of Tourism, Culture and Sport			Heritage Planner, Culture Services Unit	400 University Avenue	Toronto, ON	M7A2R9
Ministry of Tourism, Culture and Sport	Rozi	Zinger	Heritage Planner, Cultural Service Unit	401 Bay Street, Suite 1700	Toronto, ON	M7A 0A7
Ministry of Tourism, Culture and Sport			EA Coordinator	9th Floor, 900 Bay Street	Toronto, ON	M7A 2E1
Ministry of Tourism, Culture and Sport	Laura	Hatcher	Team Lead – Heritage Land Use Planning	401 Bay Street, 17th Floor	Toronto, ON	M7A 0A8
Ministry of Transportation Ontario	Tija	Dirks	Director, Transportation Planning Branch	777 Bay Street, 16th Floor	Toronto, Ontario	M5G 2E5
Ministry of Transportation Ontario	Margaret	Mikolajczak	Project Manager, Corridor Management Section, Engineering Office, Central Region	1201 Wilson Avenue, 7th Floor Building 'D'	Downsview, ON	M3M 1J8
Ministry of Transportation Ontario	Gary	Todd	Manager, Design and Contract Standards Office	301 St. Paul Street, 2nd Floor, North	St. Catharines, ON	L2R 7R4
Ministry of Transportation Ontario	Peter	Veroke	Manager, Engineering Office	1201 Wilson Avenue, 5th Floor, Building D	Downsview ON	M3M 1J8
Ministry of Transportation Ontario			EA Coordinator	Ferguson Block, 77 Wellesley Street West, 3rd Floor	Toronto ON	M7A 1Z8
Ontario Provincial Police	Gary	Elucher	Sergeant	100 Bloomington Road	Aurora, ON	L4G 7N5
Ontario Provincial Police	Joy	Fish	Manager, OPP Facilities Section	777 Memorial Avenue, 2nd Floor	Orillia, ON	L3V 6H3
Infrastructure Ontario	Lisa	Myslicki	Environmental Advisor, Environmental Management	1 Dundas Street West, Suite 2000	Toronto, ON	M5G 2L5
<b>SCHOOL BOARDS</b>						
Conseil scolaire de district catholique Centre-Sud				110 Avenue Drewry	Toronto, ON	M2M 1C8
Conseil scolaire Viamonde			L' élève, le reflect de l'avenir	116 Cornelius Parkway	Toronto, ON	M6L 2K5
York Catholic District School Board	Patricia	Preston	Director of Education	320 Bloomington Road West	Aurora, ON	L4G 0M1
York Catholic District School Board	Christine	Hyde	Planner	320 Bloomington Road West	Aurora, ON	L4G 0M1
York Catholic District School Board, Catholic Education Centre	Giovanna	DiGirolamo	Planner	320 Bloomington Road West	Aurora, ON	L4G 3G8
York Region District School Board			Director of Education, The Education Centre - Aurora	60 Wellington Street West, Box 40	Aurora, ON	L4G 3H2
<b>MUNICIPAL-EMERGENCY SERVICES</b>						
City of Vaughan Fire Rescue and Service Department	Larry	Bentley	Chief	City Hall 2141 Major Mackenzie Drive	Vaughan, ON	L6A 1T1
St. John Ambulance - York Region Branch				13025 Yonge Street, Suite 202	Richmond Hill ON	L4E 1A4
York Regional Police	Eric	Joliffe	Chief	17250 Yonge Street	Newmarket, ON	L3Y 4W5
<b>FIRST NATIONS</b>						
Chippewas of Georgina Island	Donna	Big Canoe	Chief	R.R.#2, P.O Box N-13	Sutton West, ON	L0E 1R0
Mississaugas of Scugog Island	Kelly	Larocca		22521 Island Road	Port Perry, ON	L9L 1B6
Mississaugas of Scugog Island	Dave	Mowat	Consultation Specialist	22522 Island Road	Port Perry, ON	L9L 1B7
The Chiefs of Ontario	Sue	Chiblow	Environmental Coordinator	111 Peter Street, Suite 804	Toronto, ON	M5V 2H1

ORGANIZATION	FIRST NAME	LAST NAME	POSITION	ADDRESS	CITY/PROVINCE	POSTAL CODE
Miississaugas of the New Credit First Nations	Bryan	LaForme	Chief	2789 Mississauga Rd., R.R. #6	Hagersville, ON	N0A 1H0
Chippewas of Rama	Rodney	Noganosh	Chief	5884 Rama Road, suite 200	Rama, ON	L0K 1T0
Beausoleil First Nation (Christian Island)	Roland	Monague	Chief	11 O-Gema Miikaan	Christian Island, ON	L9M 0A9
The Métis Nation of Ontario	Melanie	Paradis	Director	75 Sherbourne Street, Suite 222	Toronto, ON	M5A 2P9
<b>MUNICIPALITIES AND REGIONS</b>						
Heritage Vaughan Committee			City of Vaughan Recreation and Culture Department, Cultural Services Division	2141 Major Mackenzie Drive, City Hall Level 300	Vaughan, ON	L6A 1T1
Smart Commute - North Toronto, Vaughan	Brian	Shefman	Executive Director	36C York Lanes 4700 Keele Street	Toronto, ON	M3J 1P3
Toronto and Region Conservation	June	Little	Manager, Development, Planning and Regulation	5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Deborah	Martin-Downs	Director of Ecology	5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Carolyn	Woodland	Senior Director, Planning, Greenspace and Communications	5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Suzanne	Bevan	Senior Planner, Environmental Assessment	5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Jan	Moryk		5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Diana	Khademi	Water Resources Engineering	5 Shoreham Drive	Downsview ON	M3N 1S4
Toronto and Region Conservation	Arlen	Leeming	Project Manager	5 Shoreham Drive	Downsview, ON	M3N 1S4
Toronto and Region Conservation	June	Little	Senior Manager, Planning and Development	5 Shoreham Drive	Downsview ON	M3N 1S4
York Region	Brett	Bloxam	Manager, Operations and Maintenance	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Augustine	Ko		17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Shahid	Matloob		17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Vi	Bui		17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Stephen	Fung	Manager, Capital Planning	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Richard	Hui	Manager, System Planning, Transportation and Community Planning	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Dan	Kostopoulos	Director, Capital Planning	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region	Brian	Titherington	Director, Roads and Traffic Operations	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
York Region			Regional Clerk	17250 Yonge Street	Newmarket, ON	L3Y 6Z1
<b>UTILITIES</b>						
Bell Canada	Lynn	Chaput		Floor 5 Blue, 100 Borough Drive	Scarborough, ON	M1P 4W2
Bell Canada	Leanne	DeMarco	Executive Office of Customer Relations	P.O. Box 593 Station A	Toronto, ON	M5W 1E4
Enbridge Gas	Jim	Arnot	Municipal Coordination Advisor	500 Consumers Road	North York ON	M2J 1P8
Enbridge Gas Distribution	Jamie	Delaney		500 Consumers Road	Scarborough ON	M1P 4W2
Enbridge Gas	Jamie	Rochford	Planning & Design Lead	101 Honda Blvd	Markham, ON	L6C0M6
Enbridge Gas	Neerajah	Raviraj	Planning Manager			
Hydro One Networks	Walter	Klootra	Manager, Transmission Lines Sustainment Investment Planning	483 Bay Street, North Tower, 15th Floor	Toronto, ON	M5G 2P5
Hydro One Networks						
Powerstream			EA Coordinator	161 Cityview Boulevard	Vaughan, ON	L4H 0A9
Powerstream	Rey	Tenorlas	Supervisor Capital	162 Cityview Boulevard	Vaughan, ON	L4H 0A09
Powerstream	Patrick	Leung		161 Cityview Boulevard	Vaughan, ON	L4H 0A09
Powerstream	Joe	Crozier		161 Cityview Boulevard	Vaughan, ON	L4H 0A09
Powerstream	Tony	D'Onofrio		161 Cityview Boulevard	Vaughan, ON	L4H 0A09
Powerstream	Jiacheng	Zhang		161 Cityview Boulevard	Vaughan, ON	L4H 0A09
Rogers Cable	Brian	Duggan		244 Newkirk Road	Richmond Hill, ON	L4C 3S5
Rogers Cable	Roger	Patton	Director of Technical Operations	301 Marwood Drive	Oshawa, ON	L1H 1J4
<b>TRANSIT</b>						
Metrolinx GO Transit	Trevor	Anderson	Transportation Planner, Policy, Planning and Innovation	20 Bay Street, Suite 600	Toronto, ON	M5J 2W3
Metrolinx GO Transit	Lisa	Salsburg	Regional Transportation Plan	20 Bay Street, Suite 600	Toronto, ON	M5J 2W3
Toronto Transit Commission - TYSSE	Srdjana	Jaksic	Property & Agreements Coordinator	5160 Yonge Street, 11th Floor	Toronto, ON	M2N 6L6
Toronto Transit Commission - TYSSE	Ronan	Olwill	Subway Design Review	1900 Yonge Street	Toronto, ON	M4S 1Z2
York Region Transit - York Region Rapid Transit Corporation	Arad	McHaghegh	Project Specialist, Subways	3601 Highway 7, 12th Floor	Markham, ON	L3R 0M3
York Region Transit - York Region Rapid Transit Corporation	Carolyn	Ryall	Design Chief, Infrastructure and Development	50 High Tech Road, 5th Floor	Richmond Hill, ON	L4B 4N7
York Region Transit - York Region Rapid Transit Corporation	Mary Frances	Turner	President	50 High Tech Road, 5th Floor	Richmond Hill ON	L4B 4N7
York Region Transit - York Region Rapid Transit Corporation	Nima	Naghavi	Project Specialist I	3601 Highway 7, 12th Floor	Markham, ON	L3R 0M3
York Region Transit - York Region Rapid Transit Corporation	Michael	Norton		3601 Highway 7	Markham, ON	L3R 0M3
Canadian National Railway				277 Front Street West, Suite 920	Toronto, ON	M5V 2X4
St. John Ambulance - York Region Branch				10325 Yonge Street, Suite 202	Richmondhill, ON	L4E 1A4

# **Notices, Invitations and Reply Forms**



June 12, 2015

Mr. Stephen Keen, P.Eng.  
Director, Transportation  
CIMA Canada Inc.  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT STUDY FOR:  
PORTAGE PARKWAY WIDENING FROM APPLEWOOD CRESCENT TO JANE STREET  
AND  
PORTAGE PARKWAY EXTENSION FROM JANE STREET TO CREDITSTONE ROAD**

Dear Mr. Keen,

The City of Vaughan has initiated the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to inform your agency of the Study, and invite a representative of your agency to participate as a member of the Technical Agencies Committee (TAC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

- Part A – Portage Parkway Widening from Applewood Crescent to Jane Street
- Part B – Portage Parkway Extension from Jane Street to Creditstone Road

Further information and details regarding the Study are provided in the enclosed Notice of Study Commencement. A representative of your agency is invited to participate as a member of the TAC. The Committee will meet in advance of key decision points to review areas of interest, the potential need for approvals, and the evaluation of alternative solutions and designs.

We kindly ask you to indicate your interest in the Study and/or TAC by completing and returning the enclosed reply form to the undersigned before June 26, 2015. If this Notice has reached you in error, we would appreciate it if you could forward this request to the appropriate contact within your organization or advise CIMA either by returning the reply form or contacting the undersigned directly.

We look forward to your reply. Should you have any questions, please do not hesitate to contact the undersigned at (289) 288-0287, ext. 6812 or [Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca).

Sincerely,

**CIMA Canada Inc.**

Sonya Kapusin, MCIP, RPP  
Consultant EA Planner  
[Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca)

Encl.

cc: Marta Roias, RPP, City of Vaughan  
Stephen Keen, P.Eng., CIMA

**REPLY FORM (Please print)**

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

**Date:** \_\_\_\_\_  
**Name:** \_\_\_\_\_  
**Title:** \_\_\_\_\_  
**Agency:** \_\_\_\_\_  
**Address:** \_\_\_\_\_  
\_\_\_\_\_ **Postal Code:** \_\_\_\_\_  
**Phone:** \_\_\_\_\_ **Fax:** \_\_\_\_\_  
**Email:** \_\_\_\_\_

**Does your agency wish to be kept informed of the Study? (Circle Yes or No)**

Yes                  No

**Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)**

Yes                  No

**Part of interest (Circle Part A or Part B or Both):**

Part A                  Part B                  Both (Part A and Part B)

**Comments:** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Please return this form to the contact below by June 26, 2015:**

**Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)**

June 12, 2015

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT STUDY FOR:  
PORTAGE PARKWAY WIDENING FROM APPLEWOOD CRESCENT TO JANE STREET  
AND  
PORTAGE PARKWAY EXTENSION FROM JANE STREET TO CREDITSTONE ROAD**

Dear Land Owner,

The City of Vaughan has initiated the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to inform you of the Study and invite you to participate as a member of the Stakeholders Group.

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

- Part A – Portage Parkway Widening from Applewood Crescent to Jane Street
- Part B – Portage Parkway Extension from Jane Street to Creditstone Road

Further information and details regarding the Study are provided in the enclosed Notice of Study Commencement. You are invited to participate as a member of the Stakeholders Group. The Group will meet in advance of key decision points to review areas of interest and the evaluation of alternative solutions and designs.

We kindly ask you to indicate your interest in the Study and/or Stakeholders Group by completing and returning the enclosed reply form to the undersigned before June 26, 2015.

We look forward to your reply. Should you have any questions, please do not hesitate to contact the undersigned at (289) 288-0287, ext. 6812 or [Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca).

Sincerely,

**CIMA Canada Inc.**



Sonya Kapusin, MCIP, RPP  
Consultant EA Planner  
[Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca)

Encl.

cc: Marta Roias, RPP, City of Vaughan  
Stephen Keen, P.Eng., CIMA

**REPLY FORM (Please print)**

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

**Date:** \_\_\_\_\_  
**Name:** \_\_\_\_\_  
**Affiliation (if applicable):** \_\_\_\_\_  
**Address:** \_\_\_\_\_  
\_\_\_\_\_ **Postal Code:** \_\_\_\_\_  
**Phone:** \_\_\_\_\_ **Fax:** \_\_\_\_\_  
**Email:** \_\_\_\_\_

**Do you wish to be kept informed of the Study? (Circle Yes or No)**

Yes                  No

**Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)**

Yes                  No

**Part of interest (Circle Part A or Part B or Both):**

Part A                  Part B                  Both (Part A and Part B)

**Comments:** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Please return this form to the contact below by June 26, 2015:**

**Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)**

## COMMENCEMENT NOTICE ENVIRONMENTAL ASSESSMENT STUDY

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street (Part A) and extend Portage Parkway from Jane Street to Creditstone Road (Part B) as a strategic network improvement and in support of the development in the Vaughan Metropolitan Centre.

### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class EA process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class Environmental Assessment process.

The City has initiated the Class EA Study, which will establish the preferred alternative design for the widening, extension and improvements for:

- Portage Parkway Widening from Applewood Crescent to Jane Street (\*Part A)
- Portage Parkway Extension from Jane Street crossing the Black Creek channel to Creditstone Road (\*Part B)

The Study will consider the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding for two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B will be prepared and available for public review and comment on study completion.

### CONSULTATION

Consultation with interested stakeholders (public and agencies) is a key component of the study. Public Information Centres (PIC) will be held to present the study, review the scope and discuss issues related to the project including alternative solutions and designs, evaluation criteria, and environmental impacts and mitigation measures. Public Information Centre details will be advertised closer to consultation dates.

For information on study progress and updates, please visit:

[http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

### CONTACTS

If you have any questions/comments or wish to be added to the study mailing list, please contact:

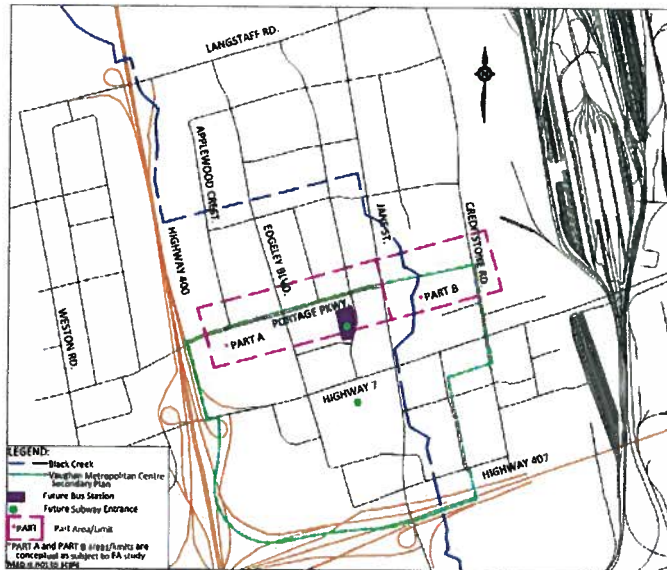
**Marta Rojas, RPP**  
City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
E: [Marta.Rojas@vaughan.ca](mailto:Marta.Rojas@vaughan.ca)

or

**Stephen Keen, P.Eng.**  
CIMA Canada Inc. Consultant Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7  
T: 289-288-0287 Ext. 6834  
F: 289-288-0285  
E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)

*Information is being collected under the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.*

**ANDREW PEARCE**, Director of Development Engineering and Infrastructure Planning Services





## COMMENCEMENT NOTICE ENVIRONMENTAL ASSESSMENT STUDY

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

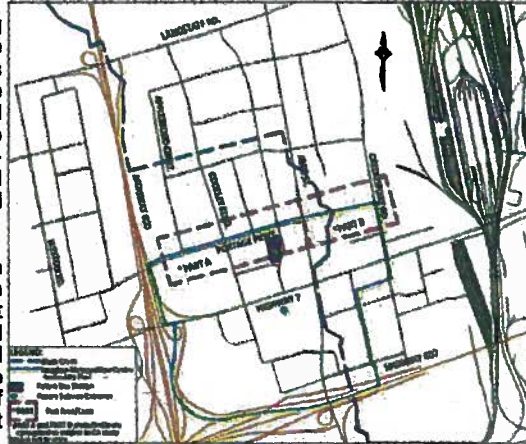
### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street (Part A) and extend Portage Parkway from Jane Street to Creditstone Road (Part B) as a strategic network improvement and in support of the development in the Vaughan Metropolitan Centre.

### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class EA process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class Environmental Assessment process.



The City has initiated the Class EA Study, which will establish the preferred alternative design for the widening, extension and improvements for:

- Portage Parkway Widening from Applewood Crescent to Jane Street ("Part A")
- Portage Parkway Extension from Jane Street crossing the Black Creek channel to Creditstone Road ("Part B")

The Study will consider the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding for two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B will be prepared and available for public review and comment on study completion.

### CONSULTATION

Consultation with interested stakeholders (public and agencies) is a key component of the study. Public Information Centres (PIC) will be held to present the study, review the scope and discuss issues related to the project including alternative solutions and designs, evaluation criteria, and environmental impacts and mitigation measures. Public Information Centre details will be advertised closer to consultation dates.

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<b>Marta Rolas, RPP</b> <b>City of Vaughan Project Manager</b> 2141 Major Mackenzie Drive Vaughan, ON L6A 1T1 T: 905-832-8585 Ext. 8026 F: 905-832-6145 E: Marta.Rolas@vaughan.ca	or	<b>Stephen Keen, P.Eng.</b> <b>CIMA Canada Inc. Consultant Project Manager</b> 3027 Harvester Road, Suite 400 Burlington, ON L7N 3G7 T: 289-288-0287 Ext. 6834 F: 289-288-0285 E: Stephen.Keen@cima.ca
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**ANDREW PEARCE**, Director of Development Engineering and Infrastructure Planning Services





## COMMENCEMENT NOTICE ENVIRONMENTAL ASSESSMENT STUDY

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street (Part A) and extend Portage Parkway from Jane Street to Creditstone Road (Part B) as a strategic network improvement and in support of the development in the Vaughan Metropolitan Centre.

### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class EA process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class Environmental Assessment process.



The City has initiated the Class EA Study, which will establish the preferred alternative design for the widening, extension and improvements for:

- Portage Parkway Widening from Applewood Crescent to Jane Street (\*Part A)
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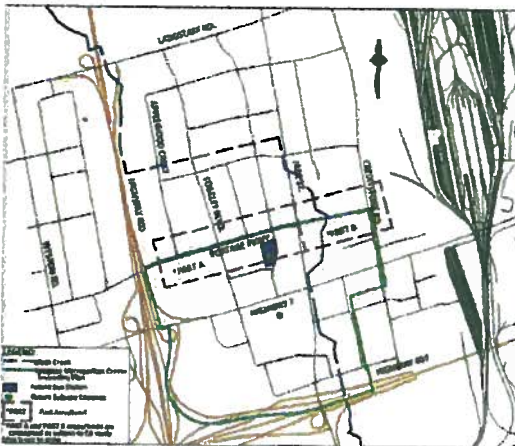
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**ANDREW PEARCE**, Director of Development Engineering and Infrastructure Planning Services

This Notice first issued June 12, 2015

## **COMMENCEMENT NOTICE ENVIRONMENTAL ASSESSMENT STUDY**

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**ANDREW PEARCE**, Director of Development Engineering and Infrastructure Planning Services

November 09, 2015

Environmental Assessment Coordination Unit  
Aboriginal Affairs and Northern Development Canada  
25 St. Clair Avenue East, 8th Floor  
Toronto, ON M4T 1M2

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT STUDY FOR:  
PORTAGE PARKWAY WIDENING FROM APPLEWOOD CRESCENT TO JANE STREET  
AND  
PORTAGE PARKWAY EXTENSION FROM JANE STREET TO CREDITSTONE ROAD  
PUBLIC INFORMATION CENTRE #1**

Dear Sir/Madam,

The City of Vaughan is advancing the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to inform your agency of the Study, and invite a representative of your agency to attend the first Public Information Centre (PIC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

- Part A – Portage Parkway Widening from Applewood Crescent to Jane Street
- Part B – Portage Parkway Extension from Jane Street to Creditstone Road

The PIC will be held to present the study, including information on alternative planning solutions, existing conditions, evaluation criteria, and design considerations. The PIC will be an opportunity to review project information and ask questions or discuss comments with members of the project team and is being held on:

**DATE: Wednesday, November 25, 2015**

**TIME: 5:00 pm to 8:00 pm (drop-in session)**

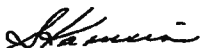
**LOCATION: Homewood Suites by Hilton, 618 Applewood Crescent, Vaughan, Ontario**

Further information and details regarding the Study are provided in the enclosed Notice of Public Information Centre #1. If this Notice has reached you in error, we would appreciate it if you could forward this request to the appropriate contact within your organization or advise CIMA by contacting the undersigned directly.

Should you have any questions, please do not hesitate to contact the undersigned at (289) 288-0287, ext. 6812 or [Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca).

Sincerely,

**CIMA Canada Inc.**



Sonya Kapusin, MCIP, RPP  
Consultant EA Planner  
[Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca)

cc: Marta Roias, RPP, City of Vaughan  
Stephen Keen, P.Eng., CIMA



November 09, 2015

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT STUDY FOR:  
PORTAGE PARKWAY WIDENING FROM APPLEWOOD CRESCENT TO JANE STREET  
AND  
PORTAGE PARKWAY EXTENSION FROM JANE STREET TO CREDITSTONE ROAD  
PUBLIC INFORMATION CENTRE #1**

Dear Landowner,

The City of Vaughan is advancing the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to inform you of the Study, and invite you to attend the first Public Information Centre (PIC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

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cc: Marta Roias, RPP, City of Vaughan  
Stephen Keen, P.Eng., CIMA



**NOTICE OF PUBLIC INFORMATION CENTRE #1  
ENVIRONMENTAL ASSESSMENT STUDY**

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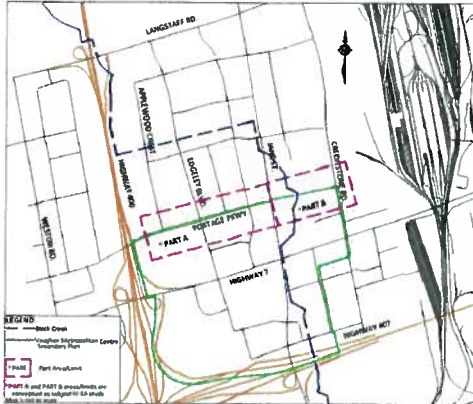
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**THE PROCESS**

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This Notice first issued November 10, 2015



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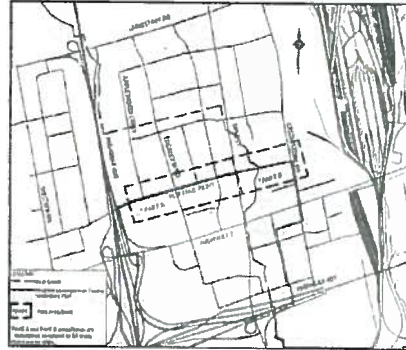
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This Notice first issued November 10, 2015





## NOTICE OF PUBLIC INFORMATION CENTRE #1 ENVIRONMENTAL ASSESSMENT STUDY

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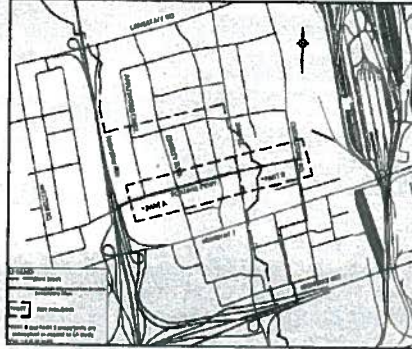
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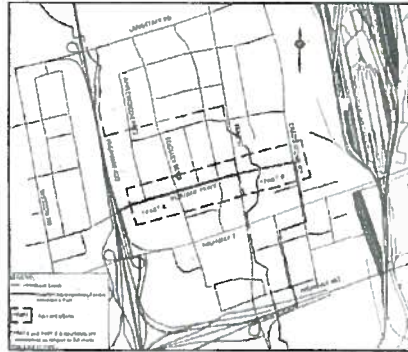
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The City has initiated the Class EA Study, which will establish the preferred alternative design for the widening, extension and improvements for:

- PART A\*: Portage Parkway Widening from Applewood Crescent to Jane Street
- PART B\*: Portage Parkway Extension from Jane Street crossing the Black Creek channel to Creditstone Road

The Study will consider the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding for two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B will be prepared and available for public review and comment on study completion.

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**DATE: Wednesday, November 25, 2015**

**TIME: 5:00 pm to 8:00 pm (drop-in session)**

**LOCATION: Homewood Suites by Hilton, 618 Applewood Crescent, Vaughan, Ontario**

For information on study progress and updates, please visit:  
[http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

**CONTACTS**

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**Marta Roias, RPP**  
City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
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**ANDREW PEARCE, Director, Development Engineering and Infrastructure Planning**

This Notice first issued November 10, 2015



## NOTICE OF PUBLIC INFORMATION CENTRE #1 ENVIRONMENTAL ASSESSMENT STUDY

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

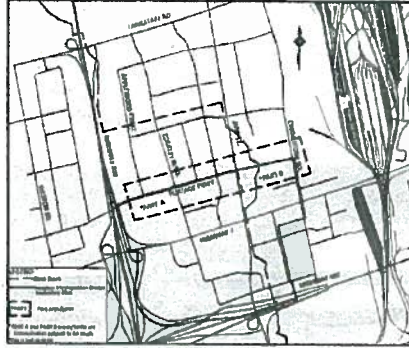
### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street (Part A) and extend Portage Parkway from Jane Street to Creditstone Road (Part B) as a strategic network improvement and in support of the development in the Vaughan Metropolitan Centre.

### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class Environmental Assessment (EA) process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class EA process.



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The Study will consider the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding for two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B will be prepared and available for public review and comment on study completion.

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**ANDREW PEARCE**, Director, Development Engineering and Infrastructure Planning

This Notice first issued November 10, 2015

February 23, 2016

**Re: City of Vaughan Environmental Assessment Study for:  
Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Portage Parkway Extension from Jane Street to Creditstone Road  
Public Information Centre No. 2**

Dear Sir/Madam

The City of Vaughan is completing the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to invite a representative of your agency to attend the second and final Public Information Centre (PIC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

- Part A – Portage Parkway Widening from Applewood Crescent to west of the Black Creek
- Part B – Portage Parkway Extension from west of the Black Creek, crossing the Black Creek channel to Creditstone Road

The first Public Information Centre (PIC) was held on November 25, 2015 to present the study, including information on alternative planning solutions, existing conditions, evaluation criteria, and design considerations. With consideration for public input, the preferred solution was selected as a combination of alternatives:

- Travel Demand Management Initiatives being implemented by the City as a separate strategy
- Alternative Modes of Transportation (e.g., sidewalk, cycling systems)
- Localized Intersection and Operational Improvements (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street crossing the Black Creek channel to Creditstone Road (e.g., new 4 lane roadway)

A second PIC will be held to present the assessment of alternative design concepts for the widening and extension of Portage Parkway and preliminary preferred design. The PIC will be an opportunity to review project information and ask questions or discuss comments with members of the project team. The PIC will be held on:

**Wednesday, March 9, 2016  
5:00 pm to 8:00 pm (drop-in session)  
Homewood Suites by Hilton, 618 Applewood Crescent, Vaughan, Ontario**

Further information and details regarding the Study are provided in the enclosed Notice of Public Information Centre No. 2. Should you have any questions, please do not hesitate to contact the undersigned at (289) 288-0287 ext. 6834 or [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca).

Sincerely,



Stephen Keen, P.Eng.

Consultant Project Manager

cc: Marta Roias, RPP, City of Vaughan

February 23, 2016

**Re: City of Vaughan Environmental Assessment Study for:  
Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Portage Parkway Extension from Jane Street to Creditstone Road  
Public Information Centre No. 2**

Dear Landowner,

The City of Vaughan is completing the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to invite you to attend the second and final Public Information Centre (PIC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

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Sincerely,



Stephen Keen, P.Eng.

Consultant Project Manager

cc: Marta Roias, RPP, City of Vaughan



**NOTICE OF PUBLIC INFORMATION CENTRE #2  
ENVIRONMENTAL ASSESSMENT STUDY**



**Portage Parkway Widening and Easterly Extension to Creditstone Road  
Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

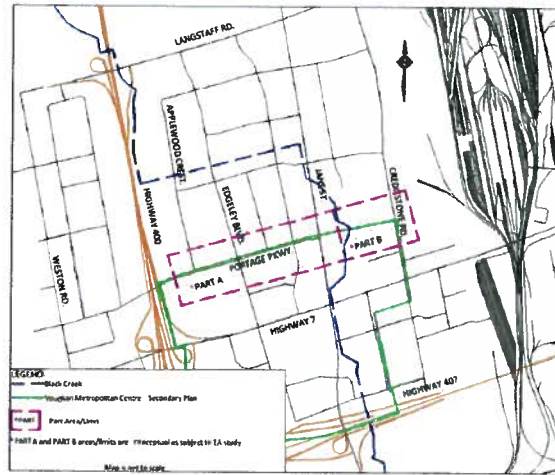
**THE STUDY**

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street and extend Portage Parkway from Jane Street to Creditstone Road) as a strategic network improvement and in support of development in the Vaughan Metropolitan Centre.

**THE PROCESS**

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class Environmental Assessment (EA) process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class EA process.



The City is completing the Class EA Study, which establishes the preferred design for the widening, extension and improvements for:

- PART A\*:Portage Parkway Widening from Applewood Crescent to the west side of the Black Creek
- PART B\*:Portage Parkway Extension from the west side of the Black Creek, crossing the Black Creek channel, to Creditstone Road

The Study considers the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding in two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B, the limits of which are undergoing refinement through the process, will be prepared and available for public review and notices issued upon study completion.

**PUBLIC CONSULTATION – YOU ARE INVITED**

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**You are invited to attend the second and final PIC for this study, which is being held to present the assessment of alternative design concepts for the widening and extension of Portage Parkway and preliminary preferred design.**

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**ANDREW PEARCE**, Director, Development Engineering and Infrastructure Planning

This Notice first issued February 23, 2016

# NOTICE OF PUBLIC INFORMATION CENTRE #2 ENVIRONMENTAL ASSESSMENT STUDY



## Portage Parkway Widening and Easterly Extension to Creditstone Road Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and Part B: Portage Parkway Extension from Jane Street to Creditstone Road

### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street and extend Portage Parkway from Jane Street to Creditstone Road) as a strategic network improvement and in support of development in the Vaughan Metropolitan Centre.

### THE PROCESS

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## NOTICE OF PUBLIC INFORMATION CENTRE #2 ENVIRONMENTAL ASSESSMENT STUDY



### Portage Parkway Widening and Easterly Extension to Creditstone Road Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and Part B: Portage Parkway Extension from Jane Street to Creditstone Road

#### THE STUDY

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This Notice first issued February 23, 2016





## NOTICE OF PUBLIC INFORMATION CENTRE #2 ENVIRONMENTAL ASSESSMENT STUDY



### Portage Parkway Widening and Easterly Extension to Creditstone Road Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and Part B: Portage Parkway Extension from Jane Street to Creditstone Road

#### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street and extend Portage Parkway from Jane Street to Creditstone Road as a strategic network improvement and in support of development in the Vaughan Metropolitan Centre.

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This Notice first issued February 23, 2016

# NOTICE OF PUBLIC INFORMATION CENTRE #2

## ENVIRONMENTAL ASSESSMENT STUDY



### Portage Parkway Widening and Easterly Extension to Creditstone Road Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and Part B: Portage Parkway Extension from Jane Street to Creditstone Road

#### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street and extend Portage Parkway from Jane Street to Creditstone Road as a strategic network improvement and in support of development in the Vaughan Metropolitan Centre.



#### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class Environmental Assessment (EA) process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class EA process.

The City is completing the Class EA Study, which establishes the preferred design for the widening, extension and improvements for:

- PART A\*: Portage Parkway Widening from Applewood Crescent to the west side of the Black Creek
- PART B\*: Portage Parkway Extension from the west side of the Black Creek, crossing the Black Creek channel, to Creditstone Road

The Study considers the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding in two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B, the limits of which are undergoing refinement through the process, will be prepared and available for public review and notices issued upon study completion.

#### PUBLIC CONSULTATION – YOU ARE INVITED

Consultation with interested stakeholders (public and agencies) is a key component of the study. The first of two Public Information Centres (PICs) for the study was held on November 25, 2015 to present the alternative planning solutions, future conditions, evaluation criteria and design considerations. With consideration for public input, the preferred solution was selected as a combination of alternatives:

- Travel Demand Management Initiatives being implemented by the City as a separate strategy
- Alternative Modes of Transportation (e.g., sidewalk, cycling systems)
- Localized Intersection and Operational Improvements (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street crossing the Black Creek channel to Creditstone Road (e.g., new 4 lane roadway)

**You are invited to attend the second and final PIC for this study, which is being held to present the assessment of alternative design concepts for the widening and extension of Portage Parkway and preliminary preferred design.**

The PIC will be an opportunity for the public to review project information and ask questions or discuss comments with members of the project team and is being held on:

**DATE: Wednesday, March 9, 2016**

**TIME: 5:00 pm to 8:00 pm (drop-in session)**

**LOCATION: Homewood Suites by Hilton, 618 Applewood Crescent, Vaughan, Ontario**

For information on study progress and updates, please visit:

[http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

#### CONTACTS

If you have any questions/comments and cannot attend the PIC, please contact:

**Marta Rolas, RPP**  
City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
E: Marta.Rolas@vaughan.ca

or **Stephen Keen, P.Eng.**  
CIMA Canada Inc. Consultant Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7  
T: 289-288-0287 Ext. 6834  
F: 289-288-0285  
E: Stephen.Keen@cima.ca

*Information is being collected under the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.*

**ANDREW PEARCE**, Director, Development Engineering and Infrastructure Planning

April 21, 2016

**Re: City of Vaughan Environmental Assessment Study for:  
Portage Parkway Widening and Easterly Extension to Creditstone Road**

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

Dear Sir or Madam,

The City of Vaughan is completing the Class Environmental Assessment (Class EA) Study for the planned widening and easterly extension of Portage Parkway from Applewood Crescent to Creditstone Road. **As a landowner along Portage Parkway within the study limits, you are invited to meet with the EA Study Project Team to review specific issues associated with the preliminary preferred design.**

The preliminary preferred design will require property on the north and south side of the existing Portage Parkway and the easterly extension to Creditstone Road. A plan showing the proposed road improvements in the vicinity of your property can be viewed online at [http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx). If you would like a hardcopy of the plan, please contact the undersigned.

Two Public Information Centres (PICs) were held on November 25<sup>th</sup>, 2015 and March 9<sup>th</sup>, 2016 to present the study, including the assessment of alternative planning solutions and design concepts, and the preliminary preferred design. Based on the assessment of alternatives and public consultation, the preliminary preferred design (the project limits of which are subject to the study process) is to:

- Widen Portage Parkway to a 33 metre right-of-way that accommodates improvements to active transportation facilities, 4 through lanes (2 in each direction), and centre left and auxiliary turn lanes at intersections, facilitating vehicular travel demand, movements and operations.
- Extend Portage Parkway from West of Black Creek to Creditstone Road, via an alignment for a new road within a right-of-way (approximately 28m – 33m) that accommodates a structured crossing of the Black Creek, active transportation facilities, 4 through lanes, and auxiliary turn lanes at intersections, facilitating vehicular travel demand, movements and operations.

Landowners along the Portage Parkway study area corridor are invited to attend an open house to review specific issues associated with their property. The **open house** will be held:

**Thursday May 5, 2016  
4:00 p.m. – 8:00 p.m.  
City of Vaughan City Hall – Committee Room 244  
2141 Major Mackenzie Drive, Second Floor**

Should you wish to schedule a meeting during this time or on an alternate day, please contact either of the project team contacts below by May 3, 2016 at:

**Marta Roias, RPP**                      **or**  
**City of Vaughan Project Manager**  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

**Stephen Keen, P.Eng.**  
**CIMA Canada Inc. Consultant Project Manager**  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7  
T: 289-288-0287 Ext. 6834  
F: 289-288-0285  
E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)

We look forward to hearing from you. Please do not hesitate to contact the undersigned at (289) 288-0287 ext. 6834 or [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca) should you have any questions regarding this study.

Sincerely,



Stephen Keen, P.Eng.  
Consultant Project Manager

cc: Marta Roias, RPP, City of Vaughan

# **Meetings, Meeting Minutes and Public Information Centre Boards**



## MINUTES OF MEETING

**CLIENT** : City of Vaughan (City)

**PROJECT** : Environmental Assessment Study for  
Part A – Portage Parkway Widening, Applewood Crest. to Jane St.  
Part B – Portage Parkway Extension, Jane St. to Creditstone Rd.

**MEETING** : Meeting with Toronto and Region Conservation Authority (TRCA)

**DATE OF MEETING** : May 15, 2015, 9:30 am

**LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive, 2<sup>nd</sup> Floor  
Engineering Commission Boardroom

**ATTENDEES** : Dana Khademi, Water Resources Engineer, TRCA  
Jan Moryk, Planning Ecologist, TRCA  
June Little, Senior Manager, Planning and Development, TRCA  
Suzanne Bevan, Senior Planner, TRCA  
Marta Roias, Transportation Project Manager, City  
Jennifer Cappola-Logullo, VMC Project Manager, City  
Saad Yousaf, Storm Drainage Engineer, City  
Chris Davidson, Water Resources Engineer, Golder Associates  
Ltd. (Golder)  
Richard Booth, Ecologist, Golder  
Stephen Keen, Project Manager, CIMA Canada Inc. (CIMA)  
Sonya Kapusin, Environmental Planning, CIMA

**C.C. TO** : Attendees

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**Note:** If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.



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**DISCUSSION TOPICS****ACTION BY**

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**1 WELCOME AND INTRODUCTIONS**

All present were welcomed and asked to introduce themselves.

The main purpose of the meeting was to initiate discussion regarding hydraulic modelling and criteria for the Black Creek crossing at Portage Parkway.

**2 OVERVIEW OF EA STUDY**

The City presented background to the two concurrent Class EA projects for Portage Parkway, and the respective study areas.

CIMA presented the preliminary problem and opportunity for Portage Parkway as identified in the City's Transportation Master Plan (TMP):

- Serve the growing Vaughan Metropolitan Centre (VMC) and industrial area to the north.
- Provide a continuous route to Creditstone Road.
- Build on and refine the need and justification identified in previous studies.

A number of technical reports (environmental investigations) will be generated to support the Class EA (e.g., Hydrology, Stormwater Management, Environmental Impact Study (EIS)).

CIMA/Golder

**3 PROJECT ISSUES**

The City presented background to the Black Creek EA.

**Regarding Edgely Pond at Jane Street and Highway 7:**

- Quantity control is for 100 year storm and not for Regional storm.
- Water quality was considered in the VMC Master Servicing for Portage Parkway.
- Currently there are two culverts crossing Highway 7 (one culvert short for capacity).



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**DISCUSSION TOPICS****ACTION BY**

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**Regarding Black Creek Class EA:**

- The Black Creek EA is currently on hold; It is anticipated to be reinitiated in the first quarter of 2016 and finalized in the fourth quarter of 2017.
- The forebay pond has approval through the VMC TMP; Detailed design for the pond is anticipated by late 2016.
- Four lanes are anticipated to cross over the Black Creek.

**4 DATA REQUIREMENTS****Regarding Portage Parkway EA:**

- CIMA is responsible for project management.
- Golder is responsible for specialties (e.g., hydraulics, cultural heritage, archaeology, topographic survey).

**Regarding hydraulics and flood levels:**

- Study must support existing hydraulic conditions. CIMA/Golder
  - TRCA would like to see the Stormwater Management plan, how water will be treated from the road and erosion setbacks. Golder
  - Look at Regional flood levels as the base. Golder
  - Improvements to flood levels would be ideal; however, TRCA is looking for no impacts to flood levels at minimum. CIMA/Golder
  - Final ultimate design for change in downstream creek should be considered. Golder
  - Black Creek Optimization EA Study has up to date (2012) existing conditions hydraulics model. Golder
  - Future conditions model should include an update for 2 to 3 culverts downstream of Portage Parkway. Golder
  - HEC RAS and hydrology models will be provided by TRCA; Request model through Suzanne Bevan at TRCA and carbon copy to Marta Roias at the City and Stephen Keen at CIMA; Chris Davidson will communicate with TRCA for the model. TRCA/Golder
- Regarding Environmental Impact Study (EIS):**
- Approach for EIS is outlined on the TRCA website. Golder
  - Include existing conditions, impacts and mitigation. Golder
  - Creek realignment and vegetation removal are anticipated.
  - Stake features to determine extent of impacts. CIMA/Golder
  - Consider compensation where mitigation is not possible in the context of the overall plan of the pond and corridor. Golder
  - Note changes to responsibilities of Fisheries and Oceans and the proponent with regard to approvals. City/CIMA/Golder

DISCUSSION TOPICS	ACTION BY
<ul style="list-style-type: none"> <li>TRCA noted that watercourses may be present within the study area of Part A.</li> </ul>	
<p><b>Regarding Aboriginal Consultation:</b></p>	
<ul style="list-style-type: none"> <li>A Stage 2 Archaeological Assessment was completed for the pond as a result of a Part II Order on the Servicing Plan; The City will forward a copy to CIMA.</li> </ul>	City
<ul style="list-style-type: none"> <li>Broader study highlights areas with potential for archaeological concern.</li> </ul>	
<ul style="list-style-type: none"> <li>Berkley Development owns lands at Jane and Highway 7; The City will forward a contact to CIMA for property access.</li> </ul>	City/CIMA
<ul style="list-style-type: none"> <li>The Ministry of Natural Resources and Forestry (MNR) would need to be involved if endangered species are involved; The potential for endangered species was screened out in previous studies for the area.</li> </ul>	
<p><b>5 NEXT STEPS</b></p>	
<p><b>Regarding TRCA review process:</b></p>	
<ul style="list-style-type: none"> <li>Send Notice of Study Commencement to TRCA for screening and contact information.</li> </ul>	CIMA/TRCA
<ul style="list-style-type: none"> <li>Send Notice of PIC and PIC materials to TRCA.</li> </ul>	CIMA
<ul style="list-style-type: none"> <li>TRCA indicated they would need two weeks to review PIC materials.</li> </ul>	TRCA
<ul style="list-style-type: none"> <li>Send full copy of Draft Environmental Study Report (ESR) to TRCA for review; One month is needed for review.</li> </ul>	CIMA
<p><b>Regarding Technical Agencies Committee (TAC):</b></p>	
<ul style="list-style-type: none"> <li>A TAC will be established for this project; TRCA will respond to the TAC invitation.</li> </ul>	CIMA/TRCA

## MINUTES OF MEETING

- CLIENT** : City of Vaughan (City)
- PROJECT** : Environmental Assessment Study for  
Part A – Portage Parkway Widening, Applewood Crest. to Jane St.  
Part B – Portage Parkway Extension, Jane St. to Creditstone Rd.
- MEETING #1** : Meeting with Technical Agencies Committee (TAC) and Stakeholders Group (SG)
- DATE OF MEETING** : July 16, 2015, 2:00 pm
- LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive, Committee Room 242
- SG ATTENDEES** :
- 
- TAC ATTENDEES** : Powerstream (Jiacheng Zhang, Tony D’Onofrio)  
York Catholic District School Board (Christine Hyde)  
York Region (Shahid Matloob)
- City of Vaughan (Amy Roots, Danny Woo, Dino Macchiusi, Gerardo Paez Alonso, Jennifer Cappola-Logullo, Niaz Muhammad, Steven Dixon)
- PROJECT TEAM** : City of Vaughan (Marta Roias)  
CIMA Canada Inc. (CIMA; Stephen Keen, Sonya Kapusin)
- C.C. TO** : Metrolinx (Briana Ingram)  
Ministry of Environment and Climate Change (Nisha Shirali)  
Toronto and Region Conservation (Suzanne Bevan)  
Toronto-York Spadina Subway Extension (Adam Light)  
York Region (Vi Bui, Richard Hui, Brian Titherington)

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**DISCUSSION TOPICS****ACTION BY**

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York Region Rapid Transit Corporation (Carolyn Ryal)

City of Vaughan (Brienne Clace, Frank Facchini, Selma Hubjer, Andy Lee, Stephen Lue, Andrew Pearce, Daniel Rende, Paul Salerno, Tim Simmonds, Saad Yousaf)

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Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.



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**DISCUSSION TOPICS****ACTION BY**

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**1 WELCOME AND INTRODUCTIONS**

The City of Vaughan (City) welcomed all present and introduced the project. Refer to slide 1 in the attached presentation for the project title and study area (Part A and Part B). During the meeting and in response to questions on the study schedule, the City clarified that the study, proceeding concurrently for two parts, is anticipated to be completed at the end of 2016. The approach is intended to provide flexibility and respond to either the need to expedite either Part and/or issues that arise that may otherwise cause delay to either Part relative to anticipated completion.

CIMA introduced the project team and initiated roundtable introductions.

Refer to presentation slide 2 for the meeting agenda. The meeting was a starting point for discussion. All were invited to ask questions and share comments during the presentation. A copy of the presentation is attached.

Refer to presentation slide 3 for a sample list of agencies and stakeholders who were invited to participate on either TAC or the Stakeholders Group.

**2 PURPOSE OF TAC AND STAKEHOLDERS GROUP**

Refer to presentation slide 4 for the role of TAC and the Stakeholders Group. Up to three (3) meetings with each group will be held for this project. The TAC and Stakeholders Group meetings may be held jointly or separately.

**3 BACKGROUND AND PLANNING CONTEXT**

Refer to presentation slide 5 for information on Portage Parkway in the Transportation Master Plan (TMP), Vaughan Metropolitan Centre (VMC) Transportation Plan, and VMC Secondary Plan. Portage Parkway is the north boundary of the VMC Secondary Plan area.

Refer to presentation slides 6, 7, and 8 for the VMC Secondary Plan Street Network showing Portage Parkway as a Major Collector and the typical cross-sections for 33 m and 28 m right-of-way (ROW), respectively. The main difference between the two cross-sections is the centre planted median within the 33 m ROW.

**4 OVERVIEW OF CLASS EA STUDY**

Refer to presentation slide 9 for information on the Schedule C Class Environmental Assessment (EA) process. Phases 1 and 2 were completed by the City's TMP and Phases 3 and 4 will be completed as part of this study. The project website is included in the presentation slide for future updates.

In response to questions, the City informed that 18 months (study completion toward the end of 2016) was the planned timeframe for the Class EA.



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**DISCUSSION TOPICS****ACTION BY**

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**5 COORDINATION WITH OTHER PLANNING INITIATIVES**

Refer to presentation slides 10, 11, and 12 for other planning initiatives that will need to be coordinated with the Class EA study for Portage Parkway, including:

- Existing and future intersection improvements
- Road improvements, realignment, and extension
- Rapid Transit Corridor
- New watermain
- Watercourse crossing

stated a concern over the proximity of the road extension to the existing CN Rail spur noting that a criteria of a 30m buffer from the rail to the nearest curb cut was a Transport Canada standard. A concern was also noted around the future viability of the area for industry given the various bicycle/pedestrian and potential mixed used development being introduced into this predominately industrial area.

noted that on the south side of Portage Parkway, there are a lot of active development applications, some of which construction is imminent. Of particular interest will be the staging of intersection improvements and driveways as part of new developments and ensuring they are compatible with the ultimate design for Portage Parkway. A cycle track is currently being designed for Applemill Road. These comments were noted and the study team agreed to meet with in the near future.

City/CIMA

also noted that functionality as well as landscaping is most important for Portage Parkway given the needs of the adjacent employment area.

It was noted there are conflicting design requirements and that the VMC Streetscape and Open Space Plan were simply "guidelines" and not standards – further work is required to develop a recommended cross-section for the road widening and extension.

In response to questions regarding the taking of property for road widening, it was noted that widening is often done equidistant around the centreline, but when development is active on one side of the road only, this may not be the case.

It was also noted there is an active development application for the southeast corner of Portage Parkway and Jane Street and consideration for staging of widening and extension to include the east leg of the Portage Parkway / Jane Street Intersection – CIMA have already received the BA Transportation Impact Study report.

**6 DESIGN GUIDELINES**

Refer to presentation slides 13, 14 and 15 for a list of elements, such as streetscape and open space plans and gateway intersections, which will guide the design for



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**DISCUSSION TOPICS****ACTION BY**

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Portage Parkway. As indicated above, the City clarified that these are elements that will guide the design but are not formally called "design guidelines".

**7****DISCUSSION OF EXISTING CONDITIONS**

Refer to presentation slides 16, 17, 18 and 19 for a description of existing conditions within the study area.

**8****DISCUSSION OF PROBLEMS AND OPPORTUNITIES**

Refer to presentation slides 20, 21 and 22 for a summary of opportunities that have been identified to date for Portage Parkway, including a summary of:

- Forecasted east-west traffic volumes for 2031 horizon years; TAC and the Stakeholders Group noted the importance of not losing the functionality of Portage Parkway
- Forecasted north-south traffic volumes for 2013 horizon years; TAC and the Stakeholders Group asked whether the City planned to widen Creditstone Road to 4 lanes; The City indicated yes, and that the planning and design is not being advanced as part of this study but is subject to future study; This study will identify improvements to Creditstone Road as part of street network staging considerations (i.e. provision of turn lanes at the intersection of Creditstone Road with Portage Parkway).

**9****DISCUSSION OF ALTERNATIVE SOLUTIONS**

Refer to presentation slide 23 for a summary of planning considerations for Portage Parkway in the TMP. CIMA noted the study will follow 4 phases of the Class EA with 2 Public Information Centres (PICs). The City will use the results of the TMP for Phases 1 and 2 of the Class EA process. The focus of this study will be Phases 3 and 4 (developing alternative design concepts versus planning solutions).

**10****DISCUSSION OF EVALUATION CRITERIA**

Refer to presentation slides 24 and 25 for a typical list of criteria to evaluate designs. CIMA asked TAC and the Stakeholders Group to advise the project team if there are other criteria that should be considered.

TAC/Stakeholders  
Group

**11****NEXT STEPS**

Refer to presentation slide 26 for next steps in the study process. The first PIC planned for Fall will present information similar to the information presented at this meeting. The second PIC planned for early 2016 will focus on the preferred design solution.

**12****CONTACT INFORMATION**

Refer to presentation slide 27 for project contact information.

In response to questions, the project team noted:

- Anticipated completion of EA for the end of 2016 with a preferred alignment and 30% design concept



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**DISCUSSION TOPICS****ACTION BY**

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- Planned intersections within the study area will be coordinated with the design process for Portage Parkway
  - The existing ROW varies
- In summary, the following comments were noted:
- will provide information on planned bike lanes (Cycletrack) on Applewood Crescent
  - has major concerns for road widening and is interested in collaborating with the City; Clarification is needed on (1) How transit oriented development and the importance of a truck route will fit together; and (2) Land use conversion outside the VMC (employment lands); A buffer is critical; In response, the City noted that Portage Parkway (as north boundary/edge of the VMC Secondary Plan) provides for a transitional zone between the emerging urban Centre and existing surrounding industrial setting.
  - Concerns for truck entrance route at 24 hour facility and for security if access to property is needed; In response, the project team does not anticipate the need to access property from this point forward in the EA study

TAC/Stakeholders  
Group  
City/CIMA

City/CIMA



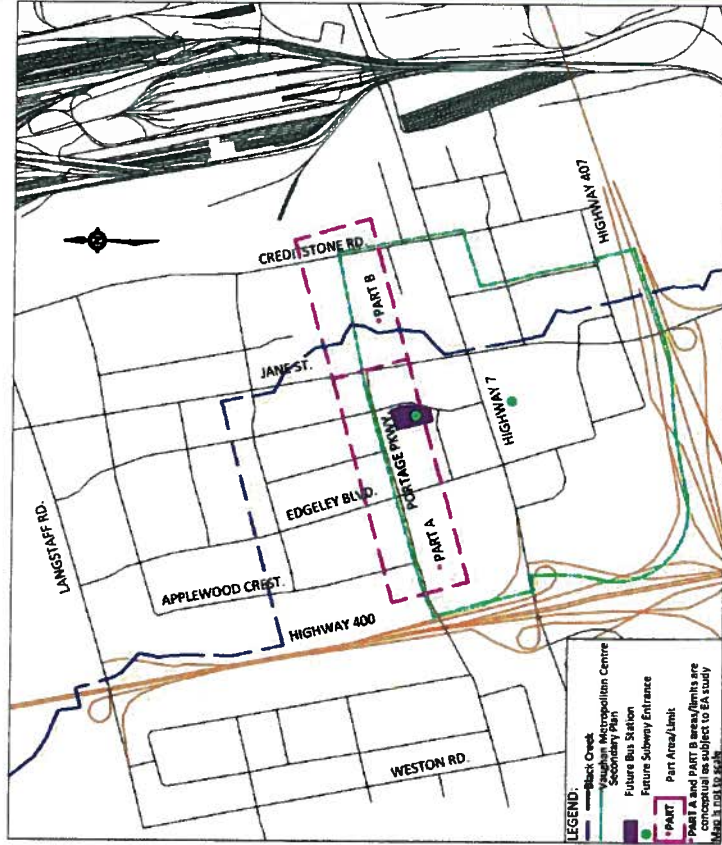


City of Vaughan

# Portage Parkway Environmental Assessment (EA)

Technical Agencies Committee  
Stakeholders Group  
Meeting No. 1

July 16, 2015



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# Agenda

- + **Introductions**
- + **Purpose of TAC and Stakeholders Group**
- + **Overview of Class EA Study**
- + **Coordination with Other Planning Initiatives**
- + **Discussion of Existing Conditions**
- + **Discussion of Problems and Opportunities**
- + **Discussion of Alternative Solutions**
- + **Discussion of Evaluation Criteria**
- + **Next Steps**

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## Introductions

- + **Technical Agencies Committee (TAC):**
  - + York Region
  - + York Region Transit
  - + York Region Rapid Transit Corporation (vivaNext)
  - + Toronto and Region Conservation
  - + York Catholic District School Board
  - + Ministry of Transportation Ontario (MTO)
  - + Metrolinx
  - + Ministry of Environment and Climate Change
  - + Powerstream
  - + Toronto-York Spadina Subway Extension (TYSSE) / Toronto Transit Commission (TTC)
- + **Stakeholders Group:**
  - + Property owners within 200 m of corridor
  - + Representatives of active / proposed development in the area

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## Purpose of TAC and Stakeholders Group

- + **Role of TAC and Stakeholders Group is to:**
  - + Review and comment on study findings
  - + Provide input on:
    - existing conditions
    - problems and opportunities
    - alternative solutions and design concepts
    - evaluation and design criteria
    - project effects and ways to minimize impacts
  
- + **Up to three (3) meetings to:**
  - + Identify problems and opportunities
  - + Identify and assess alternative solutions
  - + Identify and assess alternative design concepts



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## Background and Planning Context

- + **Transportation Master Plan (Phase 1 and 2 Class EA)**
  - + Provides need and justification for Portage Parkway
  - + Recommends Portage Parkway Widening and Easterly Extension to Creditstone Road
  
- + **Vaughan Metropolitan Centre (VMC) Transportation Plan**
  - + Portage Parkway:
    - Major Collector with minimum 28 to 33 m planned right-of-way
    - VMC boundary road forming part of Creditstone-Portage-Applewood Bypass
  
- + **VMC Secondary Plan**
  - + Municipal Servicing Class EA Master Plan
  - + Streetscape and Open Space Plan

# VMC Secondary Plan Street Network

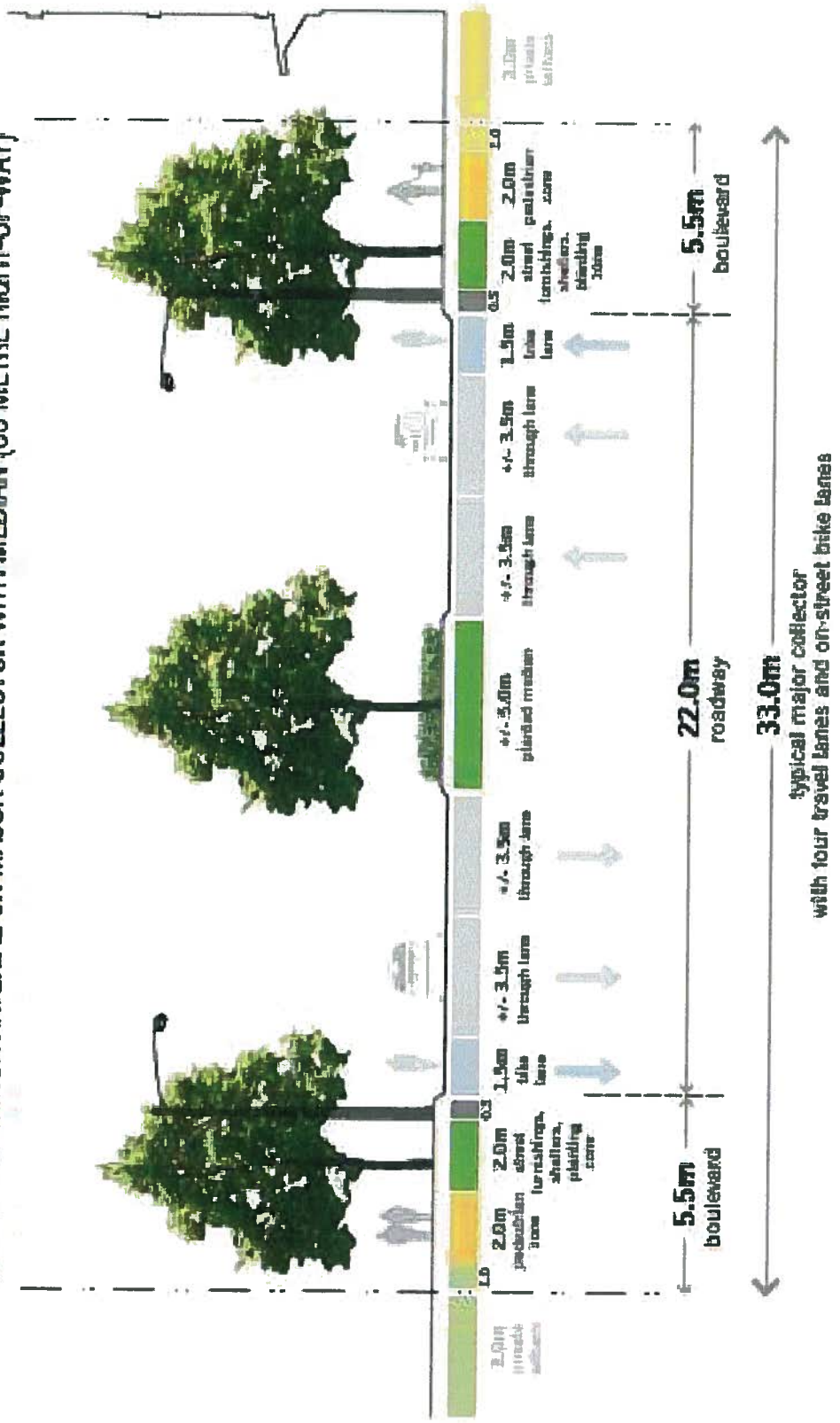


## SCHEDULE C > STREET NETWORK

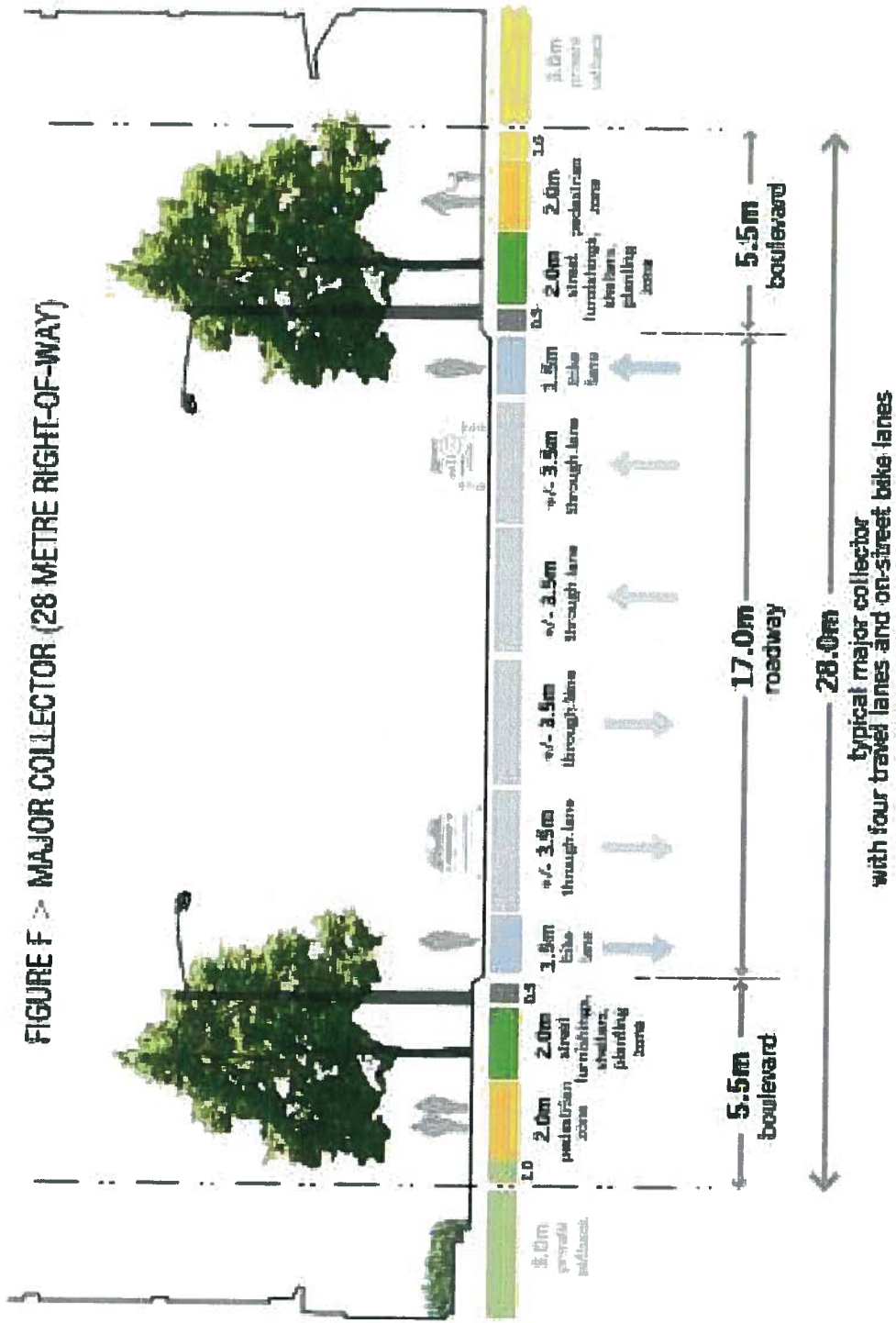
- LEGEND**
- arterials (54m+ row)
  - minor arterial (33m row)
  - major collectors (28-33m row)
  - special collector (33m row)
  - minor collectors (23-28m row)
  - local streets (15-17m row) or local streets
  - arterials (54m+ row)
  - arterials (54m+ row)
  - major parks and open spaces
  - see policy 4.3.17

# VMC Secondary Plan Typical Cross-Section

FIGURE E - MINOR ARTERIAL OR MAJOR COLLECTOR WITH MEDIAN (33 METRE RIGHT-OF-WAY)



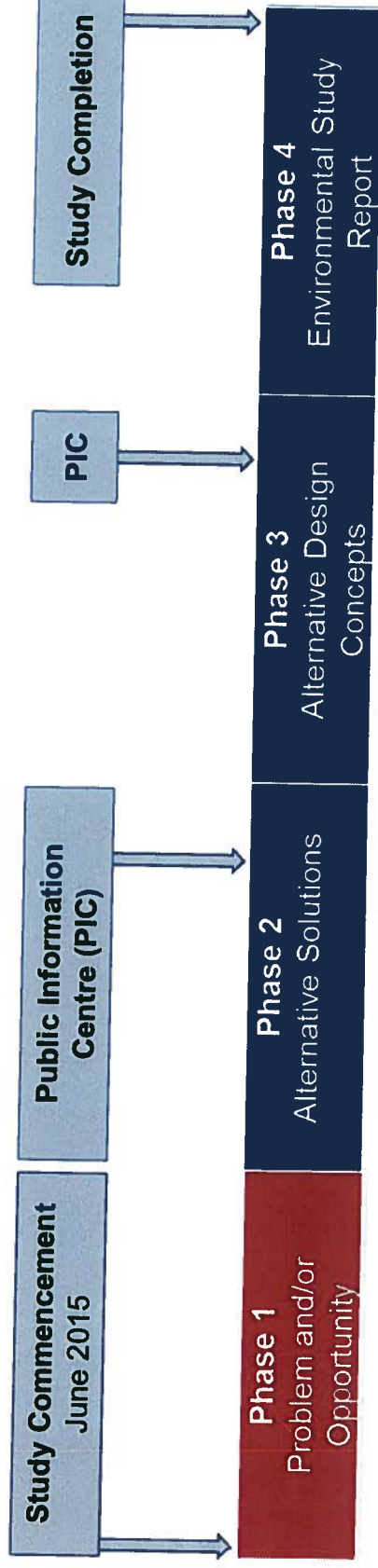
# VMC Secondary Plan Typical Cross-Section





# Overview of Class EA Study

- + **Schedule C Municipal Class EA**
- + Municipal Engineers Association, 2000 as amended in 2007 & 2011



- + **Phases 1 and 2 completed by the City of Vaughan TMP (2012)**
- + **Phases 3 and 4 recommended for the proposed widening and easterly extension of Portage Parkway to Creditstone Road**
- + **Project Website**

[http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

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## Coordination with other Planning Initiatives

- + **Street Network Implementation**
  - + Extension of Applewood Drive south to Highway 7
  - + Realignment of Millway Avenue
  - + Staging/interim design considerations
- + **York Region Rapid Transit Corridor on Jane Street**
- + **Streetscape and Open Space Plan**
  - + Active Transportation (Cycletrack, Sidewalks / Pedestrian Zones)
  - + Public Realm interface (Boulevards, Median, Gateway intersections)
- + **Black Creek Channel Crossing Structure**
  - + Black Creek Stormwater Optimization Master Plan
- + **Municipal Servicing**

# Coordination with other Planning Initiatives



VMC Secondary Plan (January 2013) information – for illustrative purposes



# Coordination with other Planning Initiatives



VMC Secondary Plan (January 2013) information – for illustrative purposes



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## Design Guidelines

- + **VMC Streetscape and Open Space Plan**
  - + Typical elements for mid-block section of Portage Parkway
    - Transitional zone between urban centre and employment lands
    - Landscape edge for the VMC
    - Pedestrian priority zones at Edgeley Boulevard
  
- + **City-wide Streetscape Implementation Manual**
  - + Design guidelines for streetscapes, identifying:
    - street types,
    - street zones, and
    - how components of each relate
  - + Levels of service for urban streetscapes:
    - standard urban,
    - enhanced, and
    - premium

# Design Guidelines

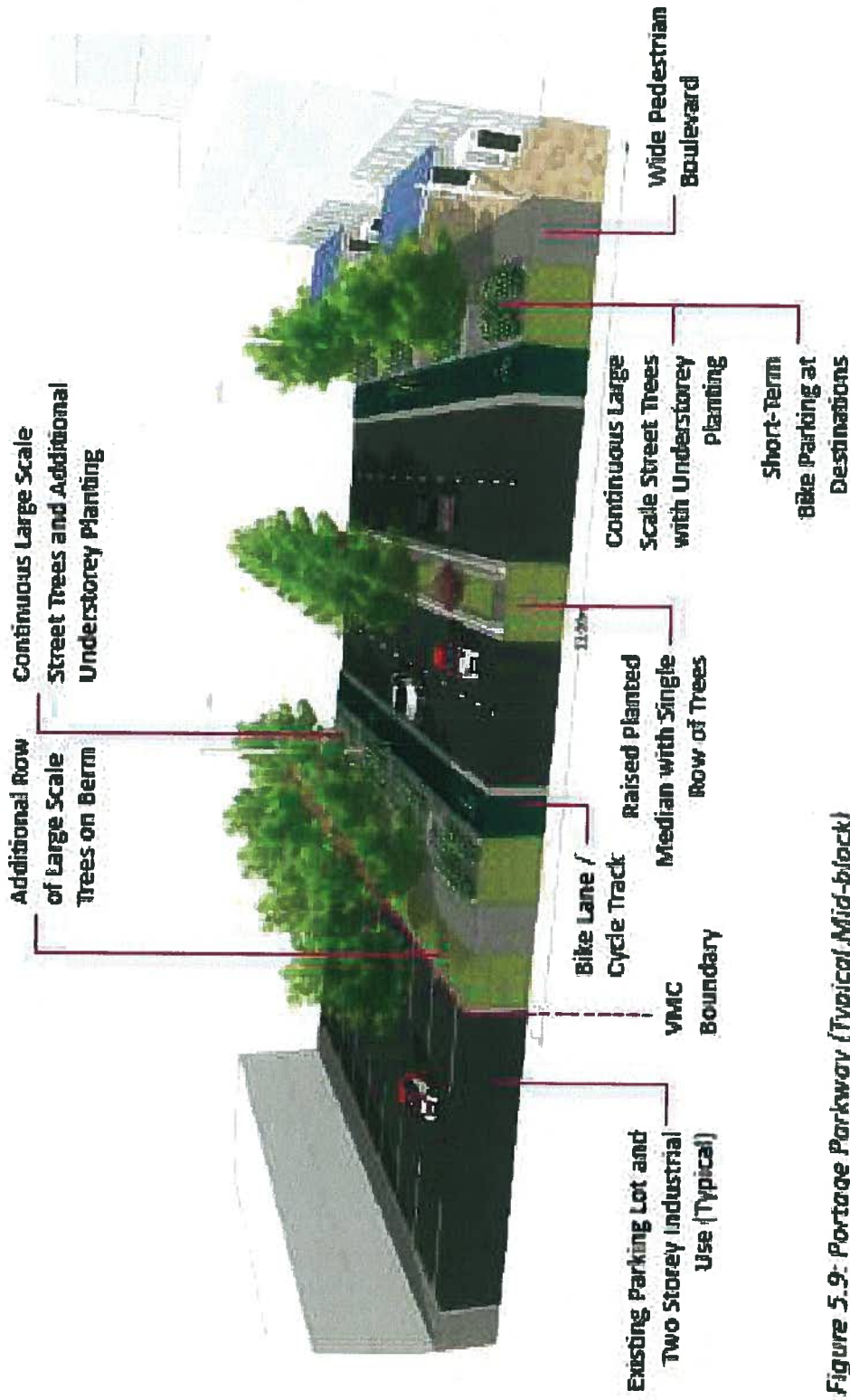
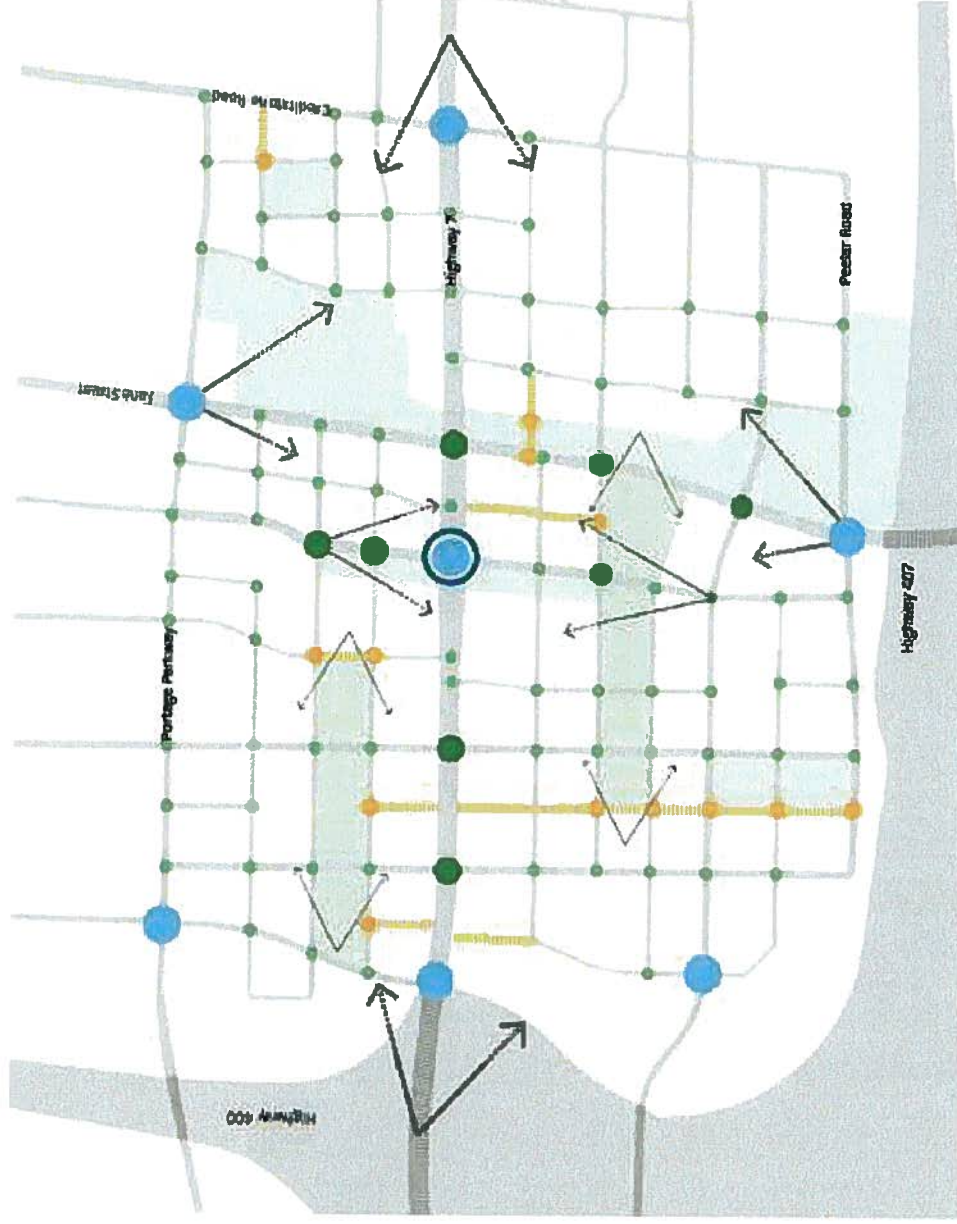










Figure 5.9: Portage Parkway (Typical Mid-block)

Vaughan Metropolitan Centre Streetscape and Open Space Plan, EDA Collaborative Inc., 2015

# Design Guidelines



- LEGEND**
-  Mobility Hub Gateway Intersection
  -  Gateway Intersection
  -  Major Intersection
  -  Minor Intersection
  -  Mews Intersection
  -  View Corridor
  -  Park Network
  -  Mews

Vaughan Metropolitan Centre Streetscape and Open Space Plan, EDA Collaborative Inc., 2015

---

## Discussion of Existing Conditions

- + **Part A: Portage Parkway, Applewood Crescent to Jane Street**
  - + Major east-west collector from Chrislea Road to Jane Street
    - 4 lane urban cross section from Chrislea Road to Edgeley Boulevard
    - 2 lane urban cross section from Edgeley Boulevard to Jane Street
- + Four (4) lanes over Highway 400 (completed in 2010)
  - Northern bypass to reduce congestion along Highway 7 and Highway 7 & 400 interchange
- + Transit Service
- + Sidewalks
- + Predominantly Industrial Land Uses



# Discussion of Existing Conditions



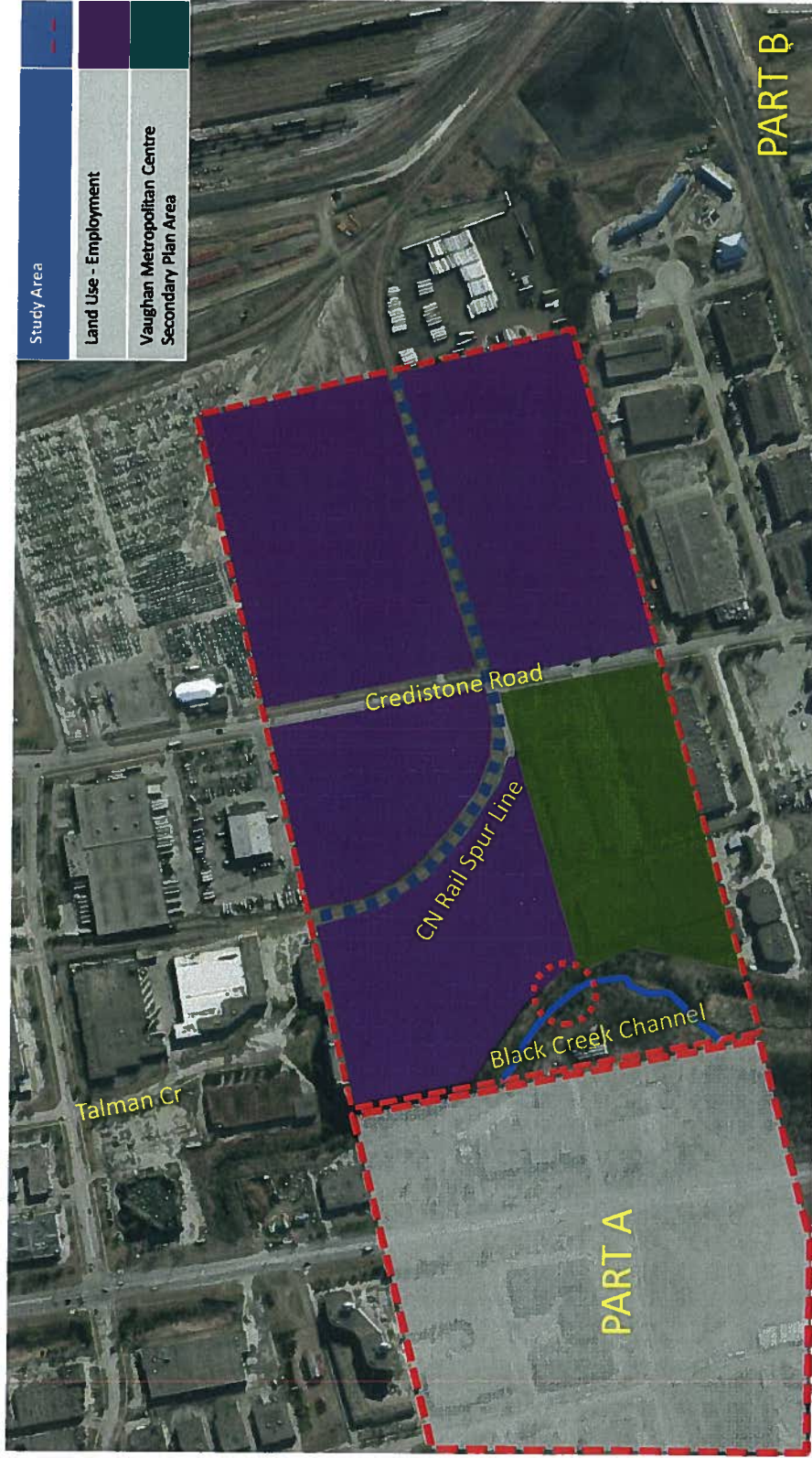


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## Discussion of Existing Conditions

- + **Part B: Portage Parkway, Jane Street to Creditstone Road**
- + Black Creek Channel Crossing
- + CN Rail Spur Line
- + Predominantly Industrial Land Uses
- + Heritage property at 7961 Jane Street (Ontario Heritage Act)
- + Cultural heritage landscape (cemetery) at 8001 Jane Street (Vaughan Heritage Inventory)

# Discussion of Existing Conditions



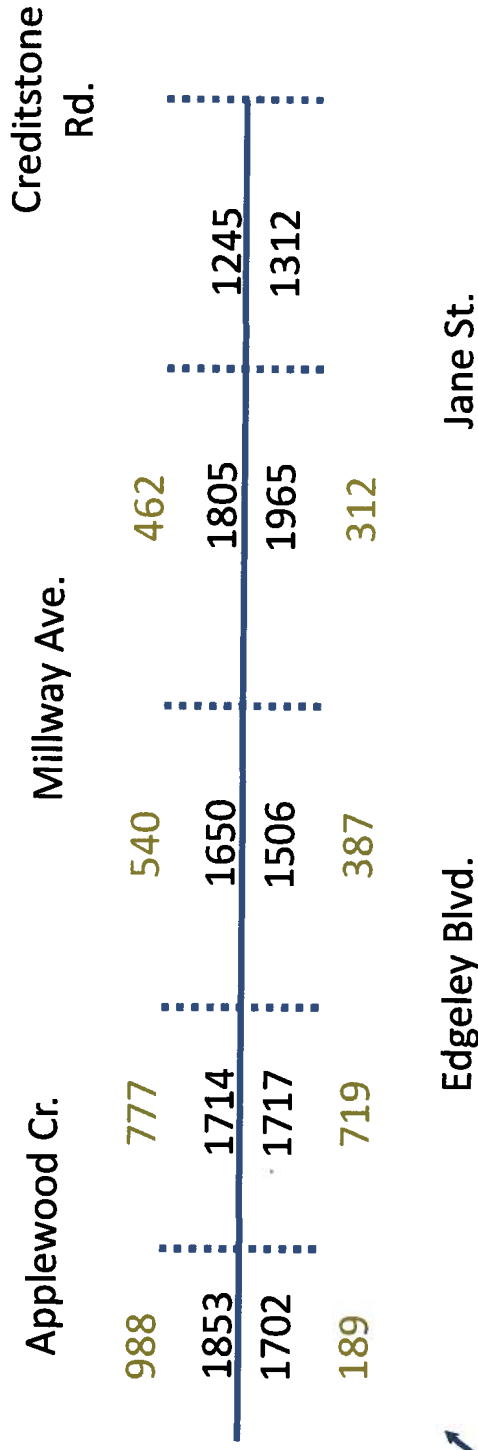
PART A and PART B area/conceptual as subject to EA study

---

## Discussion of Problems and Opportunities

- + **Serve VMC and surrounding employment area**
- + **Create alternative route for trucks to bypass the VMC core**
- + **Provide continuous route from Weston Road crossing Highway 400 and the Black Creek channel to Creditstone Road**
- + **Improve connections to local and regional infrastructure**
- + **Support transit-oriented nodes and corridors**
- + **Enhance transit ridership, cycling and walking**

# Forecasted East-West Volumes – 2031 horizon years

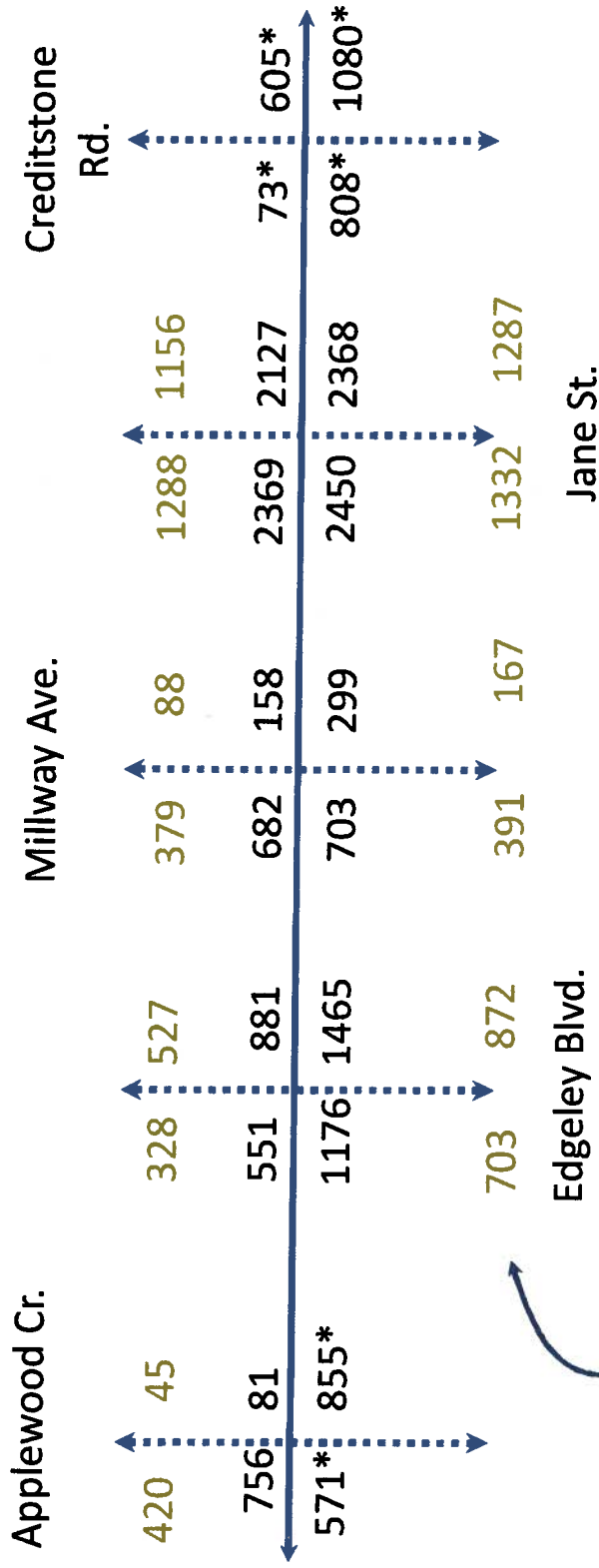


PM Peak (weekday) traffic counts, conducted in 2010, 2011 and 2014

**Legend:**  
 540 – PM Pk hr traffic count  
 1650 – EMME forecast (VMC) incl. truck diversions

Draft preliminary forecast link volumes

# Forecasted North-South Volumes – 2031 horizon years



**Legend:**  
 540 – PM Pk hr traffic count  
 1650 – EMME growth factor (VMC) applied to counts  
 \* except where counts not available

PM Peak (weekday) traffic counts, conducted in 2010, 2011 and 2014

Draft preliminary forecast link volumes



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## Discussion of Alternative Solutions

- + **Alternatives considered by the TMP:**
  - + “Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area. This led to the need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.”
- + **Socio-economic and Environmental Impacts assessed by the TMP:**
  - + “The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network.”

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## Discussion of Evaluation Criteria

- + **Technical**
  - + Alternative Transportation
  - + Level of Service
  - + Network Capacity
  - + Planning Objectives
  - + Stormwater Management/Creek Crossing
  - + Traffic Safety
  
- + **Financial**
  - + Capital Costs

---

# Discussion of Evaluation Criteria

- + **Natural**
  - + Aquatic
  - + Avian and Wildlife
  - + Natural Areas
  - + Species at Risk
  - + Vegetation
  - + Watercourses
  
- + **Socio-economic**
  - + Aesthetics and Streetscape
  - + Air Quality
  - + Archaeology
  - + Built Heritage and Cultural Landscapes
  - + Businesses
  - + Emergency Services
  - + Land Use
  - + Noise
  - + Property Access

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## Next Steps

- + **Complete existing conditions and traffic analysis**
- + **Finalize Problem and Opportunity Statement**
- + **Identify Alternative Designs and Alignments**
- + **Host TAC and Stakeholders Group Meeting No. 2**
- + **Host Public Information Centre No. 1**



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## Contact Information

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E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)

## MINUTES OF MEETING

- CLIENT** : City of Vaughan (City)
- PROJECT** : Environmental Assessment (EA) Study for  
Part A - Portage Parkway Widening, Applewood Crescent to Jane Street  
Part B - Portage Parkway Extension, Jane Street to Creditstone Road
- MEETING #1** : Meeting with Stakeholders Group (SG)
- DATE OF MEETING** : October 20, 2015, 2:00 pm
- LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive, 2<sup>nd</sup> Floor
- SG ATTENDEES** : 6 Stakeholders  
City of Vaughan (Jennifer Cappola-Logullo, Selma Hubjer)
- PROJECT TEAM** : City of Vaughan (Marta Roias)  
CIMA Canada Inc. (CIMA+; Stephen Keen, Sonya Kapusin)
- C.C. TO** : 11 Stakeholders  
City of Vaughan (Amy Roots, Andrew Pearce, Andy Lee, Brianne Clace, Daniel Rende, Danny Woo, Dino Macchiusi, Frank Facchini, Gerardo Paez Alonso, Niaz Muhammad, Paul Salerno, Saad Yousaf, Selma Hubjer, Stephen Lue, Steven Dixon, Tim Simmonds, Vince Suppa)

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Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

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**DISCUSSION TOPICS****ACTION  
BY**

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**1 STATUS OF CLASS EA STUDY**

A copy of the presentation for this meeting is attached. Refer to presentation slide 2 for the meeting agenda, and presentation slide 5 for the Class EA status.

The Study was at Phase 2 (Alternative Solutions) with plans for a Public Information Centre (PIC) in November to present the problem/opportunity statement and evaluation of alternative planning solutions to the public.

**2 DRAFT PUBLIC INFORMATION CENTRE DISPLAY**

Refer to presentation slides 3 to 31 for the draft PIC display. {Post meeting note: CIMA+ forwarded an electronic copy of the display to SG on October 23, 2015}. All present were asked to review and provide comments on the draft display by **November 3, 2015**.

TAC

**3 BACKGROUND**

Refer to presentation slides 7 to 16 for the transportation planning context and relevant information from the City's Transportation Master Plan (TMP).

**4 PROBLEM AND OPPORTUNITY STATEMENT**

Refer to presentation slide 17 for the problem and opportunity statement.

**5 PREFERRED PLANNING SOLUTION**

Refer to presentation slides 20 to 24 for a summary of alternative planning solutions. The recommended solution is a combination of alternatives:

- **Travel Demand Management Initiatives** were identified in the TMP and are being implemented by the City as a separate strategy
- **Alternative Modes of Transportation** were identified in the TMP, including continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- **Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- **Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- **Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)

**6 EXISTING CONDITIONS**

Refer to presentation slides 18 and 19 for an overview of existing conditions within the study area. The following investigations have been initiated:

- Stage 1 Archaeological Assessment
- Natural Sciences

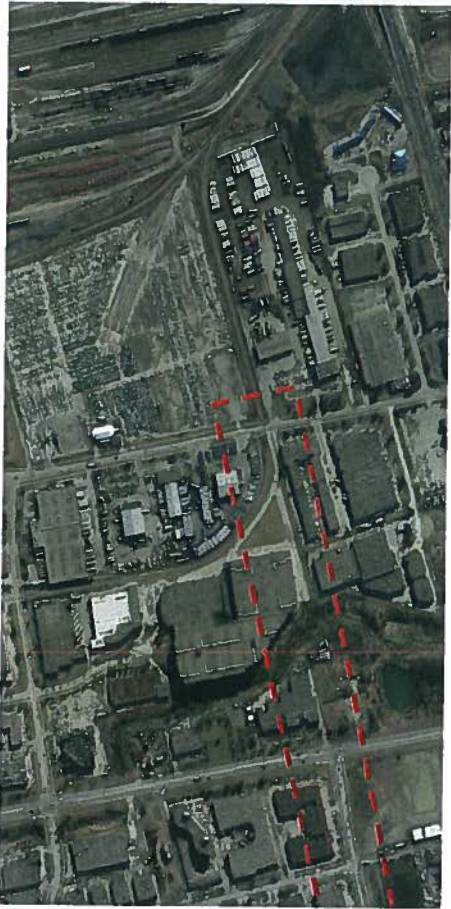
**7 ALTERNATIVE DESIGN CONCEPTS FOR PREFERRED SOLUTION**

Refer to presentation slides 25 to 27 for the design framework and evaluation criteria, and 28 to 29 for design opportunities and constraints. This information will guide the assessment of design alternatives for the preferred solution.

**8 NEXT STEPS**

- Forward comments on Draft PIC Display by **November 3, 2015**
- Attend PIC No. 1 on **November 25, 2015**
- Forward comments in response to PIC by **December 11, 2015**
- Meet with Project Team in advance of second PIC in **Spring 2016**

SG



City of Vaughan

# Portage Parkway Environmental Assessment

Stakeholders Group  
Meeting No. 2

October 20, 2015





# Agenda

- Status of Class EA Study**
- Draft Public Information Centre Display**
- Background**
  - Transportation Planning Context
  - Transportation Master Plan
- Problem and Opportunity Statement**
- Preferred Planning Solution**
- Existing Conditions**
  - Draft Technical Studies
  - Transportation and Traffic
  - Natural Environment
- Alternative Design Concepts for Preferred Solution**
  - Preliminary Design and Alignment Alternatives
  - Draft Evaluation Criteria
- Next Steps**
  - Comments on Draft Public Information Centre Display by **November 3, 2015**
  - Public Information Centre No. 1 planned for **November 25, 2015**



City of Vaughan  
**Portage Parkway  
Environmental Assessment**  
Public Information Centre No. 1

November 25, 2015



# Welcome to the Public Information Centre

This first Public Information Centre (PIC) introduces the Environmental Assessment (EA) Study for Portage Parkway Widening and Easterly Extension to Creditstone Road.

We welcome your input on:

- Background to the study
- Existing conditions of the study area
- Transportation problems or opportunities and solutions
- Design concepts for the preferred solutions
- Potential effects of the project and ways to avoid or minimize impacts

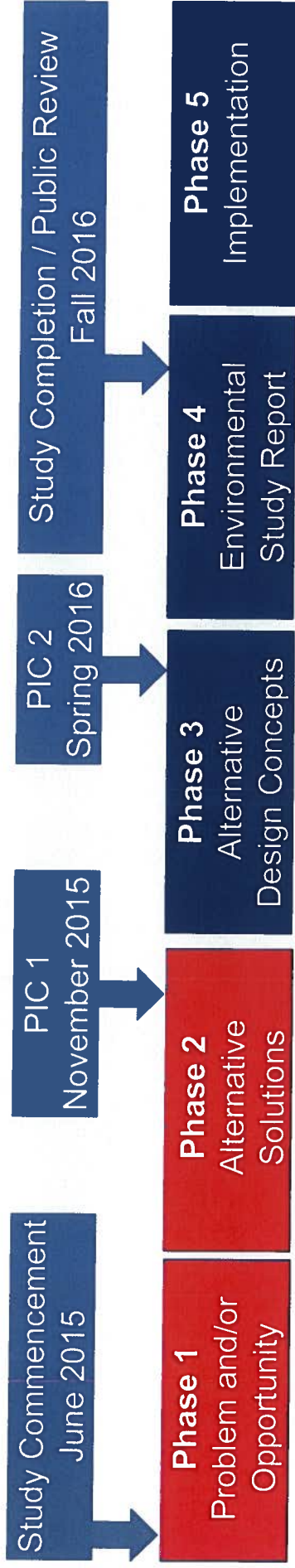
You are invited to:

- Review the information boards
- Ask questions and discuss your comments with the study team
- Fill out and submit a comment sheet
- Sign in to receive future updates on the study
- Visit the project website:

[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Class-EA-Study-for-Portage-Parkway.aspx)

# Class EA Process

- This project is subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (2000 as amended in 2007 and 2011).
- The Municipal Class EA is a planning and design process approved by the Ministry of Environment and Climate Change to meet the requirements of the *Environmental Assessment Act*.
- This study follows the Class EA process for **Schedule C** projects.



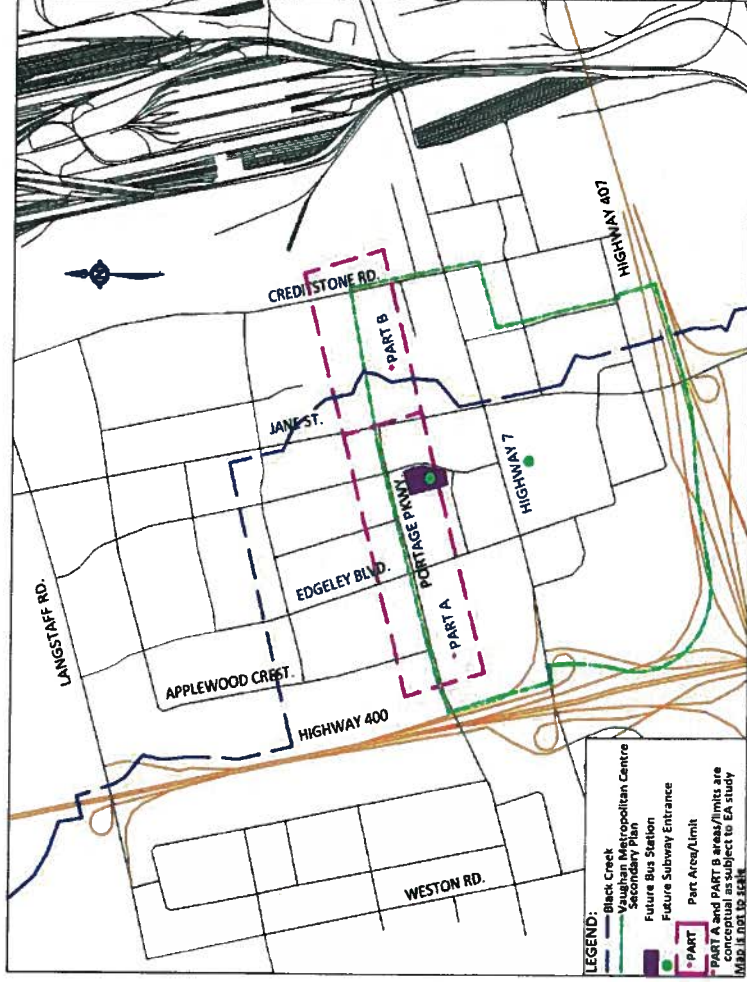
- The City's Transportation Master Plan, having satisfied Phases 1 and 2, initiated the planning and design process for the Portage Parkway Widening and Easterly Extension to Creditstone Road.
- This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
  - Part A - Portage Parkway Widening from Applewood Crescent to Jane Street
  - Part B - Portage Parkway Extension from Jane Street to Creditstone Road.



# Study Area

Improvements to Portage Parkway are being considered and proceeding as two interrelated Parts:

- **Part A** for the widening of Portage Parkway from two to four lanes from Applewood Crescent to Jane Street.
- **Part B** for the extension of Portage Parkway from Jane Street crossing the Black Creek Channel to Creditstone Road.



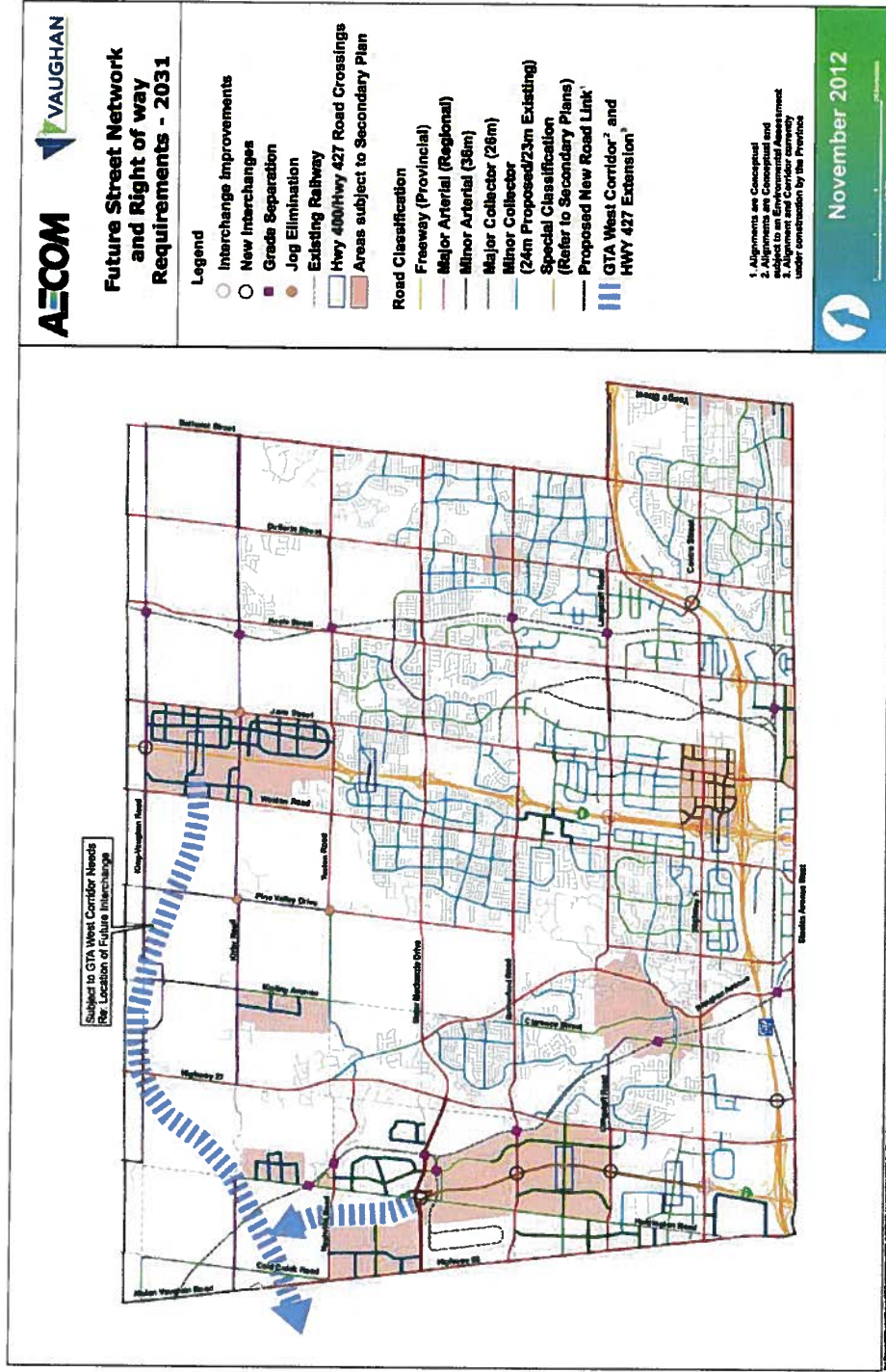
The total distance of the Study Area corridor is approximately 1.5 km.



# Transportation Master Plans

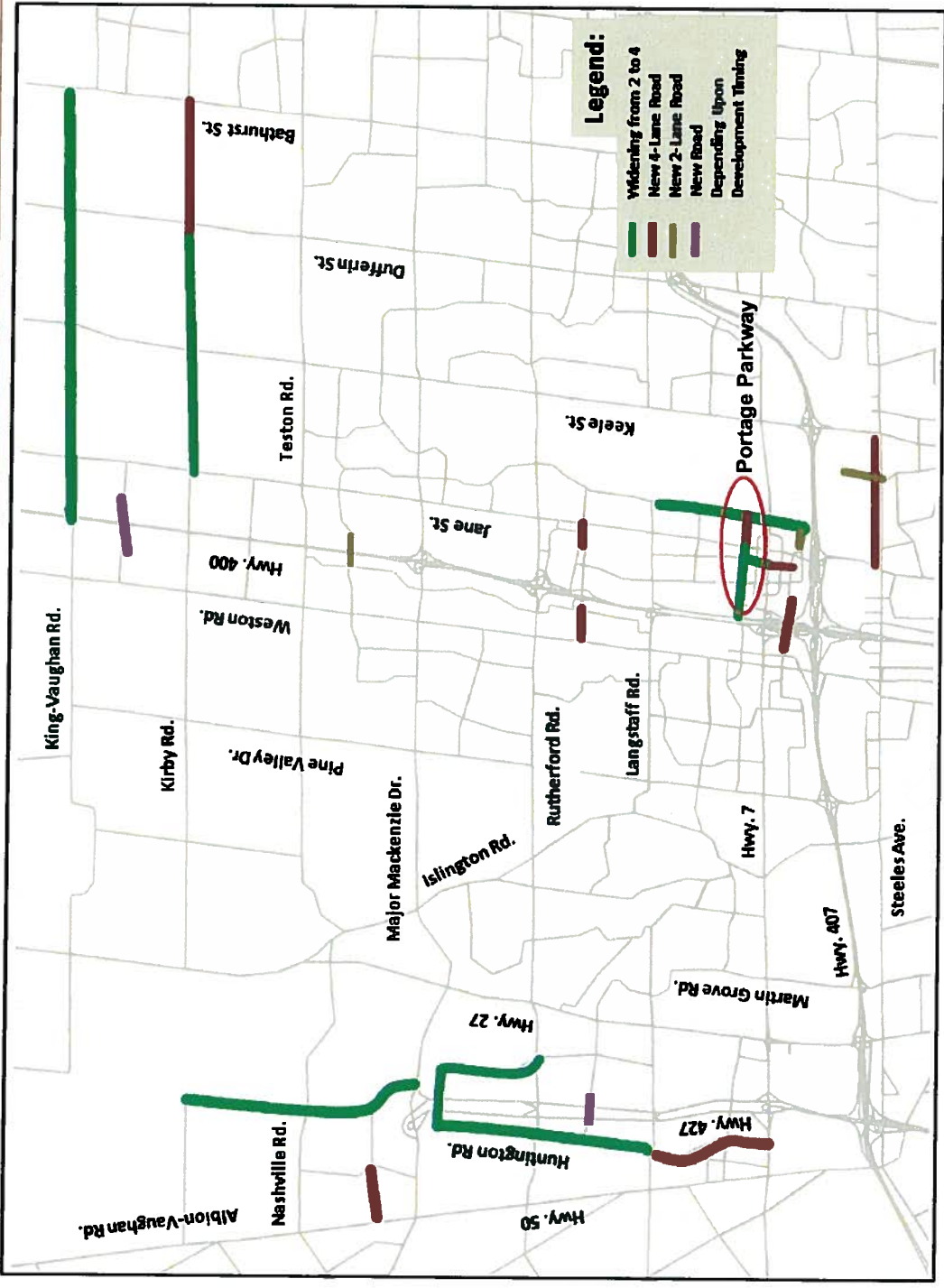
The City of Vaughan  
**Transportation Master Plan – A New Path** (2012) established the need and justification for the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element, supporting the planned level of intensification and building of the City's downtown - **Vaughan Metropolitan Centre (VMC)**.

The City of Vaughan **Pedestrian and Bicycle Master Plan** (2007) identified Portage Parkway for "Neighbourhood Bike Lanes" with pavement markings and signs.



Transportation Master Plan – A New Path, City of Vaughan, 2012  
 Pedestrian and Bicycle Master Plan Study, Marshall Macklin Monaghan with ESG International, 2007

# Recommended 2031 Road Improvements



**Road Improvements Needed by 2031  
(Assuming 15% Transit Share)** Transportation Master Plan – A New Path, City of Vaughan, 2012

# Transportation Master Plan

This study advances the planning and design process under the Municipal Class EA process for:

- Widening Portage Parkway to four lanes from Applewood Crescent to Jane Street, and
- Extending Portage Parkway with four lanes from Jane Street to Creditstone Road.


### Portage Parkway Widening and Easterly Extension

**Purpose:** With the recent completion of the Highway 400 crossing, an improved Portage Parkway is a key element of the improved VMC and road network, supporting intensification and transit-oriented development in Vaughan's pre-employment centre and future Downtown. The easterly extension to Creditstone Road will provide a direct link from the industrial area east of the VMC to the relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

**Project Description**

<b>Name</b>	<ul style="list-style-type: none"> <li>• Portage Parkway Widening and Easterly Extension</li> <li>• Between Highway 400 and Creditstone Road (see map area 7)</li> </ul>
<b>Project Limits</b>	<ul style="list-style-type: none"> <li>• Approximately 1.5 km (see map area 7)</li> </ul>
<b>Length</b>	<ul style="list-style-type: none"> <li>• Widening to 4 lanes from Applewood Crescent to Jane Street, and new 4 lane road from Jane Street to Creditstone Road</li> </ul>
<b>Project Type</b>	<ul style="list-style-type: none"> <li>• Widening to 4 lanes from Applewood Crescent to Jane Street, and new 4 lane road from Jane Street to Creditstone Road</li> </ul>
<b>Proposed Phasing</b>	<ul style="list-style-type: none"> <li>• 2012 – 2021</li> </ul>
<b>Cost</b>	<ul style="list-style-type: none"> <li>• \$4 M (excluding property)</li> </ul>

**Project Location and Environmental Features**



**Justification:**

- These are key road network improvements that will support the proposed development of the VMC. The proposed road network was developed in concert with the planning team that created the new secondary plan, and is supported by corridor deficiency analysis that identified the number of required lanes. An improved Portage Parkway will also facilitate the diversion of trucks around the VMC. In addition, the project strongly supports transit, cycling and pedestrian movement.

**Alternatives Considered**

**Transit**

- The Spadina subway extension, with its terminal station within the VMC, together with the planned Highway 7 Rapidway, provided the transit focus for the development of the new VMC transportation plan.

**Roads**

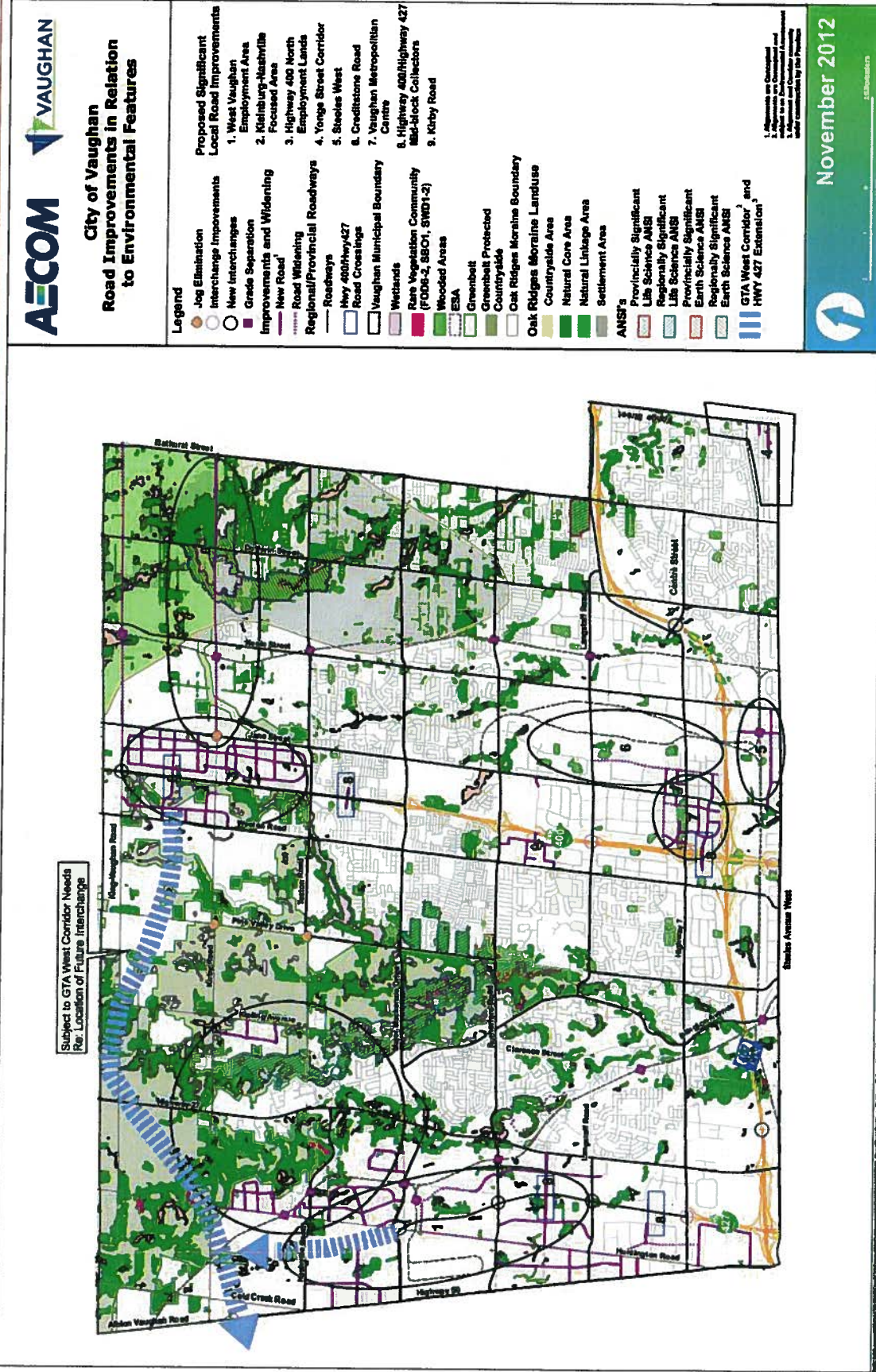
- Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area. This led to the need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.

**Potential Socio-economic and Environmental Impacts:** The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network. The extension of Portage Parkway east of Jane Street avoids a wooded area that would have been impacted with implementation of the former road network plan.

**Conclusion:** Recommended for Phase 3 and 4 EA study.



# Study Context – Natural Environment



# VMC Secondary Plan – Transportation Studies

## The VMC Secondary Plan and supporting focused Transportation Plan:

- ❑ Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- ❑ Encourages the use of Portage Parkway as a link to Highway 400
- ❑ Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street
- ❑ Acquires right-of-way for the extension of Portage Parkway to Creditstone Road.

The *VMC Transportation Plan* (2012) and the *VMC and Surrounding Areas Transportation Study* (2013) provide recommendations for transportation improvements to 2021 and 2031.

## Recommendations include:

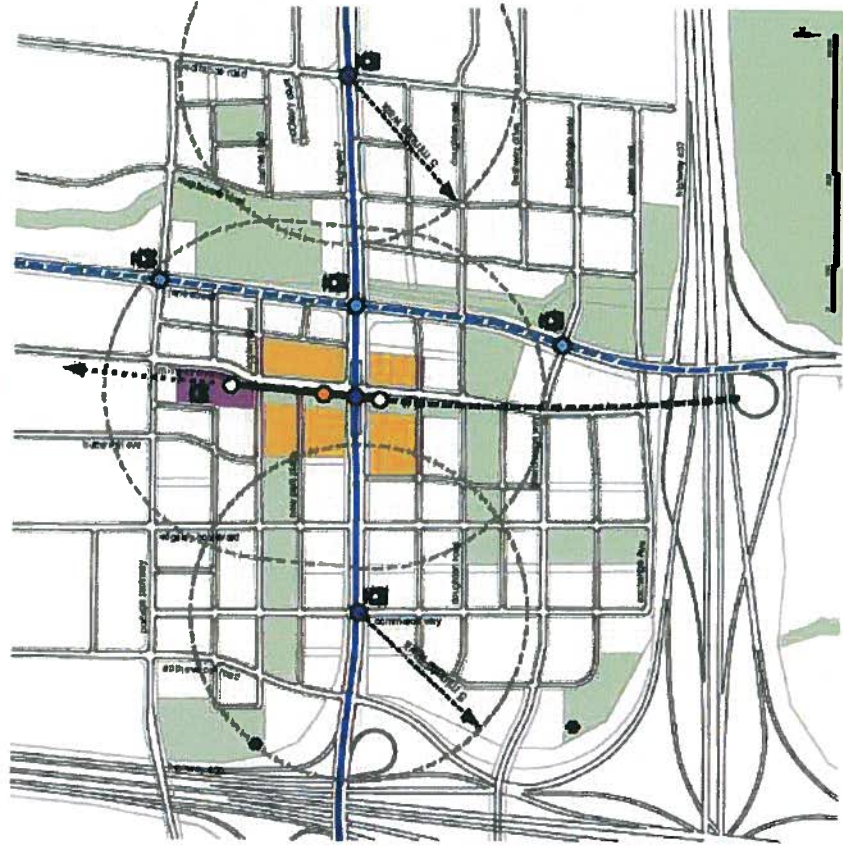
- ❑ Extending Portage Parkway to alleviate congestion on Highway 7 by 2021.



# VMC Secondary Plan - Transit and Street Network

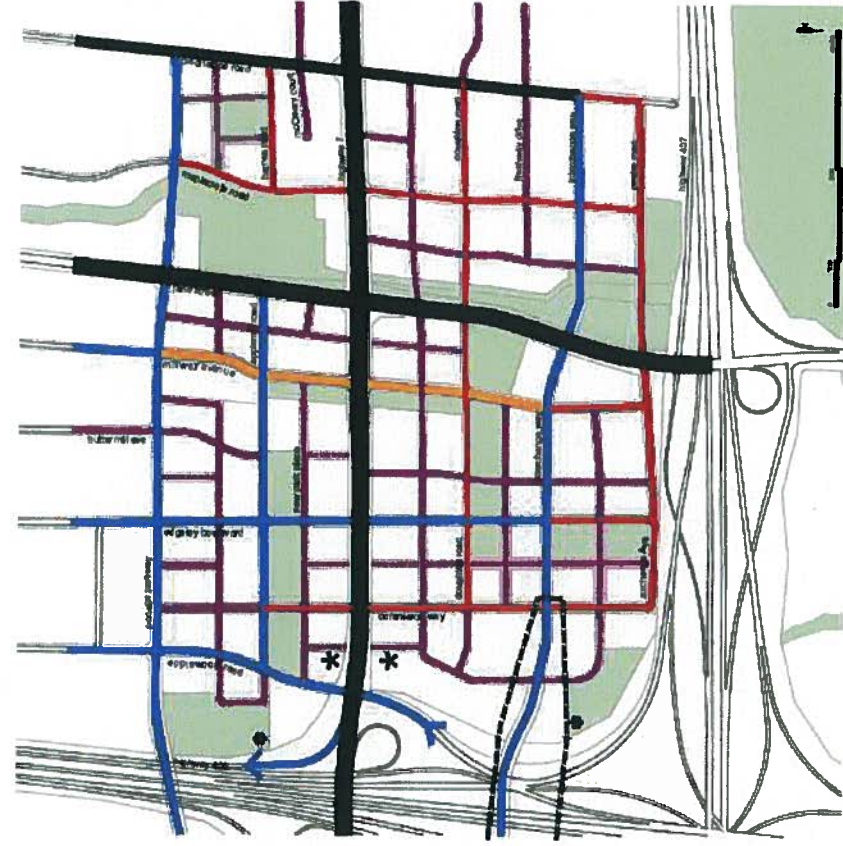
SCHEDULE B > TRANSIT NETWORK

- LEGEND**
- subway entrances
  - future subway entrances
  - potential Hwy7 rapidway stations
  - potential Jane Street rapidway stations
  - 5 minute walking radii
  - blocks adjacent to subway
  - on-street passenger pick-up and drop-off
  - see Policy 0.3.1
  - ▬ spadina subway alignment
  - ▬ future spadina subway extension
  - ▬ spadina subway station box
  - ▬ highway 7 rapidway
  - ▬ potential Jane Street rapidway
  - potential visa stations
  - bus station
  - major parks and open spaces



SCHEDULE C > STREET NETWORK

- LEGEND**
- ▬ arterials (width to be consistent with Region of York Official Plan)
  - ▬ minor arterial (33 m)
  - ▬ major collector (29-33 m)
  - ▬ special collector (33 m)
  - ▬ minor collector (23-26 m)
  - ▬ local streets (20-22 m)
  - ▬ mains or local streets (15-17 m) (see Policy 4.3.16)
  - ▬ Cobrasus Drive overpass corridor protection area (see Policy 4.3.11)
  - ▬ major parks and open spaces
  - see Policy 4.3.2
  - see Policy 4.3.17
  - see Policy 0.3.1



# VMC Secondary Plan

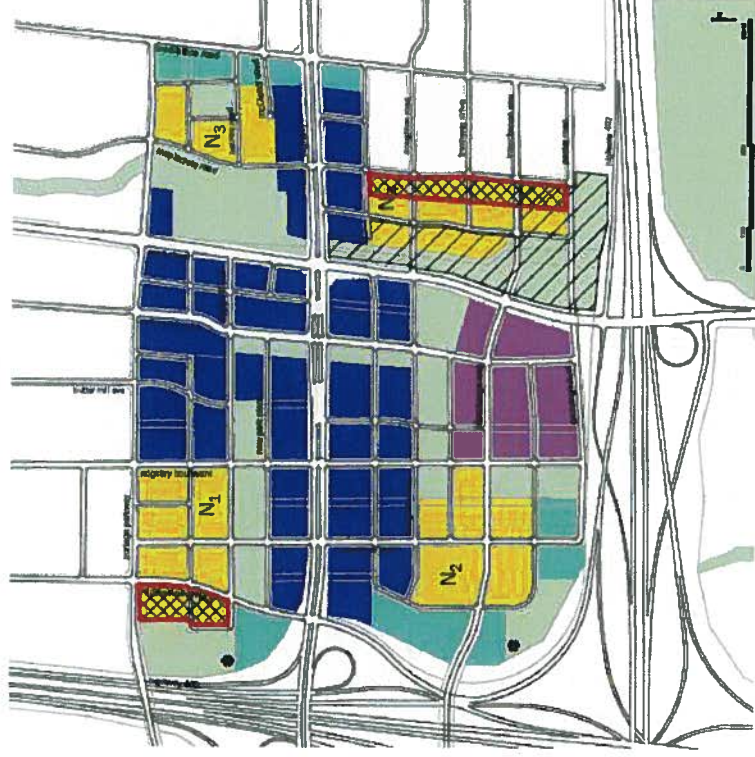
Portage Parkway Widening and Extension to Creditstone Road forms part of the planned street network addressing travel demand.

- + Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- + Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street and facilitating an alternate for trucks

SCHEDULE F - LAND USE PRECINCTS

**LEGEND**

- station precinct
- south precinct
- neighbourhood precincts
- west and east employment precincts
- major parks and open spaces
- land use designations and identification of areas are subject to results of the VMC Black Creek Renewal EA (Stages 3 & 4) and final results of the VMC Servicing and Stormwater Management Master Plan (see also Schedules C and J, and Policies 5.8.4 - 5.8.8, 6.2.7 and 8.4.3)
- office uses permitted (see Policy 8.4.4)
- see Policy 8.3.1



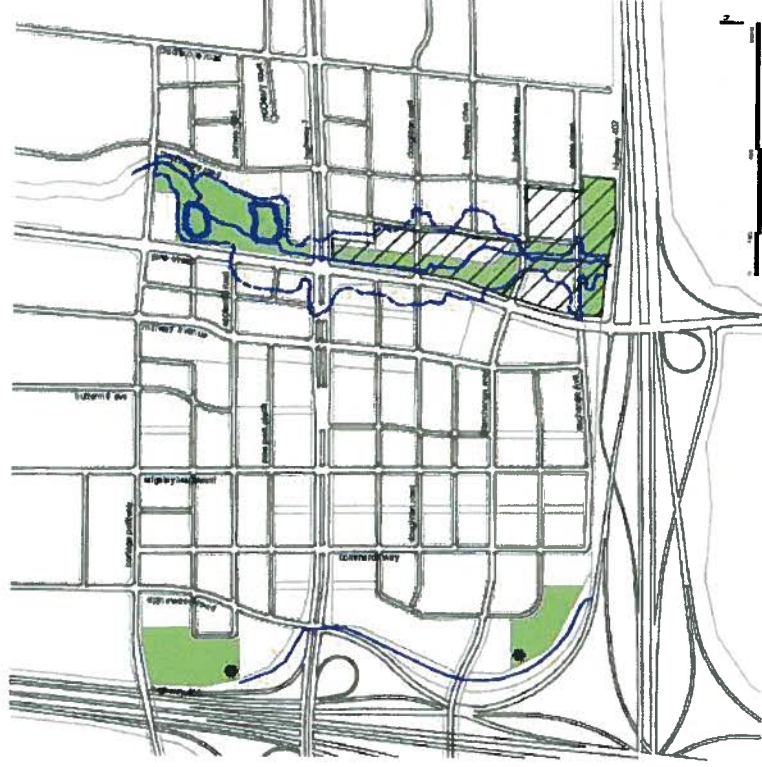


# VMC Secondary Plan

- Extension east of Jane Street to Creditstone Road will necessitate a structured crossing of the Black Creek channel.
- Black Creek Stormwater Optimization Study Master Plan Municipal Class EA Report (Phases 1 and 2) provides the broader context for advancing a co-ordinated design for the crossing of the Natural Heritage Network/System - a key structuring feature of the VMC Secondary Plan.

SCHEDULE J > FLOODPLAIN AND ENVIRONMENTAL OPEN SPACES

- LEGEND**
-  environmental open space
  -  existing floodplain (see Policies 5.0.4-5.0.8, 8.3.7 and 8.5.2)
  -  Black Creek Remediation Area (subject to Black Creek Renewal EA)
  -  see Policy 8.3.1
  -  existing watercourse (future alignment to be determined)



# Transportation Benefits

## **Widening Portage Parkway will:**

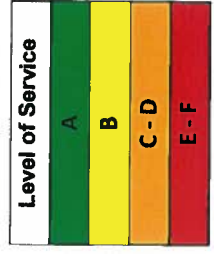
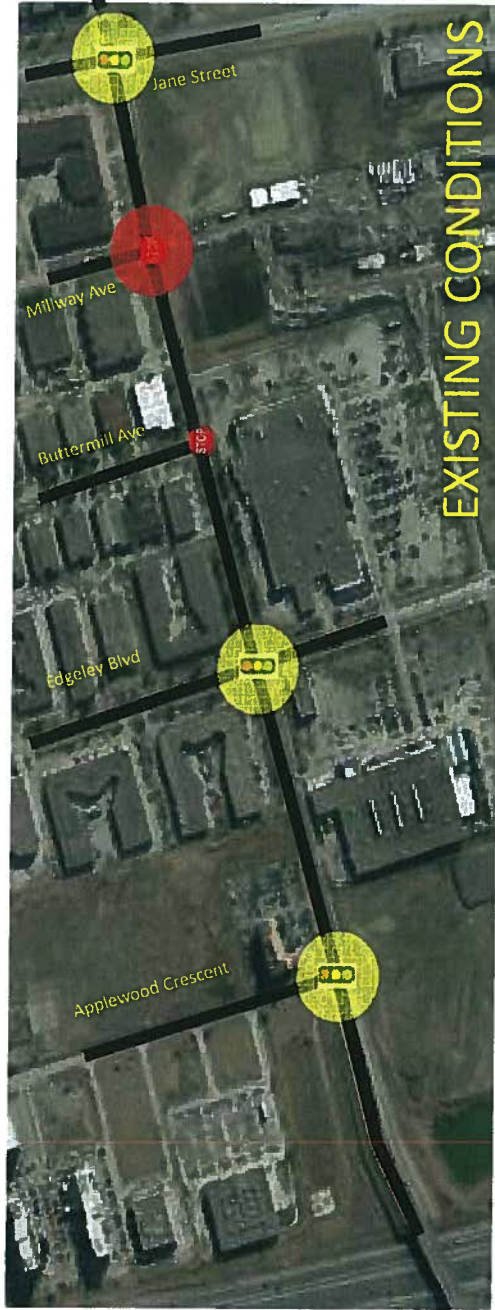
- Accommodate travel demand from future development
- Address capacity and operational issues at intersections
- Facilitate the addition of bicycle lanes as part of the widening

## **Extending Portage Parkway will:**

- Increase pedestrian access from Creditstone Road to future (2016) Jane Street vivaNext station
- Create an alternative route for trucks to bypass the VMC core
- Provide a continuous route for all travel modes from Weston Road to Creditstone Road



# Existing Traffic Conditions



\* Future conditions with VMC build-out (without widening of Portage Parkway)

# Problem and Opportunity Statement

*Transportation improvements are needed to support growth within the VMC and surrounding area.*

*Widening and extending Portage Parkway is an opportunity to:*

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,*
- Address travel demand in the VMC and surrounding area,*
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,*
- Improve connections to local and regional infrastructure,*
- Support transit-oriented nodes and corridors,*
- Enhance transit ridership, cycling and walking, and*
- Provide an alternate to Highway 7.*



# Existing Conditions - Portage Parkway, Applewood Crescent to Jane Street



**Study Area**

- 4 Lane Roadway
- 2 Lane Roadway
- Overpass
- Land Use - Employment/Industrial
- Vaughan Metropolitan Centre Secondary Plan Area
- Heritage Property
- Existing Sidewalk

**Notes:**

- Road Features: Posted speed limit is 60km/h
- Natural Sciences: Vegetation communities include old field cultural meadow and deciduous forest
- Stage 1 Archaeological Assessment: Most of the study area has low to no archaeological potential. Sections found to have archaeological potential require a Stage 2 assessment



**PART A and PART B areas are conceptual subject to EA Study**



# Existing Conditions – Portage Parkway Extension to Creditstone Road



PART A and PART B areas are conceptual subject to EA Study



# Alternative Planning Solutions in the Transportation Master Plan

## Problem or opportunity identified in the Transportation Master Plan

- “Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area.”

## Alternative solution considered by the Transportation Master Plan:

- “Need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.”

## Impacts assessed in the Transportation Master Plan:

- “The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network.”

# Alternative Planning Solutions for Portage Parkway

- Do Nothing**
- Travel Demand Management Initiatives** (e.g., flexible work hours, telecommuting, carpooling)
- Alternative Modes of Transportation** (e.g., cycling and walking facilities, transit)
- Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)

# Summary of Alternative Planning Solutions

Alternative	Advantages	Disadvantages	Summary
<b>Do Nothing</b>	<ul style="list-style-type: none"> <li>No natural environmental impacts</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>Does not conform to VMC Secondary Plan / Transportation Master Plan (TMP)</li> <li>No improvements to traffic safety</li> </ul>	<ul style="list-style-type: none"> <li>Does not address problem and opportunity statement</li> <li>Carried forward for comparative evaluation</li> </ul>
<b>Travel Demand Management Initiatives</b>	<ul style="list-style-type: none"> <li>Indirect improvements encourage alternative transportation</li> <li>Long term potential for increased traffic capacity</li> <li>Partially conforms to VMC Secondary Plan / TMP</li> <li>Long term potential for reduction in vehicular emissions</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>No improvements to traffic safety</li> </ul>	<ul style="list-style-type: none"> <li>Carried forward as part of recommended solution</li> </ul>
<b>Alternative Modes of Transportation</b>	<ul style="list-style-type: none"> <li>Direct and indirect improvements encourage alternative transportation</li> <li>Partial reduction of traffic volumes</li> <li>Partially conforms to VMC Secondary Plan / TMP / Pedestrian and Cycling Master Plan</li> <li>Potential for minor improvements to aesthetics and streetscape</li> <li>Low construction / implementation costs</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>No impact to traffic safety</li> <li>Low potential for archaeological impact</li> </ul>	<ul style="list-style-type: none"> <li>Carried forward as part of recommended solution</li> </ul>

# Summary of Alternative Planning Solutions

Alternative	Advantages	Disadvantages	Summary
<b>Localized Intersection and Operational Improvements</b>	<ul style="list-style-type: none"> <li>• Marginally improves level of service for traffic</li> <li>• Opportunity for gateway intersections per Streetscape and Open Space Plan</li> </ul>	<ul style="list-style-type: none"> <li>• Does not accommodate future traffic growth</li> <li>• Low construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>
<b>Widening Portage Parkway from Applewood Crescent to Jane Street</b>	<ul style="list-style-type: none"> <li>• Opportunity to encourage alternative transportation</li> <li>• Improves level of service for traffic</li> <li>• Conforms to VMC Secondary Plan / TMP</li> <li>• Opportunity to address traffic operations</li> <li>• Opportunity to improve aesthetics and streetscape per VMC Streetscape and Open Space Plan</li> <li>• Improves access to industry and businesses</li> <li>• Improves emergency access</li> </ul>	<ul style="list-style-type: none"> <li>• Increases surface area contributing to storm-water runoff</li> <li>• Low potential for archaeological impact</li> <li>• Property required</li> <li>• High construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>
<b>Extending Portage Parkway from Jane Street to Creditstone Road</b>	<ul style="list-style-type: none"> <li>• Opportunity to encourage alternative transportation</li> <li>• Conforms to VMC Secondary Plan / TMP</li> <li>• Provides alternative route for truck traffic</li> <li>• Improves access to industry and businesses</li> <li>• Improves emergency route</li> </ul>	<ul style="list-style-type: none"> <li>• Increases surface area contributing to storm-water runoff</li> <li>• Potential traffic noise impact</li> <li>• Potential environmental issues</li> <li>• Low potential for archaeological impact</li> <li>• Property required</li> <li>• High construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>



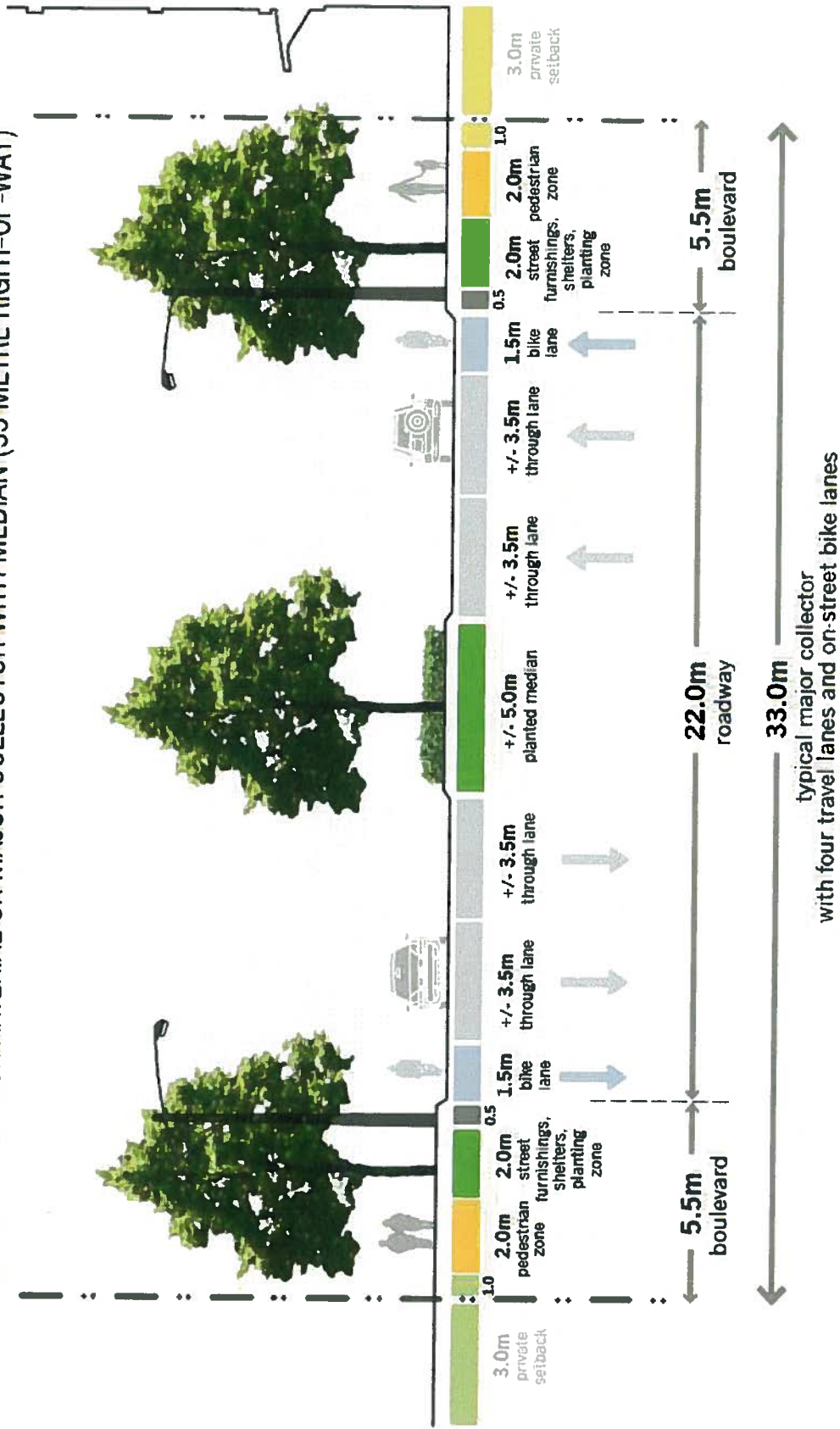
# Recommended Solution

The recommended solution for Portage Parkway is a combination of alternatives:

- Travel Demand Management Initiatives** were identified in the Transportation Master Plan and are being implemented by the City as a separate strategy
- Alternative Modes of Transportation** were identified in the Transportation Master Plan, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)

# Design - VMC Secondary Plan

FIGURE E > MINOR ARTERIAL OR MAJOR COLLECTOR WITH MEDIAN (33 METRE RIGHT-OF-WAY)



typical major collector  
with four travel lanes and on-street bike lanes

The VMC Plan – Secondary Plan – Draft Consolidation Proposed Modifications WITHOUT PREJUDICE, Urban Strategies Inc., September 2015

# Design Framework Typical Streetscape Elements for Mid-Block

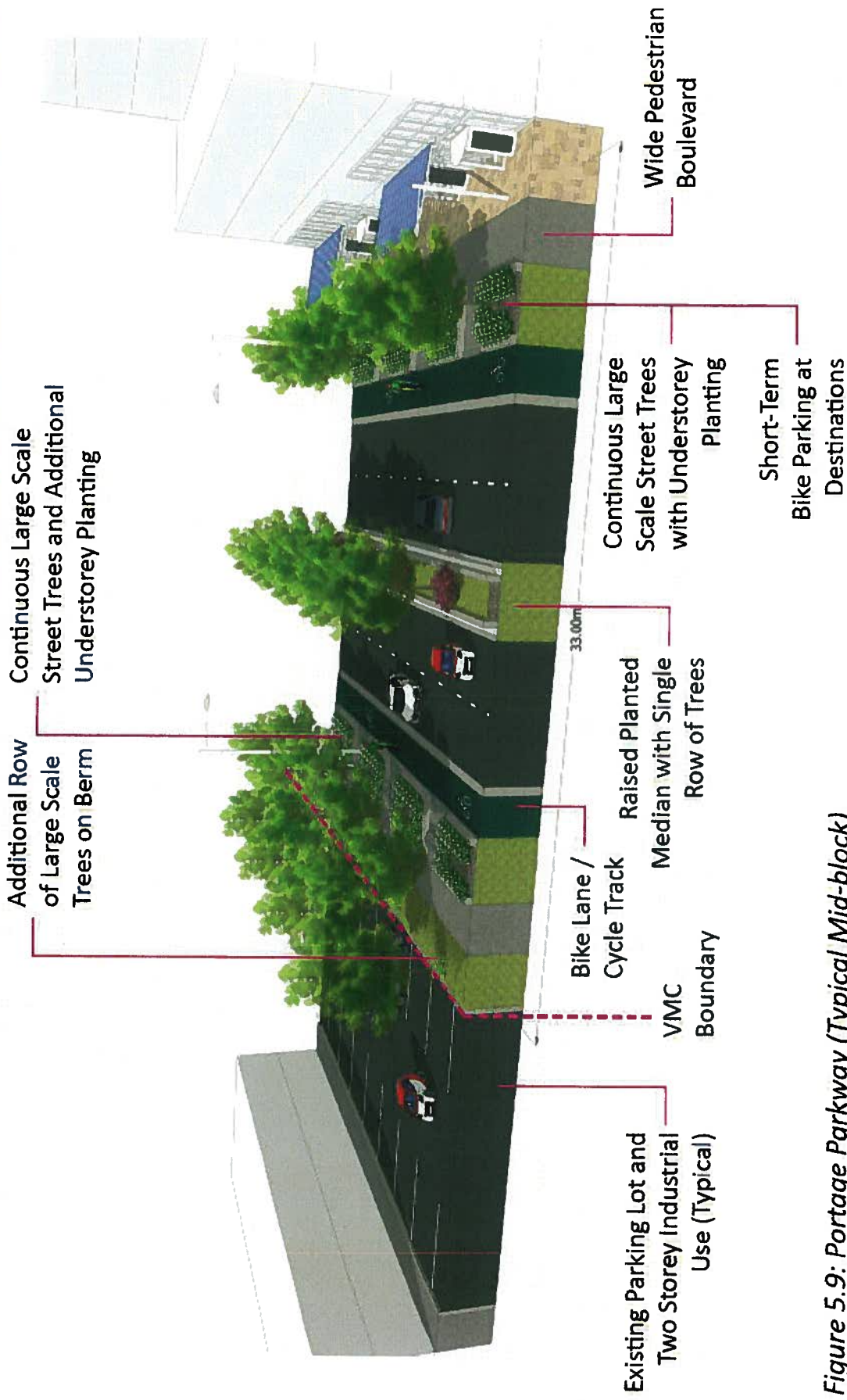


Figure 5.9: Portage Parkway (Typical Mid-block)

Vaughan Metropolitan Centre Streetscape and Open Space Plan, EDA Collaborative Inc., 2015

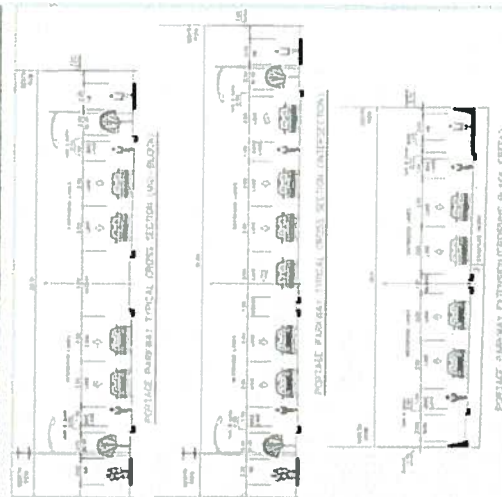
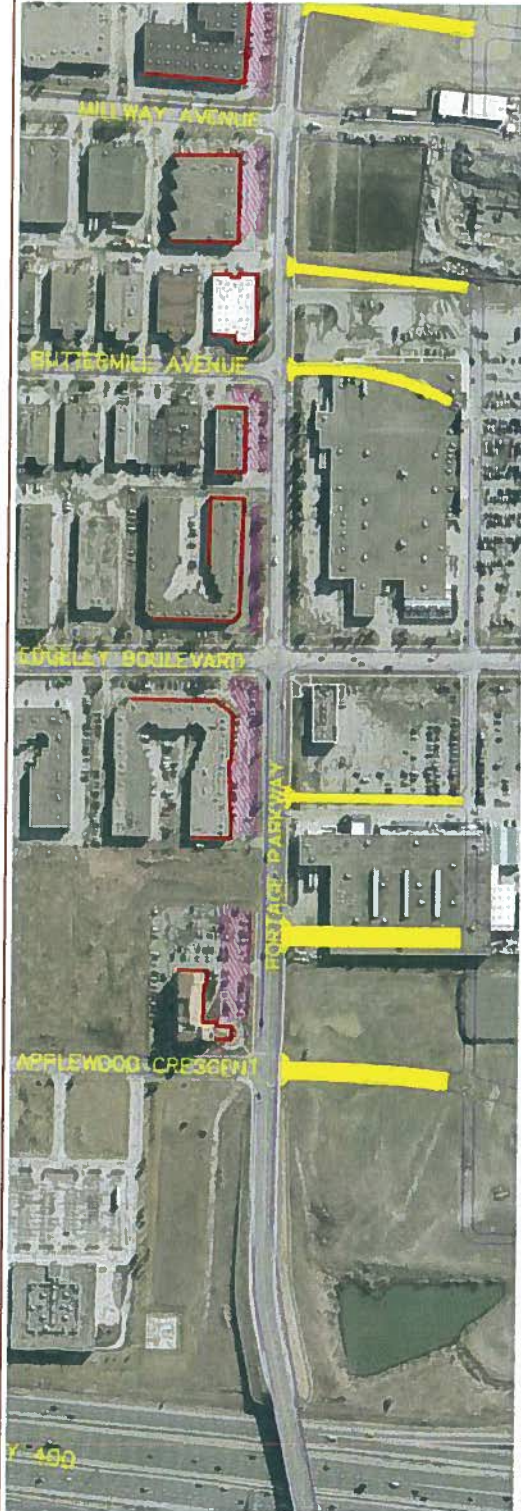
# Design Evaluation Criteria

Criteria considered for high level assessment and comparison of alternative planning solutions:

- Cultural**
  - Archaeology
  - Built Heritage and Cultural Landscapes
- Economic**
  - Capital Costs
- Implementation**
  - Regulatory Framework
  - Staging
  - Municipal Servicing Coordination
- Infrastructure Planning**
  - Alternative Transportation
  - Level of Service
  - Network Capacity
  - Planning Policy
- (Official/Secondary Plans, Transportation Master Plans)**
  - Stormwater Management / Creek Crossing
  - Traffic Safety
- Natural**
  - Aquatic
  - Avian and Wildlife
  - Natural Areas
  - Species at Risk
  - Vegetation
  - Watercourses
- Social**
  - Aesthetics and Streetscape
  - Air Quality
  - Businesses
  - Emergency Services
  - Land Use
  - Noise
  - Property Access



# Design Opportunities and Constraints



- Existing Parking
- Proposed Development
- Black Creek Channel
- Proposed Street
- Existing Building
- CH Rail Spur

Portage Parkway Class EA  
Design Constraints





# Design Opportunities and Constraints



VAC STREETScape AND OPEN SPACE PLAN (CMA) FOR CONSIDERATION OF  
 - MOBILITY AND GATEWAY INTERSECTION DESIGN AT PORTAGE PARKWAY AND MILLWAY  
 - PROPOSED AND MAINTENANCE DESIGN AT PORTAGE PARKWAY AND BUTTERMILL AVENUE  
 - PROPOSED AND MAINTENANCE DESIGN AT BUTTERMILL AVENUE AND EDGELEY BOULEVARD  
 - PROPOSED AND MAINTENANCE DESIGN AT BUTTERMILL AVENUE AND MILLWAY AVENUE  
 - PROPOSED AND MAINTENANCE DESIGN AT BUTTERMILL AVENUE AND MILLWAY AVENUE  
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 - PROPOSED AND MAINTENANCE DESIGN AT BUTTERMILL AVENUE AND MILLWAY AVENUE



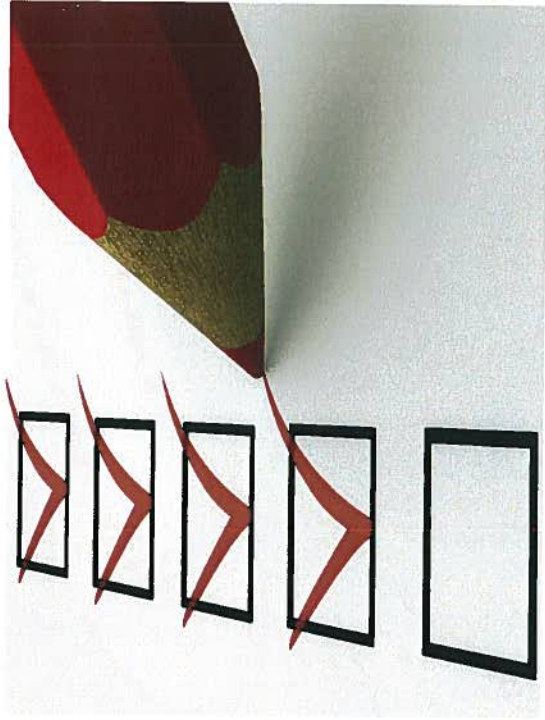
Proposed Development  
 Black Creek Channel  
 Proposed Road  
 CN Rail Spur Line

Portage Parkway Class EA  
 Design Considerations

CMA



# Next Steps



- Review your input
- Finalize Problem and Opportunity Statement
- Finalize Preferred Planning Solution
- Identify and Assess Alternative Design Concepts
- Host meetings with Technical Agencies and Stakeholders
- Host Public Information Centre No. 2 in 2016

# What do you think?

Please share your comments with either Project Manager by  
December 11, 2015:

**Marta Roias, RPP**

City of Vaughan Project Manager

2141 Major Mackenzie Drive

Vaughan, ON L6A 1T1

T: 905-832-8585 Ext. 8026

F: 905-832-6145

E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

**Stephen Keen, P.Eng.**

CIMA+ Project Manager

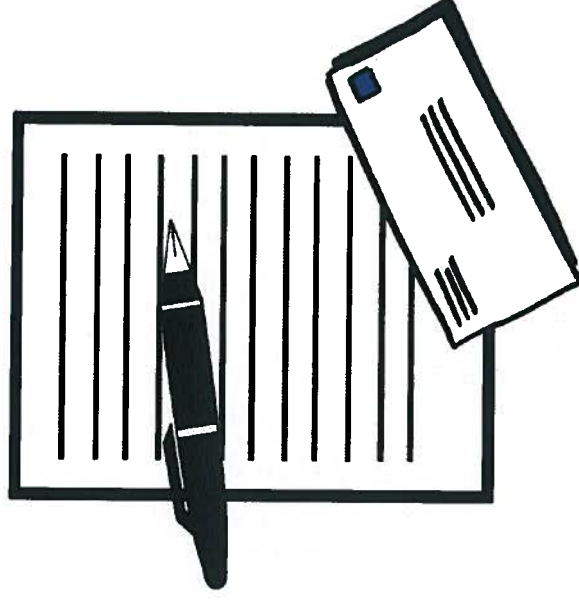
3027 Harvester Road, Suite 400

Burlington, ON L7N 3G7

T: 289-288-0287 Ext. 6834

F: 289-288-0285

E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)



Thank you for attending this PIC



## Next Steps for Stakeholders Group

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- Forward comments on Draft Public Information Centre Display by **November 3, 2015**
- Attend Public Information Centre No. 1 on **November 25, 2015**
- Forward comments in response to Public Information Centre by **December 11, 2015**
- Meet with Project Team in advance of second Public Information Centre by **Spring 2016**

## MINUTES OF MEETING

- CLIENT** : City of Vaughan (City)
- PROJECT** : Environmental Assessment (EA) Study for  
Part A - Portage Parkway Widening, Applewood Crescent to Jane Street  
Part B - Portage Parkway Extension, Jane Street to Creditstone Road
- MEETING #1** : Meeting with Technical Agencies Committee (TAC)
- DATE OF MEETING** : October 20, 2015, 10:00 am
- LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive, 2<sup>nd</sup> Floor
- TAC ATTENDEES** : Powerstream (Tony D'Onofrio)  
York Catholic District School Board (Christine Hyde)  
York Region (Shahid Matloob)  
York Region (Vi Bui)
- City of Vaughan (Amy Roots, Andy Lee, Brianne Clace, Gerardo Paez Alonso, Jennifer Cappola-Logullo, Steven Dixon, Vince Suppa)
- PROJECT TEAM** : City of Vaughan (Marta Roias)  
CIMA Canada Inc. (CIMA+; Stephen Keen, Sonya Kapusin)
- C.C. TO** : Metrolinx (Briana Ingram)  
Ministry of Environment and Climate Change (Nisha Shirali)  
Powerstream (Brian O'Gay, Jiacheng Zhang, Joe Crozier, Patrick Leung)  
Toronto and Region Conservation (Suzanne Bevan)  
Toronto Transit Corporation (TTC) (Ronan Olwill)  
Toronto-York Spadina Subway Extension (Adam Light)  
York Region (Brian Titherington, Richard Hui)  
York Region Rapid Transit Corporation (Carolyn Ryall)
- City of Vaughan (Andrew Pearce, Daniel Rende, Danny Woo, Dino Macchiusi, Frank Facchini, Niaz Muhammad, Paul Salerno, Saad Yousaf, Selma Hubjer, Stephen Lue, Tim Simmonds)

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**Note:** If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

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**DISCUSSION TOPICS****ACTION  
BY**

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**1 STATUS OF CLASS EA STUDY**

A copy of the presentation for this meeting is attached. Refer to presentation slide 2 for the meeting agenda, and presentation slide 5 for the Class EA status.

The Study was at Phase 2 (Alternative Solutions) with plans for a Public Information Centre (PIC) in November to present the problem/opportunity statement and evaluation of alternative planning solutions to the public.

**2 DRAFT PUBLIC INFORMATION CENTRE DISPLAY**

Refer to presentation slides 3 to 31 for the draft PIC display. {Post meeting note: CIMA+ forwarded an electronic copy of the display to TAC on October 23, 2015}. All present were asked to review and provide comments on the draft display by **November 3, 2015**.

TAC

**3 BACKGROUND**

Refer to presentation slides 7 to 16 for the transportation planning context and relevant information from the City's Transportation Master Plan (TMP).

**4 PROBLEM AND OPPORTUNITY STATEMENT**

Refer to presentation slide 17 for the problem and opportunity statement.

**5 PREFERRED PLANNING SOLUTION**

Refer to presentation slides 20 to 24 for a summary of alternative planning solutions. The recommended solution is a combination of alternatives:

- **Travel Demand Management Initiatives** were identified in the TMP and are being implemented by the City as a separate strategy
- **Alternative Modes of Transportation** were identified in the TMP, including continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- **Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- **Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- **Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)

**6 EXISTING CONDITIONS**

Refer to presentation slides 18 and 19 for an overview of existing conditions within the study area. The following investigations have been initiated:

- Stage 1 Archaeological Assessment
- Natural Sciences

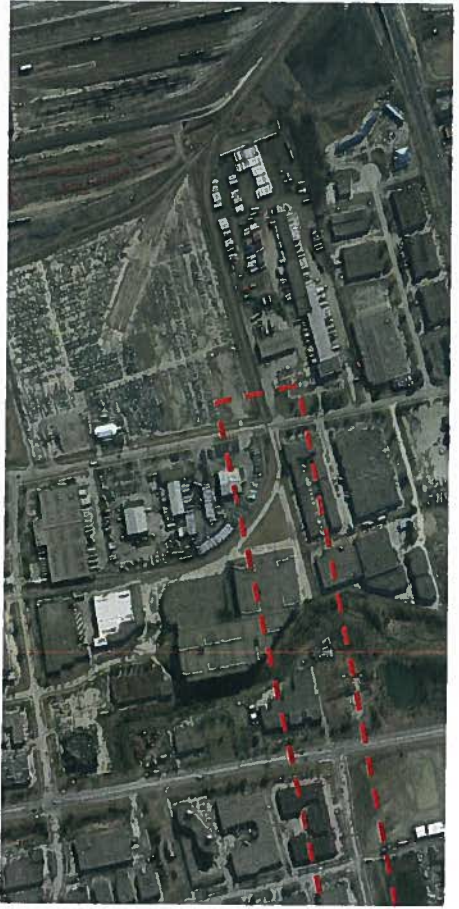
**7 ALTERNATIVE DESIGN CONCEPTS FOR PREFERRED SOLUTION**

Refer to presentation slides 25 to 27 for the design framework and evaluation criteria, and 28 to 29 for design opportunities and constraints. This information will guide the assessment of design alternatives for the preferred solution.

**8 NEXT STEPS**

- Forward comments on Draft PIC Display by **November 3, 2015**
- Attend PIC No. 1 on **November 25, 2015**
- Forward comments in response to PIC by **December 11, 2015**
- Meet with Project Team in advance of second PIC in **Spring 2016**

TAC



City of Vaughan

# Portage Parkway Environmental Assessment

Technical Agencies Committee  
Meeting No. 2

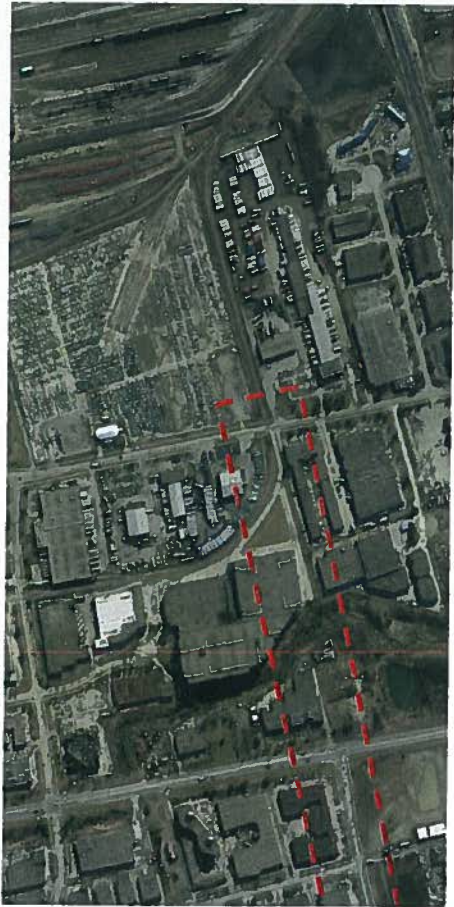
October 20, 2015





# Agenda

- Status of Class EA Study**
- Draft Public Information Centre Display**
- Background**
  - Transportation Planning Context
  - Transportation Master Plan
- Problem and Opportunity Statement**
- Preferred Planning Solution**
- Existing Conditions**
  - Draft Technical Studies
  - Transportation and Traffic
  - Natural Environment
- Alternative Design Concepts for Preferred Solution**
  - Preliminary Design and Alignment Alternatives
  - Draft Evaluation Criteria
- Next Steps**
  - Comments on Draft Public Information Centre Display by **November 3, 2015**
  - Public Information Centre No. 1 planned for **November 25, 2015**



City of Vaughan

# Portage Parkway Environmental Assessment Public Information Centre No. 1

November 25, 2015



# Welcome to the Public Information Centre

This first Public Information Centre (PIC) introduces the Environmental Assessment (EA) Study for Portage Parkway Widening and Easterly Extension to Creditstone Road.

We welcome your input on:

- Background to the study
- Existing conditions of the study area
- Transportation problems or opportunities and solutions
- Design concepts for the preferred solutions
- Potential effects of the project and ways to avoid or minimize impacts

You are invited to:

- Review the information boards
- Ask questions and discuss your comments with the study team
- Fill out and submit a comment sheet
- Sign in to receive future updates on the study
- Visit the project website:

[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

# Class EA Process

- This project is subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (2000 as amended in 2007 and 2011).
- The Municipal Class EA is a planning and design process approved by the Ministry of Environment and Climate Change to meet the requirements of the *Environmental Assessment Act*.
- This study follows the Class EA process for **Schedule C** projects.



- The City's Transportation Master Plan, having satisfied Phases 1 and 2, initiated the planning and design process for the Portage Parkway Widening and Easterly Extension to Creditstone Road.
- This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
  - Part A - Portage Parkway Widening from Applewood Crescent to Jane Street
  - Part B - Portage Parkway Extension from Jane Street to Creditstone Road.

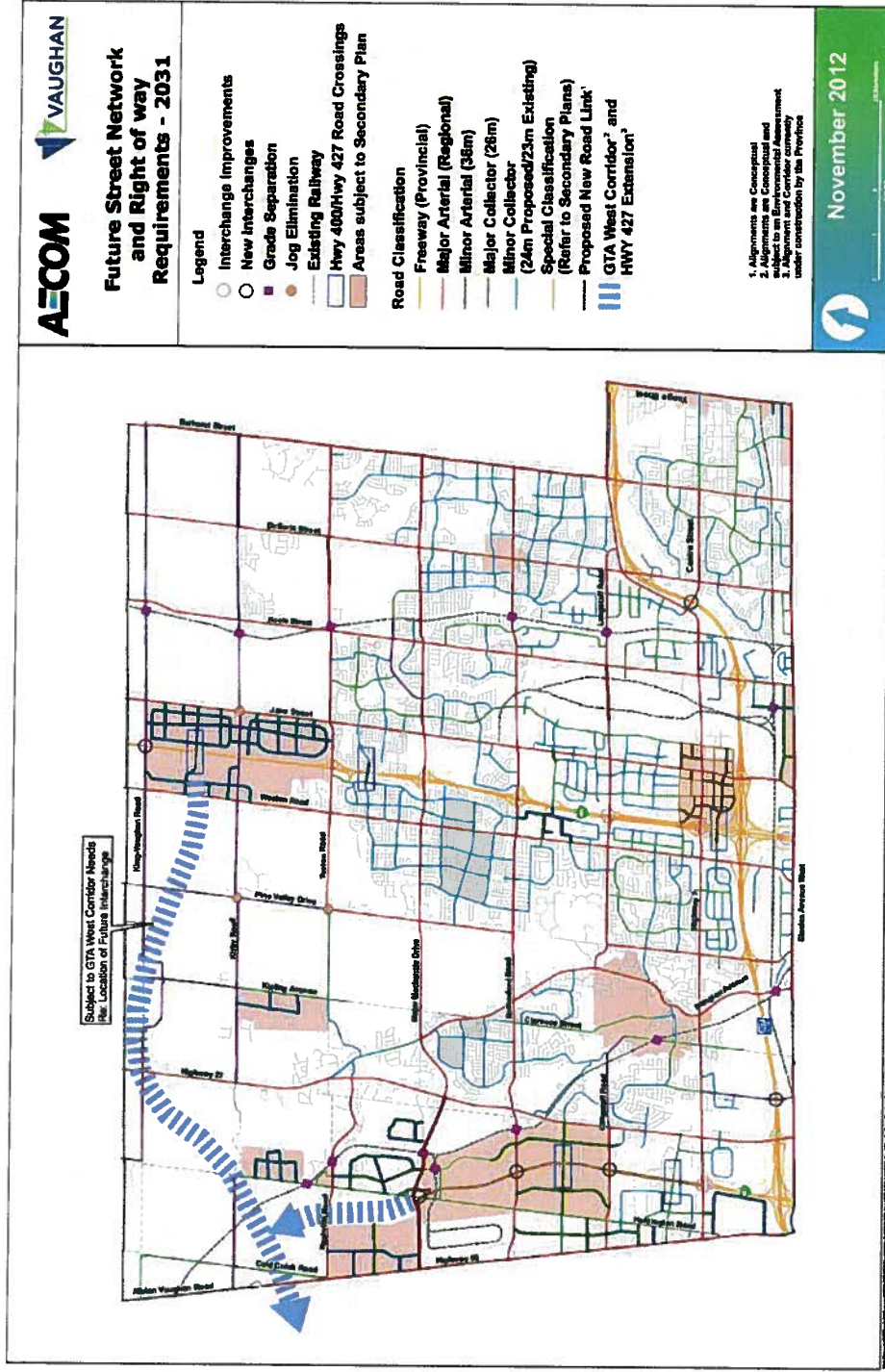




# Transportation Master Plans

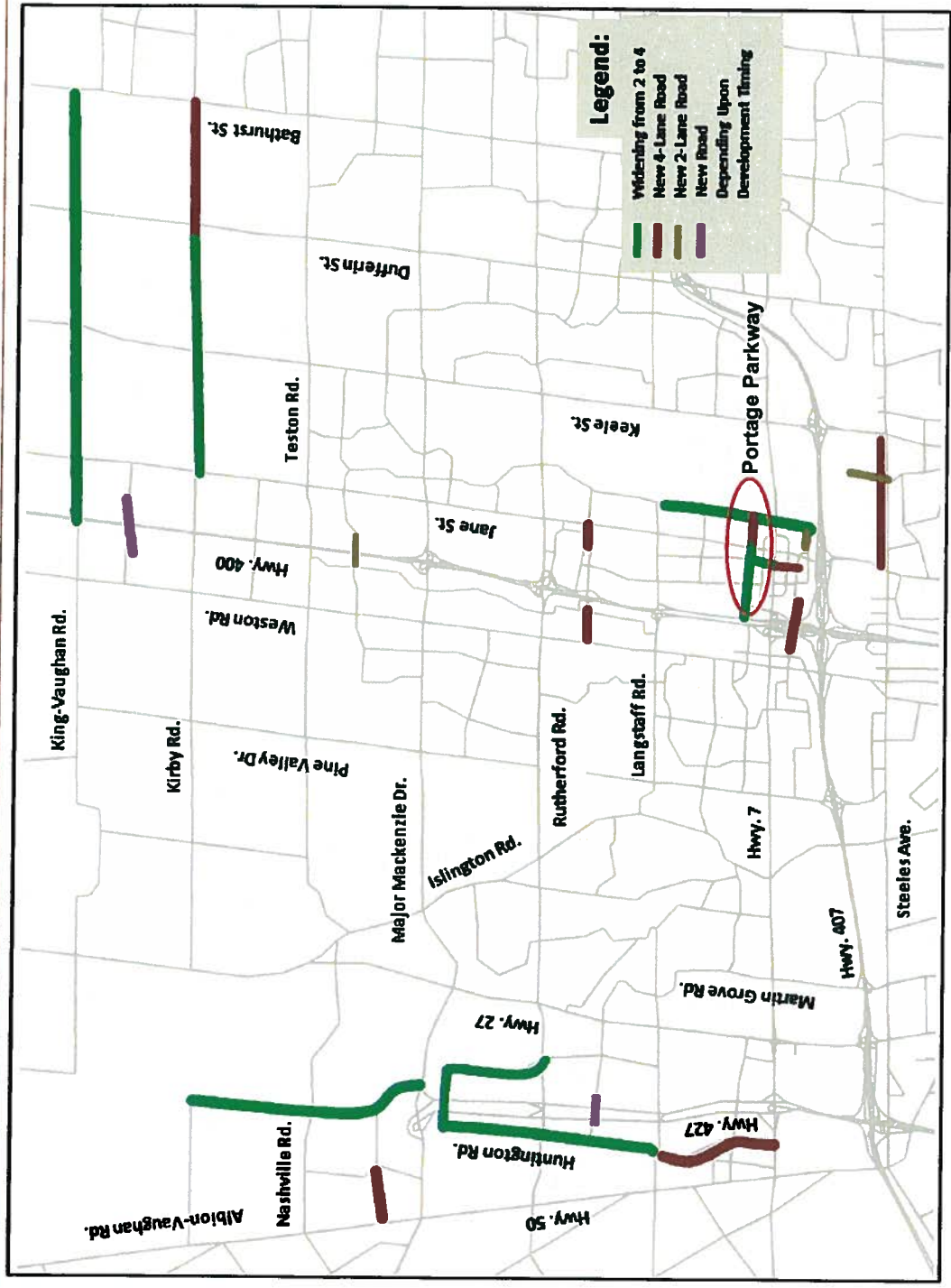
The City of Vaughan  
**Transportation Master Plan – A New Path** (2012) established the need and justification for the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element, supporting the planned level of intensification and building of the City's downtown - Vaughan Metropolitan Centre (VMC).

The City of Vaughan **Pedestrian and Bicycle Master Plan** (2007) identified Portage Parkway for "Neighbourhood Bike Lanes" with pavement markings and signs.



Transportation Master Plan – A New Path, City of Vaughan, 2012  
 Pedestrian and Bicycle Master Plan Study, Marshall Macklin Monaghan with ESG International, 2007

# Recommended 2031 Road Improvements



**Road Improvements Needed by 2031  
(Assuming 15% Transit Share)**



# Transportation Master Plan

This study advances the planning and design process under the Municipal Class EA process for:

- Widening Portage Parkway to four lanes from Applewood Crescent to Jane Street, and
- Extending Portage Parkway with four lanes from Jane Street to Creditstone Road.


## Portage Parkway Widening and Easterly Extension

**Purpose:** With the recent completion of the Highway 400 crossing, an improved Portage Parkway is a key element of the improved VMC grid road network, supporting intensification and transit-oriented development in Vaughan's pre-eminent centre and future Downtown. The easterly extension to Creditstone Road will provide a direct link from the industrial area east of the VMC to the relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

**Project Description**

<b>Name</b>	Portage Parkway Widening and Easterly Extension
<b>Project Limits</b>	Between Highway 400 and Creditstone Road (see map area 7)
<b>Length</b>	Approximately 1.5 km (see map area 7)
<b>Project Type</b>	Widening to 4 lanes from Applewood Crescent to Jane Street, and new 4 lane road from Jane Street to Creditstone Road
<b>Proposed Phasing</b>	2012 – 2021
<b>Cost</b>	\$4 M (excluding property)

**Project Location and Environmental Features**



**Justification:**

- These are key road network improvements that will support the proposed development of the VMC. The proposed road network was developed in concert with the planning team that created the new secondary plan, and is supported by corridor deficiency analysis that identified the number of required lanes. An improved Portage Parkway will also facilitate the diversion of trucks around the VMC. In addition, the project strongly supports transit, cycling and pedestrian movement.

**Alternatives Considered**

**Transit**

- The Spadina subway extension, with its terminal station within the VMC, together with the planned Highway 7 Rapidway, provided the transit focus for the development of the new VMC transportation plan.

**Roads**

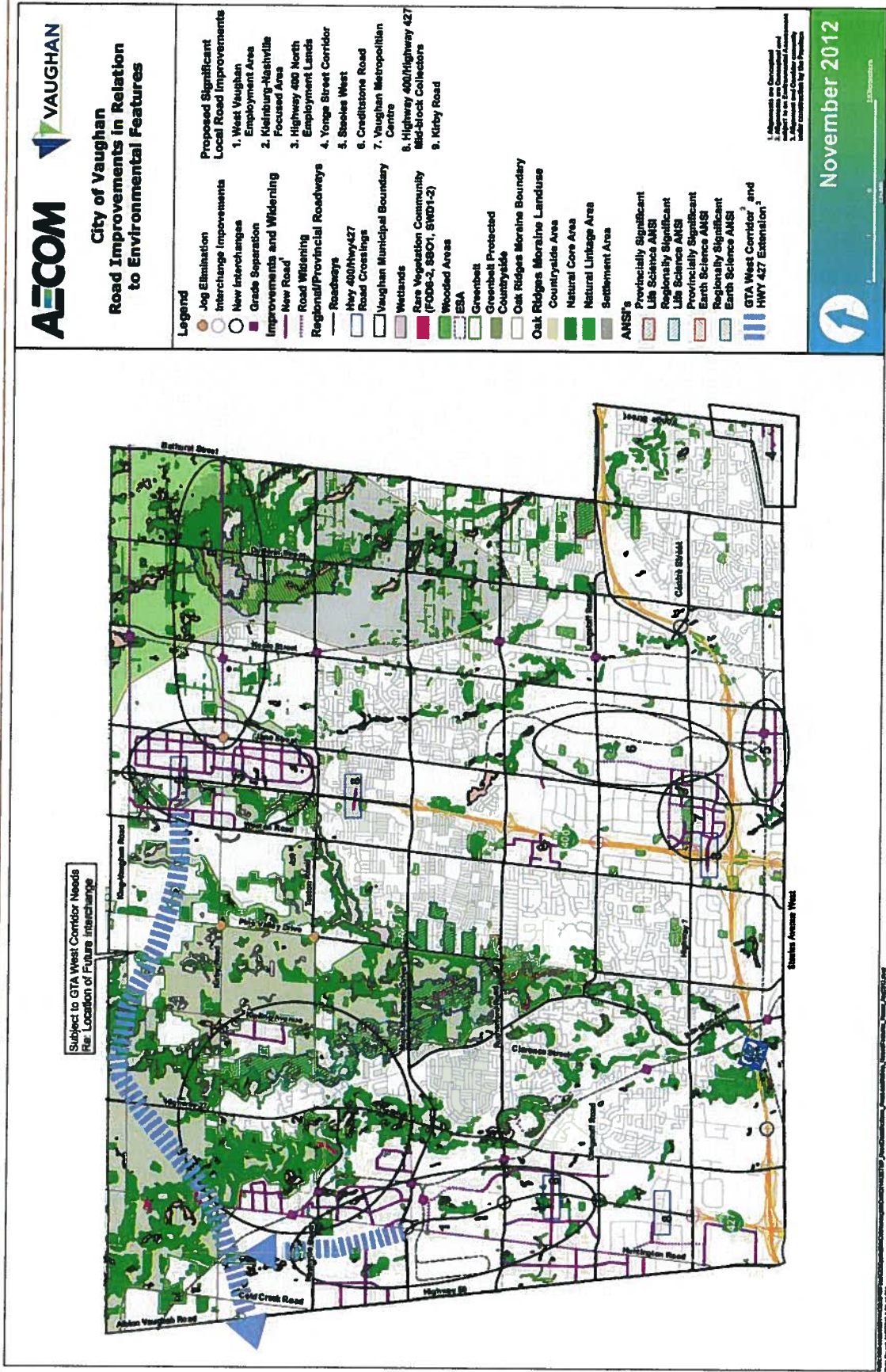
- Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area. This led to the need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.

**Potential Socio-economic and Environmental Impacts:** The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network... The extension of Portage Parkway east of Jane Street avoids a wooded area that would have been impacted with implementation of the former road network plan.

**Conclusion:** Recommended for Phase 3 and 4 EA study.



# Study Context – Natural Environment



## City of Vaughan Road Improvements in Relation to Environmental Features

- Legend**
- Jog Elimination**
  - Interchange Improvements**
  - New Interchanges**
  - Grade Separation**
  - Improvements and Widening**
  - New Road<sup>1</sup>**
  - Regional/Provincial Roadways**
  - Roadways**
  - Highway 400/Highway 427 Road Crossings<sup>2</sup>**
  - Vaughan Municipal Boundary**
  - Wetlands**
  - Rare Vegetation Community (POB-2, SBO1, SBO1-2)**
  - Wooded Area**
  - ESA**
  - Greenbelt**
  - Greenbelt Protected**
  - Countryside**
  - Oak Ridges Moraine Boundary**
  - Countryside Area**
  - Natural Core Area**
  - Natural Linkage Area**
  - Settlement Area**
  - ANSI's**
  - Provincially Significant**
  - Life Science ANSI**
  - Regionally Significant**
  - Life Science ANSI**
  - Provincially Significant**
  - Earth Science ANSI**
  - Regionally Significant**
  - Earth Science ANSI**
  - GTA West Corridor<sup>3</sup> and HWY 427 Extension<sup>3</sup>**
- Proposed Significant Local Road Improvements**
1. West Vaughan Employment Area
  2. Kleinburg-Raithville Focused Area
  3. Highway 400 North Employment Lands
  4. Yonge Street Corridor
  5. Steeles West
  6. Creditstone Road
  7. Vaughan Metropolitan Centre
  8. Highway 400/Highway 427 180-Block Collectors
  9. Kirby Road
1. Information on Environmental Features is an Overview and does not constitute a detailed assessment of the features. Further investigations by the Province are required.

**November 2012**

123456789

# VMC Secondary Plan – Transportation Studies

---

The **VMC Secondary Plan** and supporting focused Transportation Plan:

- ❑ Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- ❑ Encourages the use of Portage Parkway as a link to Highway 400
- ❑ Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street
- ❑ Acquires right-of-way for the extension of Portage Parkway to Creditstone Road.

The **VMC Transportation Plan** (2012) and the **VMC and Surrounding Areas Transportation Study** (2013) provide recommendations for transportation improvements to 2021 and 2031.

Recommendations include:

- ❑ Extending Portage Parkway to alleviate congestion on Highway 7 by 2021.

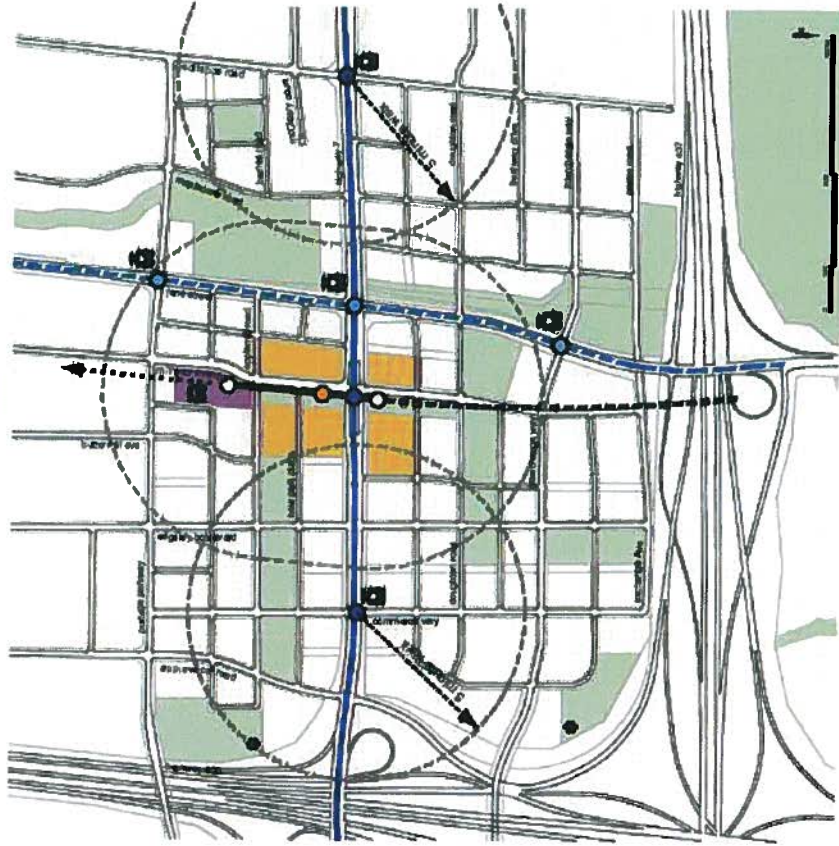


# VMC Secondary Plan - Transit and Street Network

SCHEDULE B > TRANSIT NETWORK

- LEGEND**
- subway entrances
  - future subway entrances
  - potential Hwy 7 rapidway stations
  - potential Jane Street rapidway stations
  - 5 minute walking radii
  - blocks adjacent to subway
  - on-street passenger pick-up and drop-off
  - see Policy 0.3.1

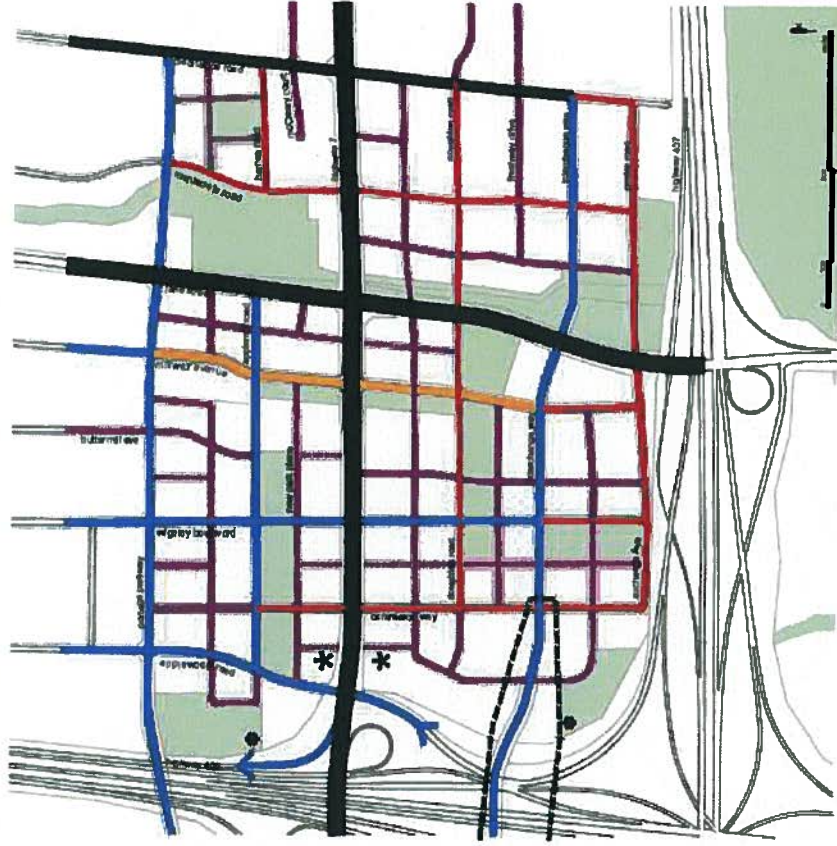
- spadina subway alignment
- future spadina subway extension
- spadina subway station box
- highway 7 rapidway
- potential Jane Street rapidway
- potential visa stations
- bus station
- major parks and open spaces



SCHEDULE C > STREET NETWORK

- LEGEND**
- arterials (width to be consistent with Region of York Official Plan)
  - minor arterial (33 m)
  - major collectors (29-33 m)
  - special collector (33 m)
  - minor collectors (23-28 m)
  - local streets (20-22 m)

- mains or local streets (15-17 m) (see Policy 4.3.10)
- Cobasas Drive overpass corridor protection area (see Policy 4.3.11)
- major parks and open spaces
- see Policy 4.3.2
- see Policy 4.3.17
- see Policy 8.3.1



# VMC Secondary Plan



Portage Parkway Widening and Extension to Creditstone Road forms part of the planned street network addressing travel demand.

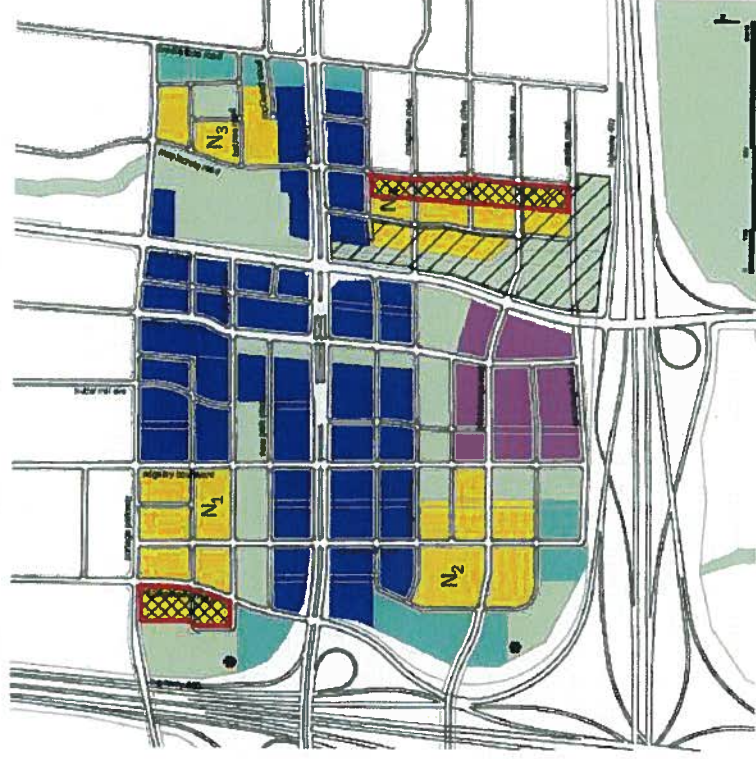
- + Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- + Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street and facilitating an alternate for trucks

## SCHEDULE F - LAND USE PRECINCTS

### LEGEND

-  station precinct
-  south precinct
-  neighbourhood precincts
-  west and east employment precincts
-  major parks and open spaces
-  land use designations and identification of areas are subject to results of the VMC Black Creek Renewal EA (Stages 3 & 4) and final results of the VMC Sewerage and Stormwater Management Master Plan (see also Schedules C and J, and Policies 5.8.4 - 5.8.8, 8.2.7 and 8.4.3)

-  office uses permitted (see Policy 6.4.4)
-  see Policy 6.3.1





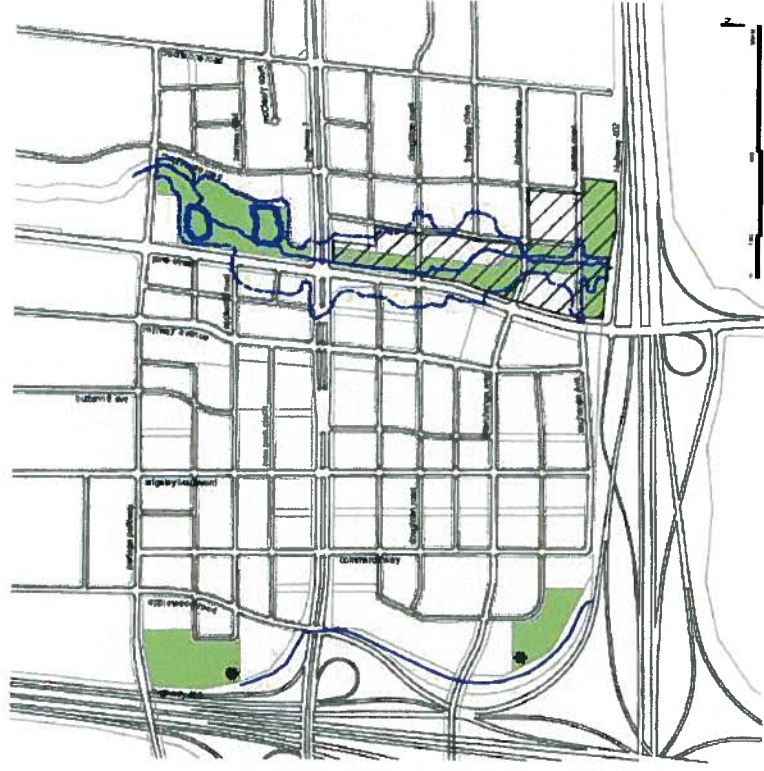
# VMC Secondary Plan

Extension east of Jane Street to Creditstone Road will necessitate a structured crossing of the Black Creek channel.

Black Creek Stormwater Optimization Study Master Plan Municipal Class EA Report (Phases 1 and 2) provides the broader context for advancing a co-ordinated design for the crossing of the Natural Heritage Network/System - a key structuring feature of the VMC Secondary Plan.

SCHEDULE J - FLOODPLAIN AND ENVIRONMENTAL OPEN SPACES

- LEGEND**
-  environmental open space
  -  existing floodplain (see Policies 8.6.4-8.8.8, 8.3.7 and 8.5.2)
  -  Black Creek Remediation Area (subject to Black Creek Renewal EA)
  -  see Policy 8.3.1
  -  existing watercourse (future alignment to be determined)



# Transportation Benefits

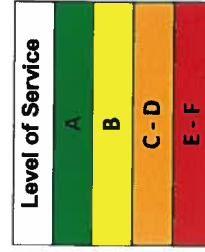
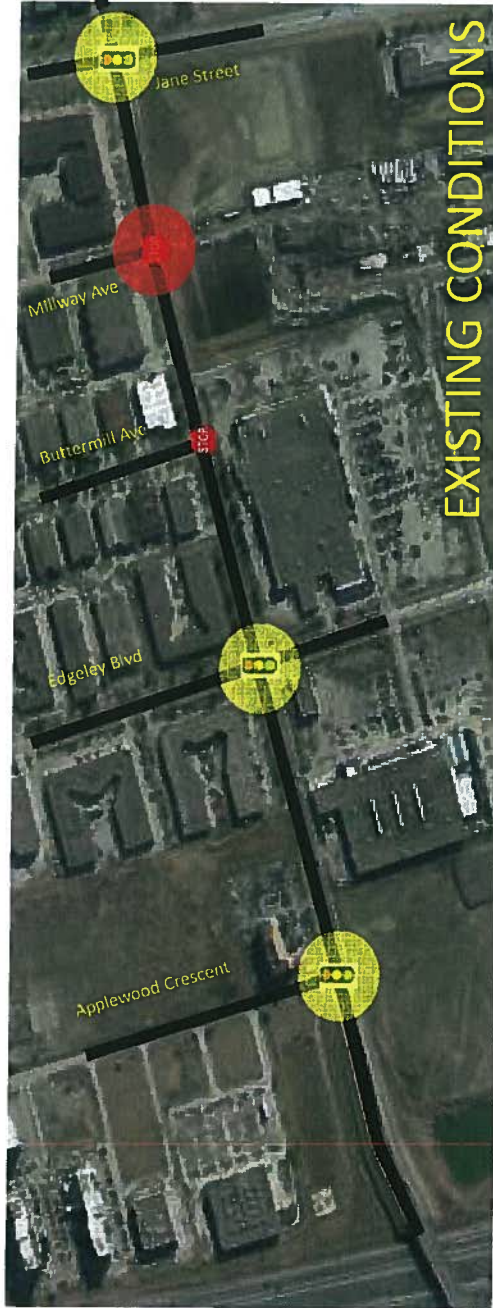
## **Widening Portage Parkway will:**

- Accommodate travel demand from future development
- Address capacity and operational issues at intersections
- Facilitate the addition of bicycle lanes as part of the widening

## **Extending Portage Parkway will:**

- Increase pedestrian access from Creditstone Road to future (2016) Jane Street vivaNext station
- Create an alternative route for trucks to bypass the VMC core
- Provide a continuous route for all travel modes from Weston Road to Creditstone Road

# Existing Traffic Conditions



\* Future conditions with VMC build-out (without widening of Portage Parkway)

# Problem and Opportunity Statement

*Transportation improvements are needed to support growth within the VMC and surrounding area.*

*Widening and extending Portage Parkway is an opportunity to:*

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,*
- Address travel demand in the VMC and surrounding area,*
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,*
- Improve connections to local and regional infrastructure,*
- Support transit-oriented nodes and corridors,*
- Enhance transit ridership, cycling and walking, and*
- Provide an alternate to Highway 7.*



# Existing Conditions - Portage Parkway, Appplewood Crescent to Jane Street



**Notes:**

- Road Features:
  - Posted speed limit is 50km/h
- Natural Sciences:
  - Vegetation communities include old field cultural meadow and deciduous forest
- Stage 1 Archaeological Assessment:
  - Most of the study area has low to no archaeological potential
  - Sections found to have archaeological potential require a Stage 2 assessment

**PART A and PART B areas are conceptual subject to EA Study**



# Existing Conditions – Portage Parkway Extension to Creditstone Road



PART A and PART B areas are conceptual subject to EA Study

# Alternative Planning Solutions in the Transportation Master Plan

- Problem or opportunity identified in the Transportation Master Plan**
  - “Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area.”
- Alternative solution considered by the Transportation Master Plan:**
  - “Need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.”
- Impacts assessed in the Transportation Master Plan:**
  - “The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network.”

# Alternative Planning Solutions for Portage Parkway

- Do Nothing**
- Travel Demand Management Initiatives** (e.g., flexible work hours, telecommuting, carpooling)
- Alternative Modes of Transportation** (e.g., cycling and walking facilities, transit)
- Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)



# Summary of Alternative Planning Solutions

Alternative	Advantages	Disadvantages	Summary
<b>Do Nothing</b>	<ul style="list-style-type: none"> <li>No natural environmental impacts</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>Does not conform to VMC Secondary Plan / Transportation Master Plan (TMP)</li> <li>No improvements to traffic safety</li> </ul>	<ul style="list-style-type: none"> <li>Does not address problem and opportunity statement</li> <li>Carried forward for comparative evaluation</li> </ul>
<b>Travel Demand Management Initiatives</b>	<ul style="list-style-type: none"> <li>Indirect improvements encourage alternative transportation</li> <li>Long term potential for increased traffic capacity</li> <li>Partially conforms to VMC Secondary Plan / TMP</li> <li>Long term potential for reduction in vehicular emissions</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>No improvements to traffic safety</li> </ul>	<ul style="list-style-type: none"> <li>Carried forward as part of recommended solution</li> </ul>
<b>Alternative Modes of Transportation</b>	<ul style="list-style-type: none"> <li>Direct and indirect improvements encourage alternative transportation</li> <li>Partial reduction of traffic volumes</li> <li>Partially conforms to VMC Secondary Plan / TMP / Pedestrian and Cycling Master Plan</li> <li>Potential for minor improvements to aesthetics and streetscape</li> <li>Low construction / implementation costs</li> </ul>	<ul style="list-style-type: none"> <li>Does not accommodate future traffic growth</li> <li>No impact to traffic safety</li> <li>Low potential for archaeological impact</li> </ul>	<ul style="list-style-type: none"> <li>Carried forward as part of recommended solution</li> </ul>

# Summary of Alternative Planning Solutions

Alternative	Advantages	Disadvantages	Summary
<b>Localized Intersection and Operational Improvements</b>	<ul style="list-style-type: none"> <li>• Marginally improves level of service for traffic</li> <li>• Opportunity for gateway intersections per Streetscape and Open Space Plan</li> </ul>	<ul style="list-style-type: none"> <li>• Does not accommodate future traffic growth</li> <li>• Low construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>
<b>Widening Portage Parkway from Applewood Crescent to Jane Street</b>	<ul style="list-style-type: none"> <li>• Opportunity to encourage alternative transportation</li> <li>• Improves level of service for traffic</li> <li>• Conforms to VMC Secondary Plan / TMP</li> <li>• Opportunity to address traffic operations</li> <li>• Opportunity to improve aesthetics and streetscape per VMC Streetscape and Open Space Plan</li> <li>• Improves access to industry and businesses</li> <li>• Improves emergency access</li> </ul>	<ul style="list-style-type: none"> <li>• Increases surface area contributing to storm-water runoff</li> <li>• Low potential for archaeological impact</li> <li>• Property required</li> <li>• High construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>
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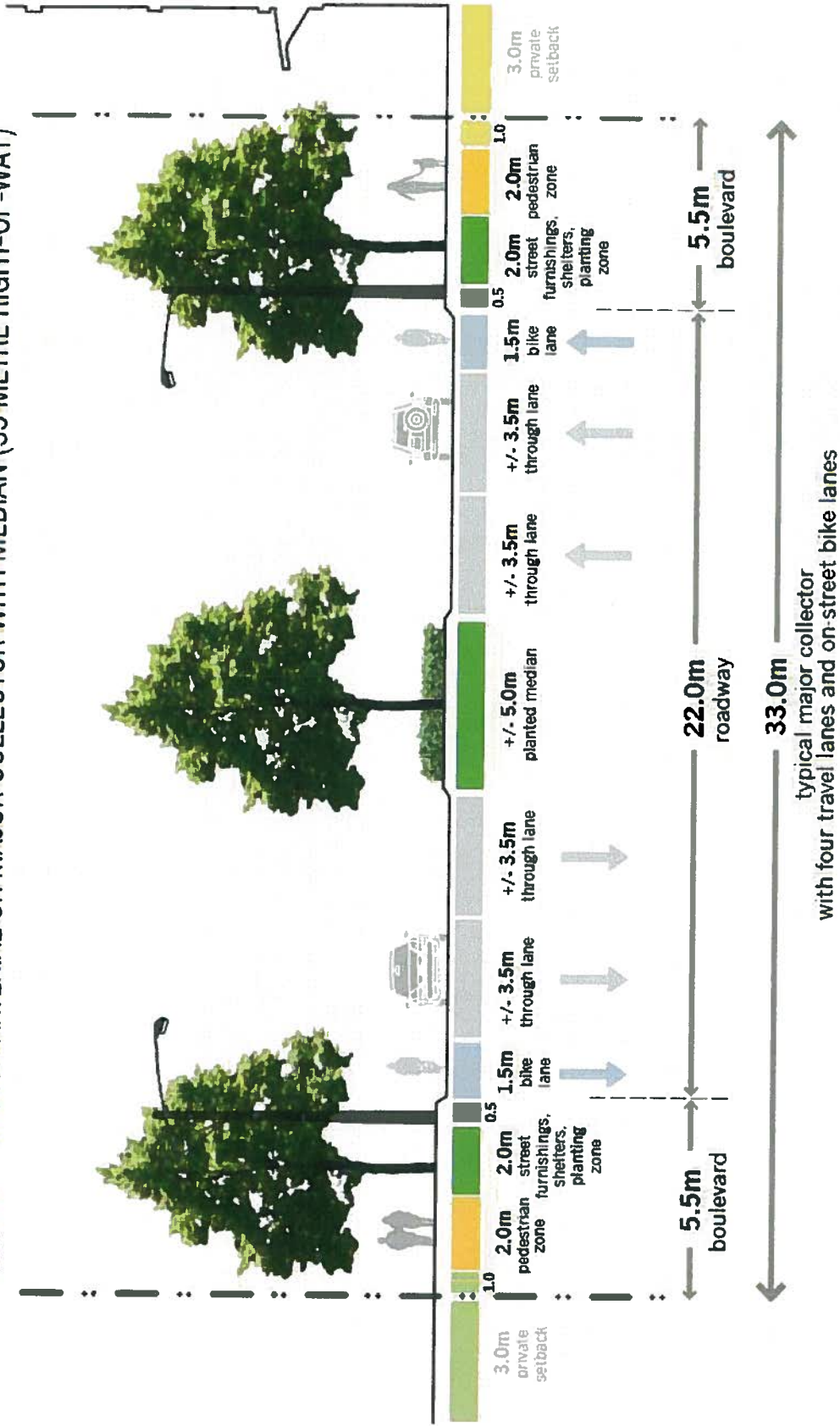
# Recommended Solution

The recommended solution for Portage Parkway is a combination of alternatives:

- ❑ **Travel Demand Management Initiatives** were identified in the Transportation Master Plan and are being implemented by the City as a separate strategy
- ❑ **Alternative Modes of Transportation** were identified in the Transportation Master Plan, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- ❑ **Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- ❑ **Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- ❑ **Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)

# Design - VMC Secondary Plan

FIGURE E > MINOR ARTERIAL OR MAJOR COLLECTOR WITH MEDIAN (33 METRE RIGHT-OF-WAY)





# Design Framework Typical Streetscape Elements for Mid-Block

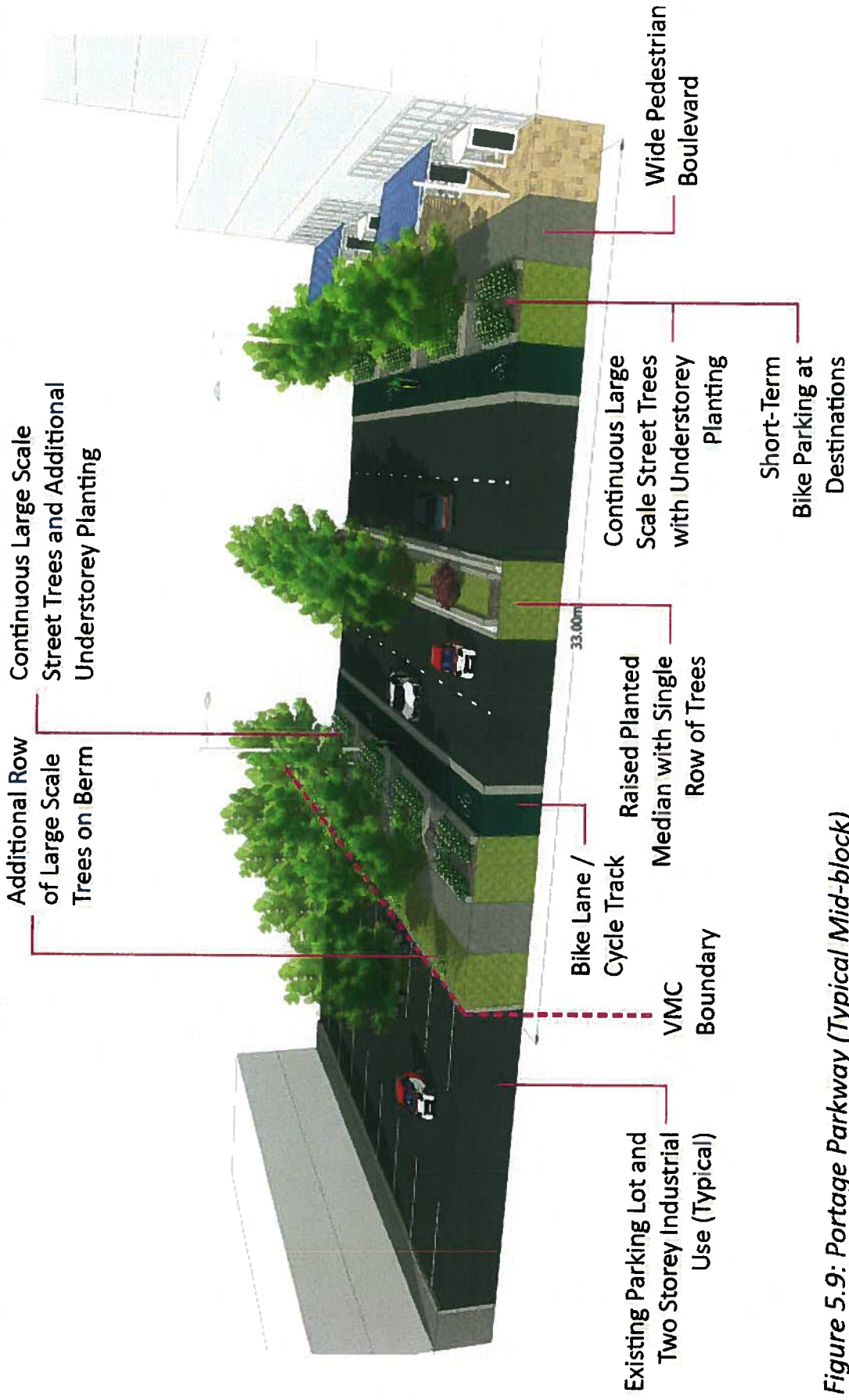


Figure 5.9: Portage Parkway (Typical Mid-block)

Vaughan Metropolitan Centre Streetscape and Open Space Plan, EDA Collaborative Inc., 2015

# Design Evaluation Criteria

Criteria considered for high level assessment and comparison of alternative planning solutions:

## Cultural

- Archaeology
- Built Heritage and Cultural Landscapes

## Economic

- Capital Costs

## Implementation

- Regulatory Framework
- Staging
- Municipal Servicing Coordination

## Infrastructure Planning

- Alternative Transportation
- Level of Service
- Network Capacity
- Planning Policy
- (Official/Secondary Plans, Transportation Master Plans)
- Stormwater Management / Creek Crossing
- Traffic Safety

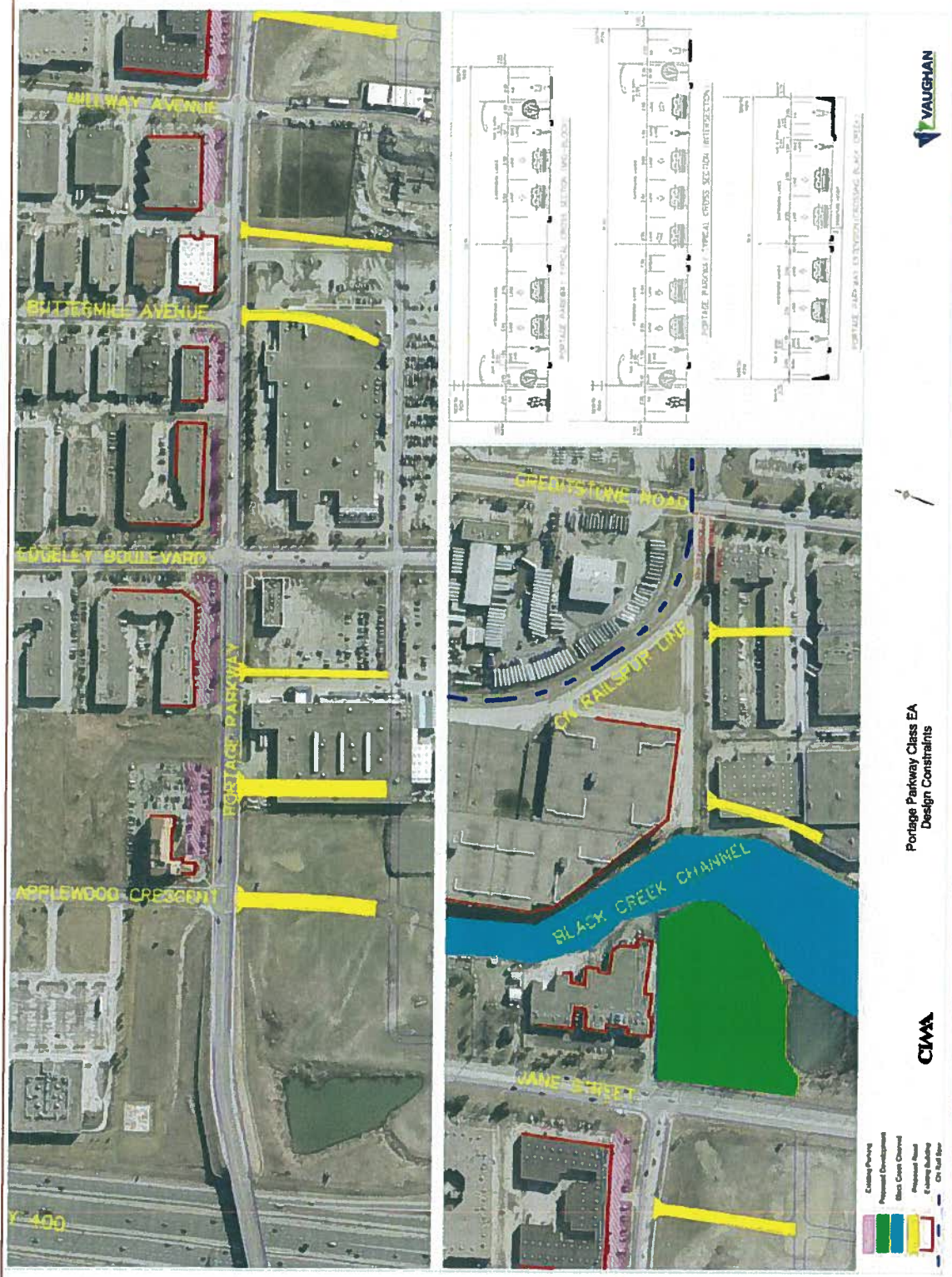
## Natural

- Aquatic
- Avian and Wildlife
- Natural Areas
- Species at Risk
- Vegetation
- Watercourses

## Social

- Aesthetics and Streetscape
- Air Quality
- Businesses
- Emergency Services
- Land Use
- Noise
- Property Access

# Design Opportunities and Constraints





# Design Opportunities and Constraints



VAC STREETS AND OPEN SPACE PLAN (2015) FOR CONSIDERATION OF LANDSCAPE DESIGN OPPORTUNITIES AND CONSTRAINTS WITH APPLEWOOD CRESCENT AND JANE STREET AND WORK INTERSECTION DESIGN AT INCLUDING INTERSECTIONS WITH THE STUDY AREA

- TRANSITIONAL ZONE BETWEEN URBAN CENTRE AND EMPLOYMENT LANDS
- PORTAGE PARKWAY AS THE LANDSCAPE EDGE FOR THE VAC
- PEDESTRIAN PROSITY ZONES AT EDSELLEY BOULEVARD



Proposed Development  
 Black Creek Channel  
 Proposed Road  
 CH Rail Spur Line

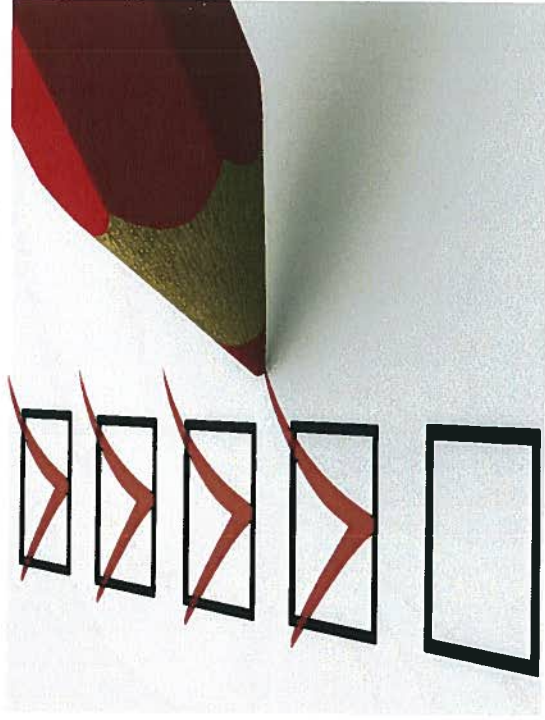
Portage Parkway Class EA  
 Design Considerations

CIMA





# Next Steps



- Review your input
- Finalize Problem and Opportunity Statement
- Finalize Preferred Planning Solution
- Identify and Assess Alternative Design Concepts
- Host meetings with Technical Agencies and Stakeholders
- Host Public Information Centre No. 2 in 2016

# What do you think?

Please share your comments with either Project Manager by  
December 11, 2015:

## **Marta Roias, RPP**

City of Vaughan Project Manager

2141 Major Mackenzie Drive

Vaughan, ON L6A 1T1

T: 905-832-8585 Ext. 8026

F: 905-832-6145

E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

## **Stephen Keen, P.Eng.**

CIMA+ Project Manager

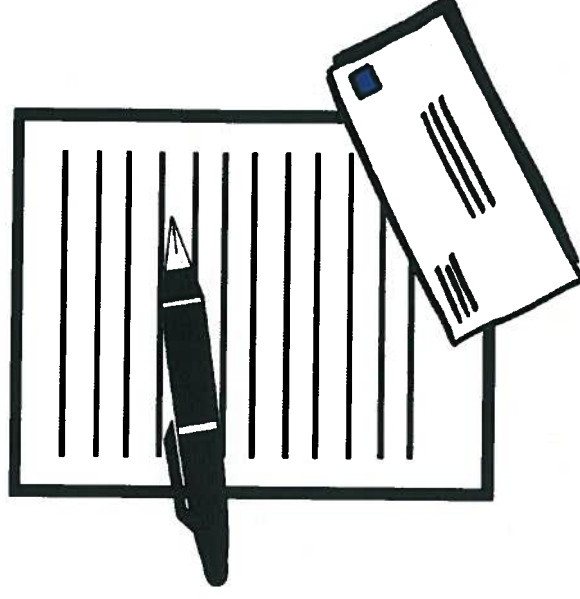
3027 Harvester Road, Suite 400

Burlington, ON L7N 3G7

T: 289-288-0287 Ext. 6834

F: 289-288-0285

E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)

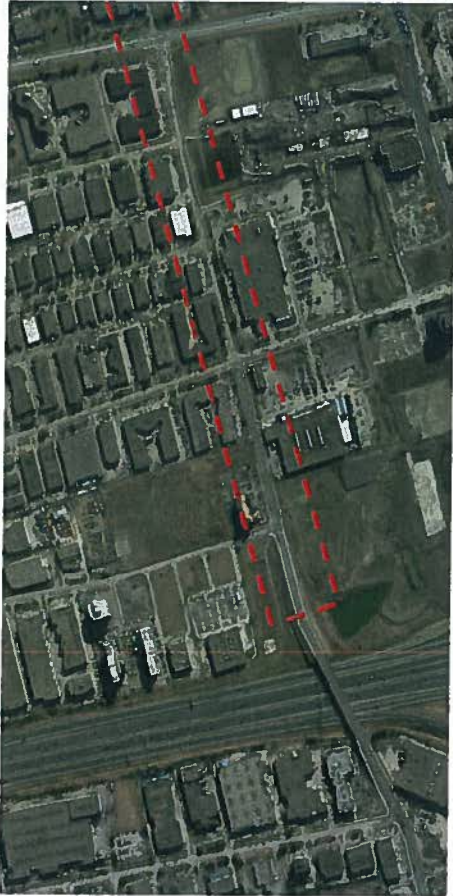


Thank you for attending this PIC

# Next Steps for Technical Agencies Committee

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- Forward comments on Draft Public Information Centre Display by **November 3, 2015**
- Attend Public Information Centre No. 1 on **November 25, 2015**
- Forward comments in response to Public Information Centre by **December 11, 2015**
- Meet with Project Team in advance of second Public Information Centre in **Spring 2016**



City of Vaughan  
**Portage Parkway  
Environmental Assessment**  
Public Information Centre No. 1

November 25, 2015





# Welcome to the Public Information Centre

This first Public Information Centre (PIC) introduces the Environmental Assessment (EA) Study for Portage Parkway Widening and Easterly Extension to Creditstone Road.

We welcome your input on:

- Background to the study
- Existing conditions of the study area
- Transportation problems or opportunities and solutions
- Design concepts for the preferred solutions
- Potential effects of the project and ways to avoid or minimize impacts

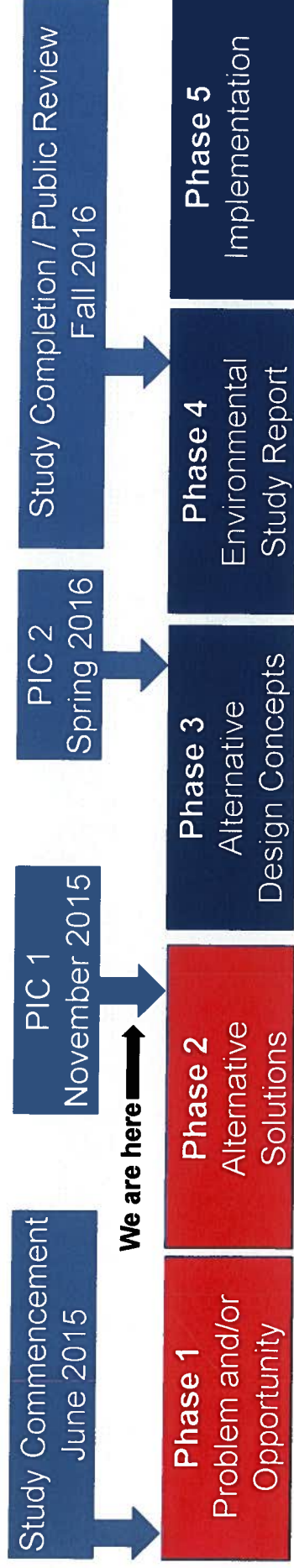
You are invited to:

- Review the information boards
- Ask questions and discuss your comments with the study team
- Fill out and submit a comment sheet
- Sign in to receive future updates on the study
- Visit the project website:

[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

# Class EA Process

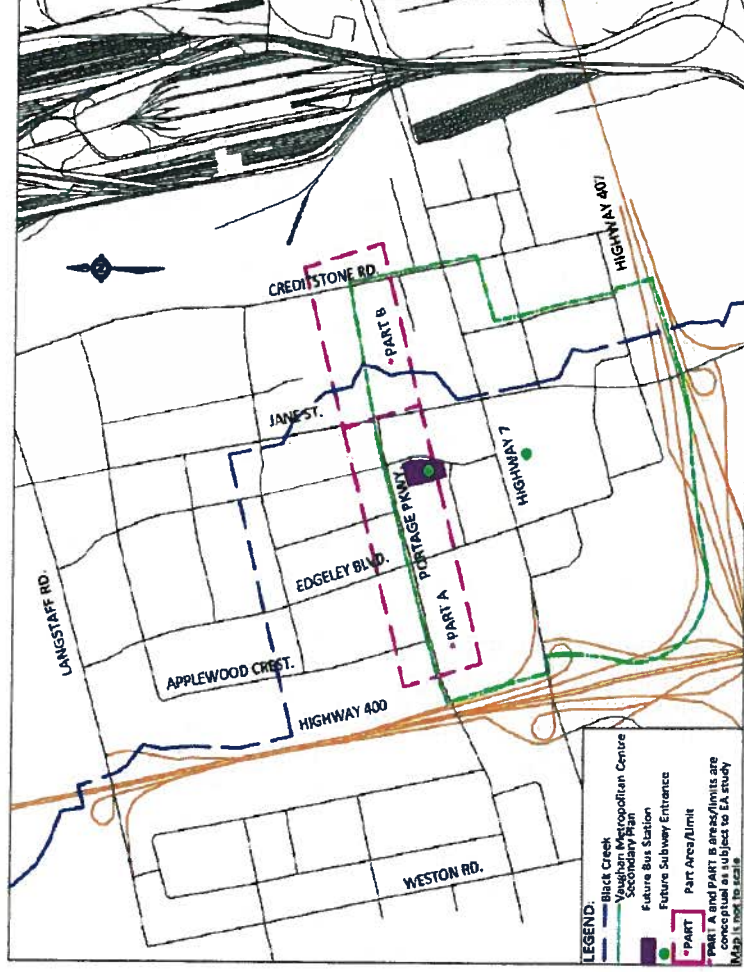
- This project is subject to the Municipal Engineers Association **Municipal Class Environmental Assessment** (2000 as amended in 2007 and 2011).
- The Municipal Class EA is a planning and design process approved by the Ministry of Environment and Climate Change to meet the requirements of the *Environmental Assessment Act*.
- This study follows the Class EA process for **Schedule C** projects.



- The City's Transportation Master Plan, having satisfied Phases 1 and 2, initiated the planning and design process for the Portage Parkway Widening and Easterly Extension to Creditstone Road.
- This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
  - Part A - Portage Parkway Widening from Applewood Crescent to Jane Street
  - Part B - Portage Parkway Extension from Jane Street to Creditstone Road.

# Study Area

- Improvements to Portage Parkway are being considered and proceeding as two interrelated Parts:
- **Part A** for the widening of Portage Parkway from two to four lanes from Applewood Crescent to Jane Street.
  - **Part B** for the extension of Portage Parkway from Jane Street crossing the Black Creek Channel to Creditstone Road.



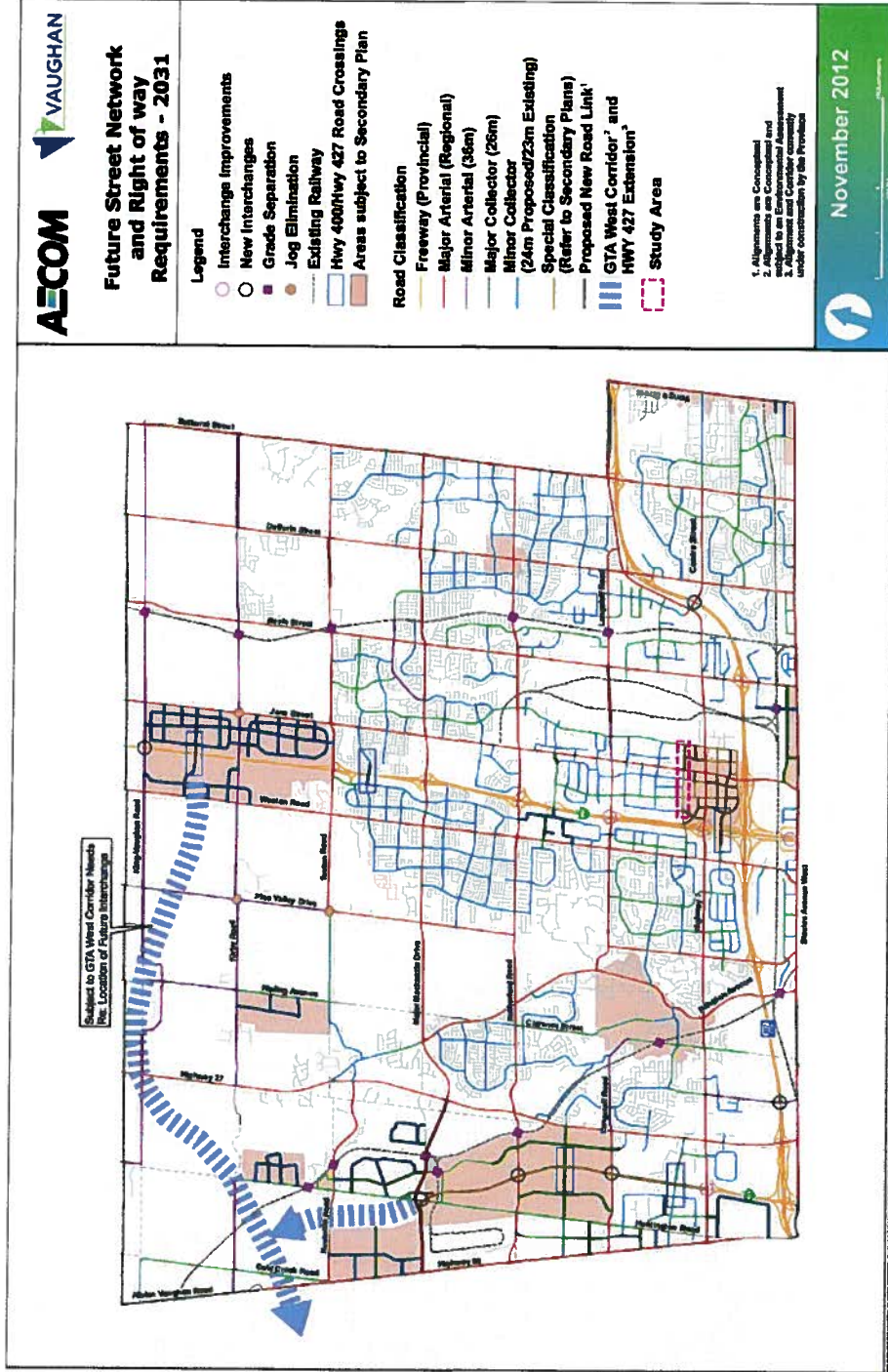
The total distance of the Study Area corridor is approximately 1.5 km.



# Transportation Master Plans

The City of Vaughan  
**Transportation Master Plan – A New Path** (2012) established the need and justification for the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element, supporting the planned level of intensification and building of the City's downtown - Vaughan Metropolitan Centre (VMC).

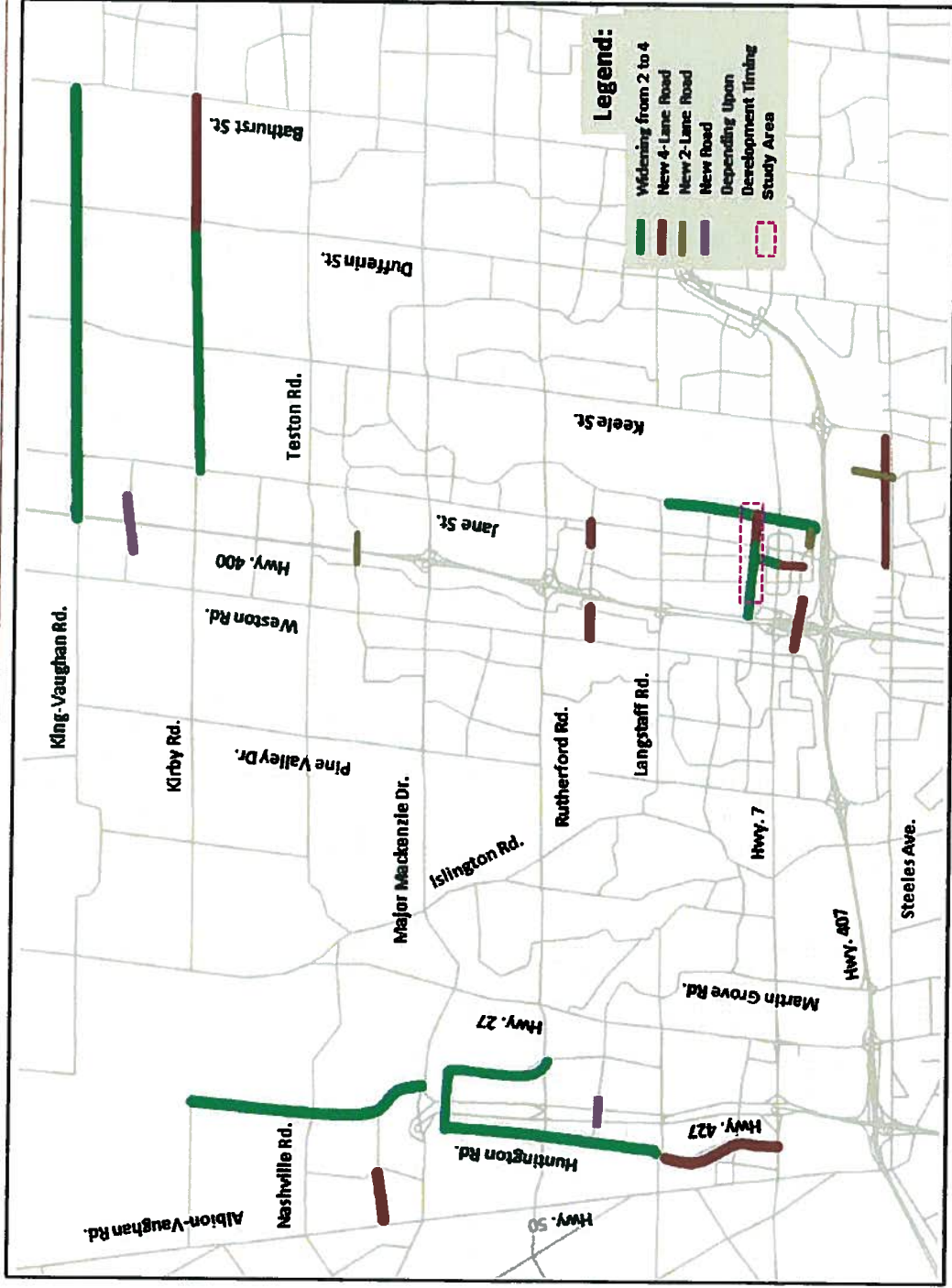
The City of Vaughan **Pedestrian and Bicycle Master Plan** (2007) identified Portage Parkway for "Neighbourhood Bike Lanes" with pavement markings and signs.



Transportation Master Plan – A New Path, City of Vaughan, 2012  
 Pedestrian and Bicycle Master Plan Study, Marshall Macklin Monaghan with ESG International, 2007



# Recommended 2031 Road Improvements



**Road Improvements Needed by 2031**  
 Transportation Master Plan – A New Path, City of Vaughan, 2012

# Transportation Master Plan

This study advances the planning and design process under the Municipal Class EA process for:

- Widening Portage Parkway to four lanes from Applewood Crescent to Jane Street, and
- Extending Portage Parkway with four lanes from Jane Street to Creditstone Road.

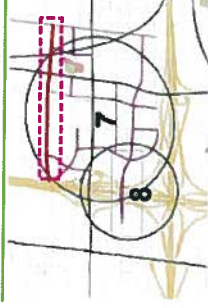
## Portage Parkway Widening and Easterly Extension

**Purpose:** With the recent completion of the Highway 400 crossing, an improved Portage Parkway is a key element of the improved VMC grid road network, supporting intensification and transit-oriented development in Vaughan's pre-eminent centre and future Downtown. The easterly extension to Creditstone Road will provide a direct link from the industrial area east of the VMC to the relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

### Project Description

<b>Name</b>	▶ Portage Parkway Widening and Easterly Extension
<b>Project Limits</b>	▶ Between Highway 400 and Creditstone Road (see map area 7)
<b>Length</b>	▶ Approximately 1.5 km (see map area 7)
<b>Project Type</b>	▶ Widening to 4 lanes from Applewood Crescent to Jane Street, and new 4 lane road from Jane Street to Creditstone Road
<b>Proposed Phasing</b>	▶ 2012 – 2021
<b>Cost</b>	▶ \$4 M (excluding property)

### Project Location and Environmental Features



### Justification:

These are key road network improvements that will support the proposed development of the VMC. The proposed road network was developed in concert with the planning team that created the new secondary plan, and is supported by corridor deficiency analysis that identified the number of required lanes. An improved Portage Parkway will also facilitate the diversion of trucks around the VMC. In addition, the project strongly supports transit, cycling and pedestrian movement.

### Alternatives Considered

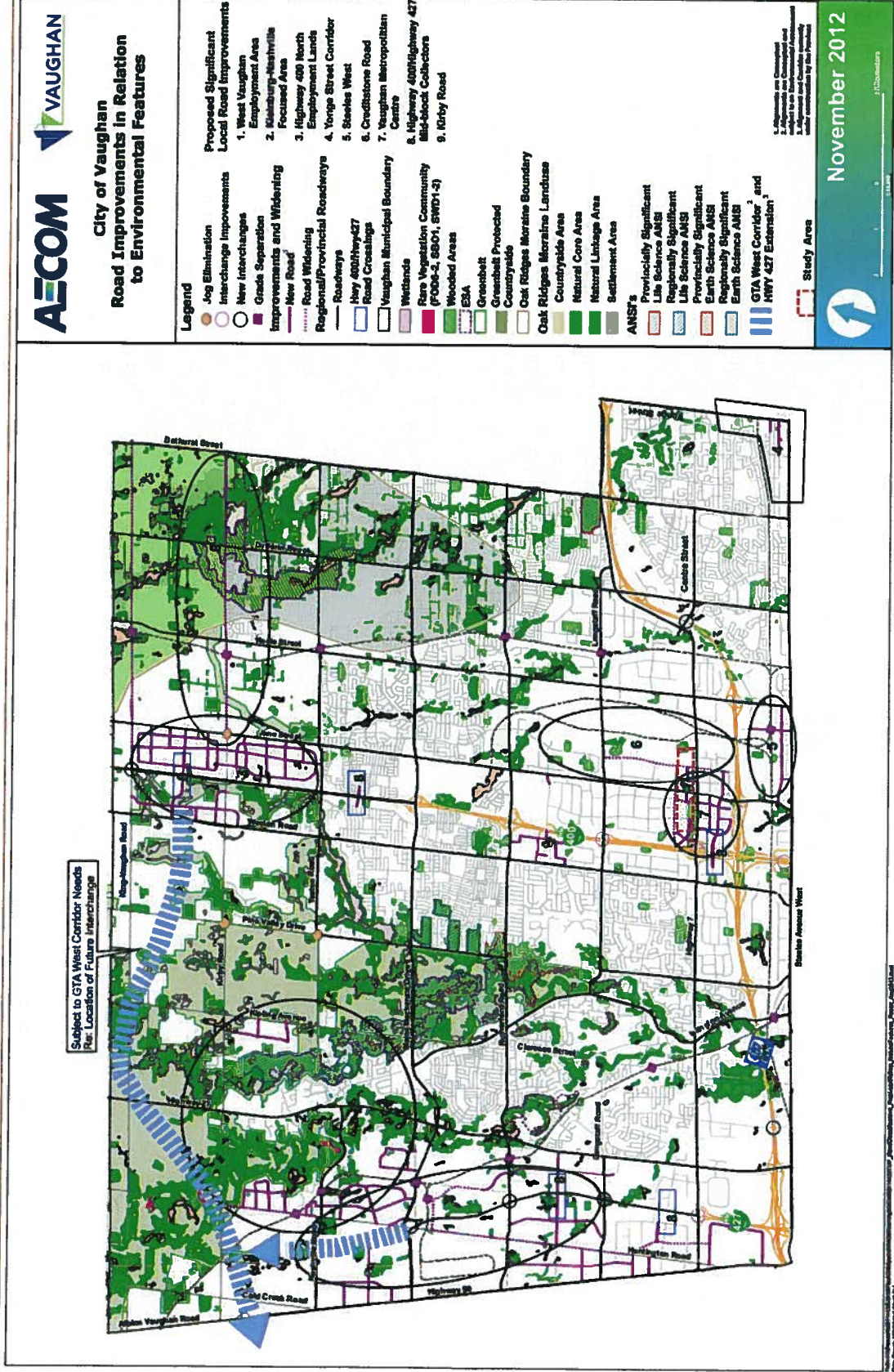
<b>Transit</b>	▶ The Spadina subway extension, with its terminal station within the VMC, together with the planned Highway 7 Rapidway, provided the transit focus for the development of the new VMC transportation plan.
<b>Roads</b>	▶ Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area. This led to the need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.

**Potential Socio-economic and Environmental Impacts:** The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network. The extension of Portage Parkway east of Jane Street avoids a wooded area that would have been impacted with implementation of the former road network plan.

**Conclusion:** Recommended for Phase 3 and 4 EA study.



# Study Context – Natural Environment



# VMC Secondary Plan – Transportation Studies

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The VMC Secondary Plan and supporting focused Transportation Plan:

- Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- Encourages the use of Portage Parkway as a link to Highway 400
- Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street
- Acquires right-of-way for the extension of Portage Parkway to Creditstone Road.

The VMC *Transportation Plan* (2012) and the *VMC and Surrounding Areas Transportation Study* (2013) provide recommendations for transportation improvements to 2021 and 2031.

Recommendations include:

- Extending Portage Parkway to alleviate congestion on Highway 7 by 2021.

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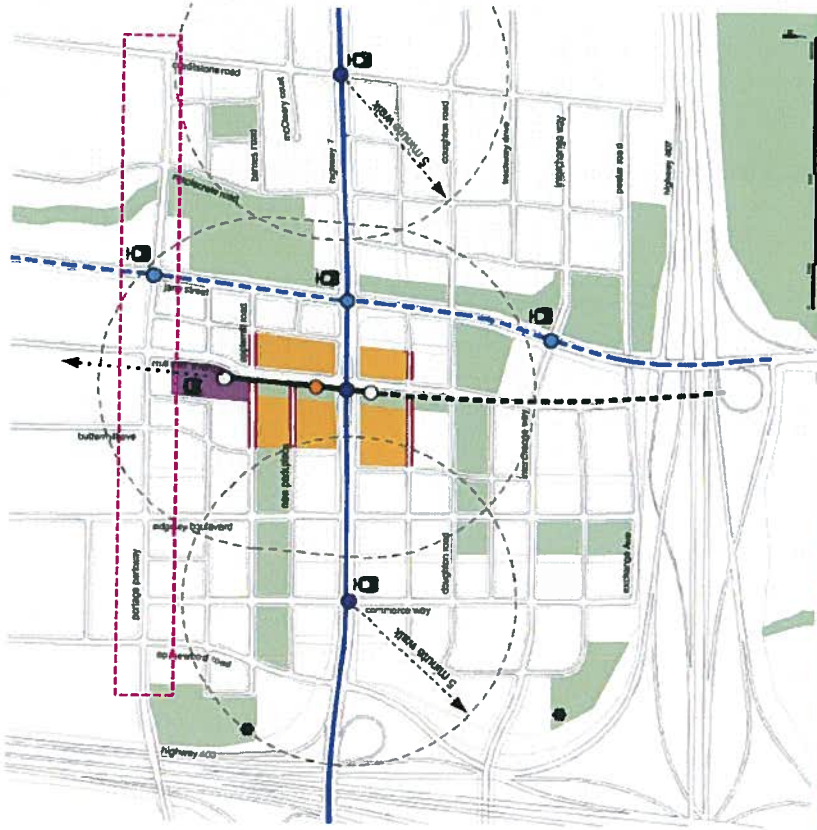
VMC Transportation Plan, AECOM, 2012  
Vaughan Metropolitan Centre and Surrounding Areas Transportation Study, Halcrow with Hatch Mott MacDonald, 2013  
The VMC Plan – Secondary Plan – Draft Consolidation Proposed Modifications WITHOUT PREJUDICE, Urban Strategies Inc., September 2015



# VMC Secondary Plan - Transit and Street Network

SCHEDULE B > TRANSIT NETWORK

- LEGEND**
- subway entrances
  - future subway entrances
  - potential Hwy7 rapidway stations
  - potential Jane Street rapidway stations
  - 5 minute walking radii
  - blocks adjacent to subway
  - on-street passenger pick-up and drop-off
  - see Policy 6.3.1
  - speeding subway alignment
  - future speeding subway extension
  - speeding subway station box
  - highway 7 rapidway
  - potential Jane Street rapidway
  - potential vias stations
  - bus station
  - major parks and open spaces



SCHEDULE C > STREET NETWORK

- LEGEND**
- arterials (width to be consistent with Region of York Official Plan)
  - minor arterial (33 m)
  - major collectors (28-33 m)
  - special collector (33 m)
  - minor collectors (23-26 m)
  - local streets (20-22 m)
  - news or local streets (15-17 m) (see Policy 4.3.16)
  - Coossus Drive overpass corridor protection area (see Policy 4.3.11)
  - major parks and open spaces
  - see Policy 4.3.2
  - see Policy 4.3.17
  - see Policy 6.3.1
  - Study Area



# VMC Secondary Plan

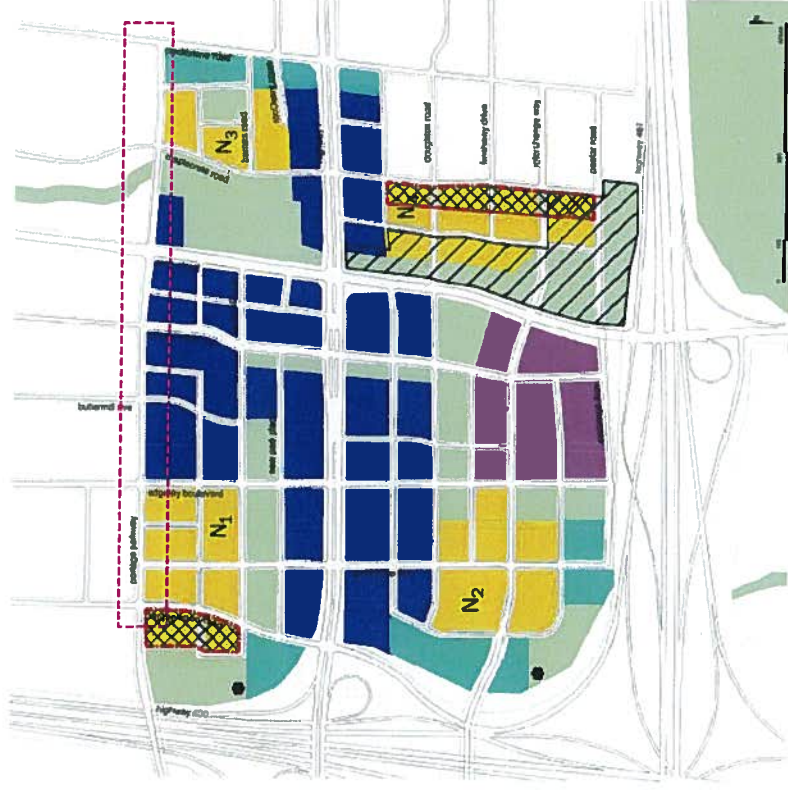
Portage Parkway Widening and Extension to Creditstone Road forms part of the planned street network addressing travel demand.

- + Designates Portage Parkway as a major collector road with a planned right-of-way of 28 to 33 metres
- + Establishes the Creditstone-Portage-Applewood Bypass to mitigate traffic on Highway 7 and Jane Street and facilitating an alternate for trucks

SCHEDULE F > LAND USE PRECINCTS

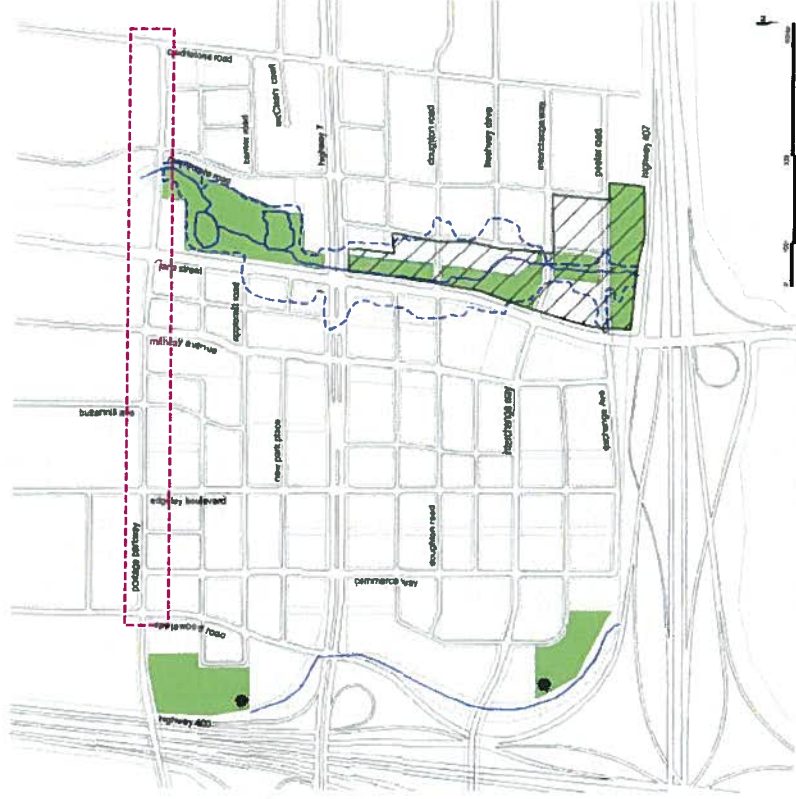
LEGEND

- station precinct
- south precinct
- neighbourhood precincts
- west and east employment precincts
- major parks and open spaces
- land use designations and identification of areas subject to results of the VMC Black Creek Renewal EA (Stages 3 & 4) and final results of the VMC Servicing and Stormwater Management Master Plan (see also Schedules C and J, and Policies 5.6.4 - 5.6.8, 8.2.7 and 8.4.3)
- office uses permitted (see Policy 8.4.4) see Policy 6.3.1
- Study Area



# VMC Secondary Plan

SCHEDULE J > FLOODPLAIN AND ENVIRONMENTAL OPEN SPACES



Extension east of Jane Street to Creditstone Road will necessitate a structured crossing of the Black Creek channel.

Black Creek Stormwater Optimization Study Master Plan Municipal Class EA Report (Phases 1 and 2) and the VMC Streetscape and Open Space Plan provide the broader context for advancing a co-ordinated design for the crossing of the Black Creek Channel - a key structuring feature of the VMC Secondary Plan.

# Transportation Benefits

## Widening Portage Parkway will:

- Accommodate travel demand from future development
- Address capacity and operational issues at intersections
- Facilitate the addition of bicycle facilities as part of the widening

## Extending Portage Parkway will:

- Increase pedestrian access from Creditstone Road to future (2016) Jane Street vivaNext station
- Create an alternative route for trucks to bypass the VMC core
- Provide a continuous route for all travel modes from Weston Road to Creditstone Road



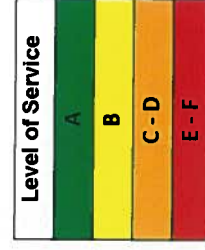
# Existing and Future Traffic Conditions



Existing conditions



Future conditions



OPTIMAL



CONGESTED

\* Future conditions with VMC to 2031 planning horizon (without widening of Portage Parkway)

# Problem and Opportunity Statement









*Transportation improvements are needed to support growth within the VMC and surrounding area.*

*Widening and extending Portage Parkway is an opportunity to:*

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,*
- Address travel demand in the VMC and surrounding area,*
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,*
- Improve connections to local and regional infrastructure,*
- Support transit-oriented nodes and corridors,*
- Enhance transit ridership, cycling and walking, and*
- Provide an alternate to Highway 7.*



# Existing Conditions - Portage Parkway, Appplewood Crescent to Creditstone Road

Study Area	
4 Lane Roadway	
2 Lane Roadway	
Land Use – Employment /Industrial	
Vaughan Metropolitan Centre Secondary Plan Area	
Heritage Property	
Existing Sidewalk	
CN Rail Spur	
Black Creek Channel	



**Notes:**

- **Road Features:**
    - Posted speed limit is 50 km/h
  - **Natural Sciences:**
    - Vegetation communities include old field cultural meadow and deciduous forest
  - **Stage 1 Archaeological Assessment:**
    - Most of the study area has low to no archaeological potential;
    - Sections found to have archaeological potential require a Stage 2 assessment
- PART A and PART B areas are conceptual subject to EA study

# Alternative Planning Solutions in the Transportation Master Plan

- Problem or opportunity identified in the Transportation Master Plan**
  - “Highway 7 is designated as a Rapid Transit corridor and, thus, will not be able to supply additional road capacity to the area.”
- Alternative solution considered by the Transportation Master Plan:**
  - “Need for parallel east-west collector roads extending throughout the VMC, optimizing network flexibility and providing additional east-west capacity.”
- Impacts assessed in the Transportation Master Plan:**
  - “The VMC area has long been designated for major growth and is located well beyond the Oak Ridges Moraine and the Green Belt, and will therefore not significantly impact the natural heritage network.”



# Alternative Planning Solutions for Portage Parkway

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- Do Nothing**
- Travel Demand Management Initiatives** (e.g., flexible work hours, telecommuting, carpooling)
- Alternative Modes of Transportation** (e.g., cycling and walking facilities, transit)
- Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
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# Summary of Alternative Planning Solutions

Alternative	Advantages	Disadvantages	Summary
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<b>Widening Portage Parkway from Applewood Crescent to Jane Street</b>	<ul style="list-style-type: none"> <li>• Opportunity to encourage alternative transportation</li> <li>• Improves level of service for traffic</li> <li>• Conforms to VMC Secondary Plan / TMP</li> <li>• Opportunity to address traffic operations</li> <li>• Opportunity to improve aesthetics and streetscape per VMC Streetscape and Open Space Plan</li> <li>• Improves access to industry and businesses</li> <li>• Improves emergency access</li> </ul>	<ul style="list-style-type: none"> <li>• Increases surface area contributing to storm-water runoff</li> <li>• Low potential for archaeological impact</li> <li>• Property required</li> <li>• High construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>
<b>Extending Portage Parkway from Jane Street to Creditstone Road</b>	<ul style="list-style-type: none"> <li>• Opportunity to encourage alternative transportation</li> <li>• Conforms to VMC Secondary Plan / TMP</li> <li>• Provides alternative route for truck traffic</li> <li>• Improves access to industry and businesses</li> <li>• Improves emergency route</li> </ul>	<ul style="list-style-type: none"> <li>• Increases surface area contributing to storm-water runoff</li> <li>• Potential traffic noise impact</li> <li>• Potential environmental issues</li> <li>• Low potential for archaeological impact</li> <li>• Property required</li> <li>• High construction costs</li> </ul>	<ul style="list-style-type: none"> <li>• Carried Forward</li> </ul>

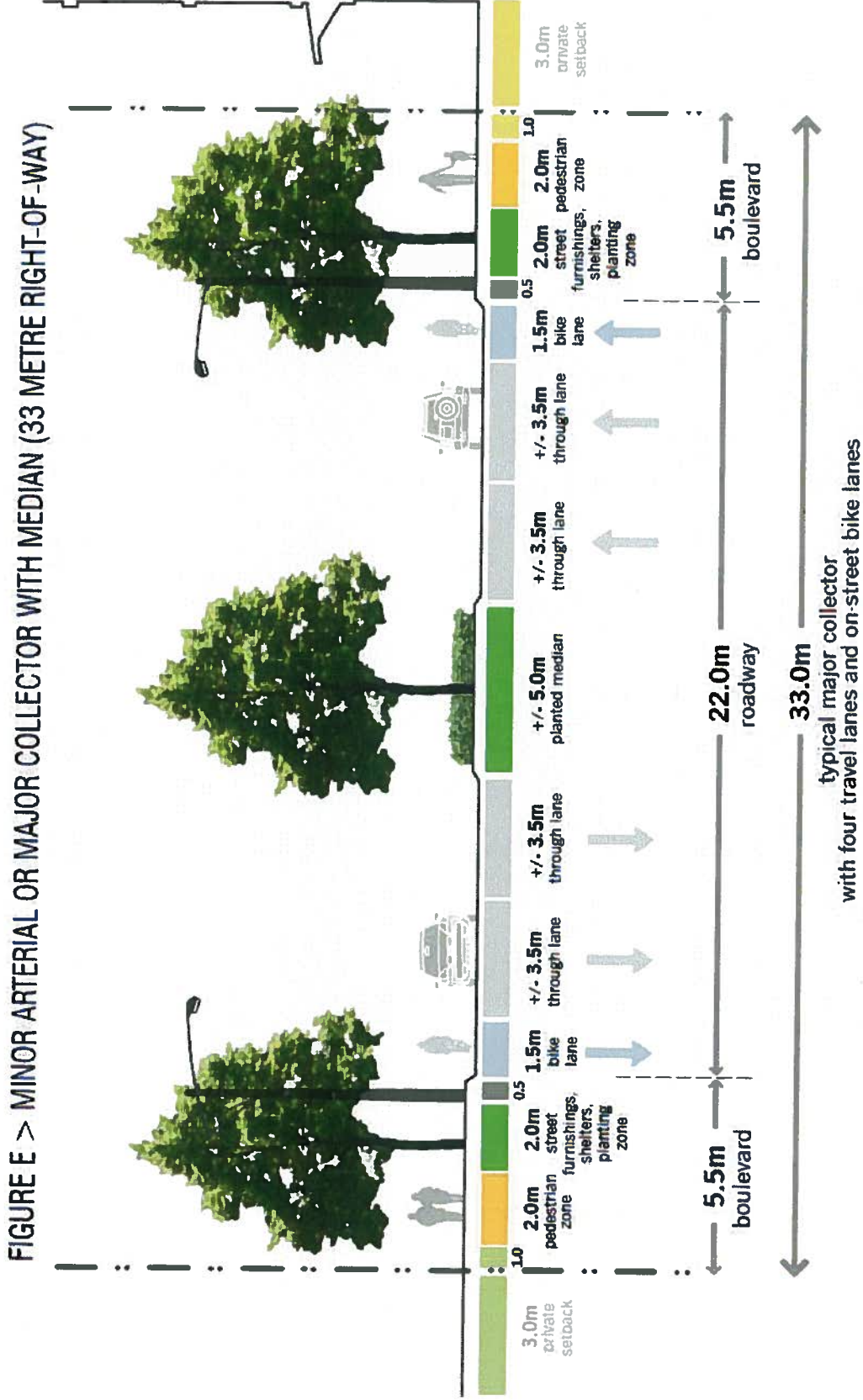
# Recommended Solution

The recommended solution for Portage Parkway is a combination of alternatives:

- ❑ **Travel Demand Management Initiatives** were identified in the Transportation Master Plan and are being implemented by the City as a separate strategy
- ❑ **Alternative Modes of Transportation** were identified in the Transportation Master Plan, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- ❑ **Localized Intersection and Operational Improvements** (e.g., traffic signals, turning lanes)
- ❑ **Widening Portage Parkway from Applewood Crescent to Jane Street** (e.g., 2 to 4 lanes)
- ❑ **Extending Portage Parkway from Jane Street to Creditstone Road** (e.g., new 4 lane roadway)



# Design - VMC Secondary Plan



The VMC Plan – Secondary Plan – Draft Consolidation Proposed Modifications WITHOUT PREJUDICE, Urban Strategies Inc., September 2015

# Design Framework

## Typical Streetscape Elements for Mid-Block

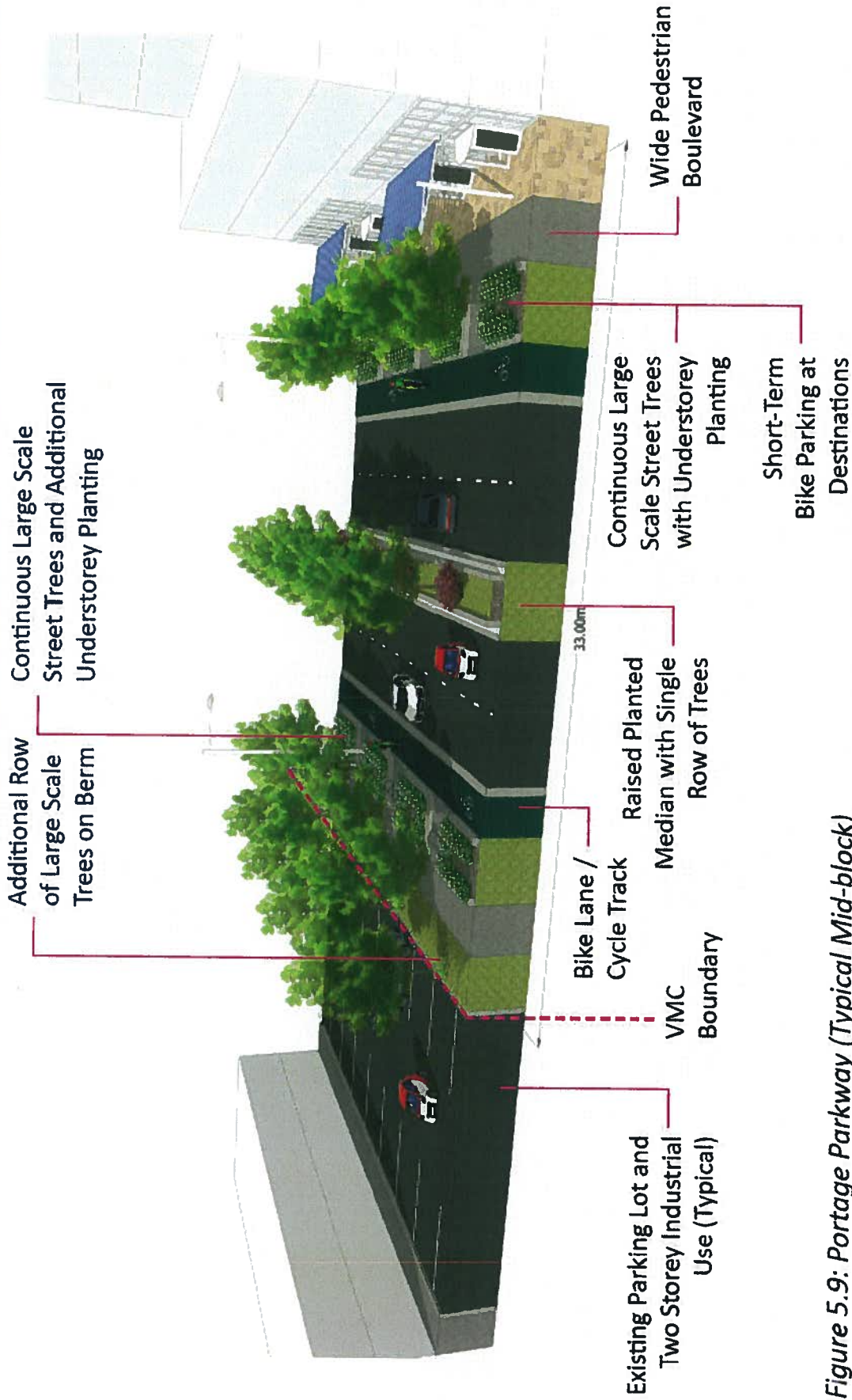


Figure 5.9: Portage Parkway (Typical Mid-block)

Vaughan Metropolitan Centre Streetscape and Open Space Plan, EDA Collaborative Inc., Draft January 2015

# Design Evaluation Criteria

Criteria considered for high level assessment and comparison of alternative planning solutions:

## Cultural

- Archaeology
- Built Heritage and Cultural Landscapes

## Economic

- Capital Costs

## Implementation

- Regulatory Framework
- Staging
- Municipal Servicing Coordination

## Infrastructure Planning

- Alternative Transportation
- Streetscape
- Level of Service
- Network Capacity
- Planning Policy (Official/Secondary Plans, Transportation Master Plans)
- Stormwater Management / Creek Crossing
- Traffic Safety

## Natural

- Aquatic
- Avian and Wildlife
- Natural Areas
- Species at Risk
- Vegetation
- Watercourses

## Social

- Air Quality
- Businesses
- Emergency Services
- Land Use
- Noise
- Property Access



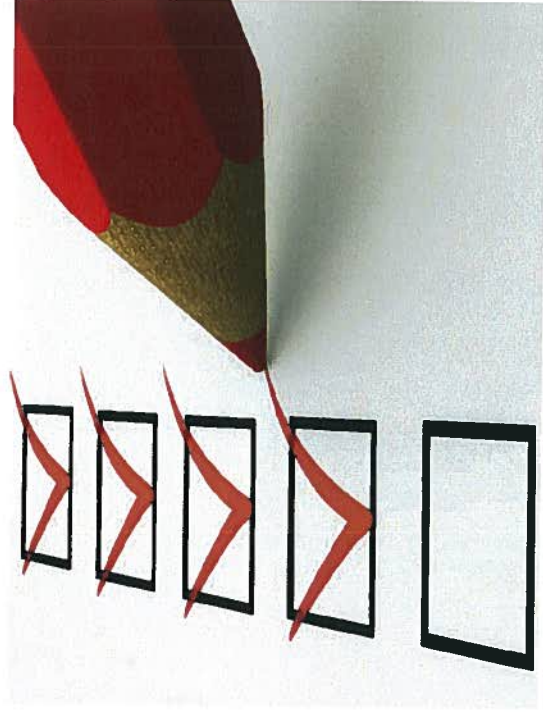
# Design Opportunities and Constraints







# Next Steps



- Review your input
- Finalize Problem and Opportunity Statement
- Finalize Preferred Planning Solution
- Identify and Assess Alternative Design Concepts
- Identify preliminary preferred design concept
- Host meetings with Technical Agencies and Stakeholders
- Host Public Information Centre No. 2 in 2016

# What do you think?

Please share your comments with either Project Manager by  
December 11, 2015:

**Marta Roias, RPP**

City of Vaughan Project Manager

2141 Major Mackenzie Drive

Vaughan, ON L6A 1T1

T: 905-832-8585 Ext. 8026

F: 905-832-6145

E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

**Stephen Keen, P.Eng.**

CIMA+ Project Manager

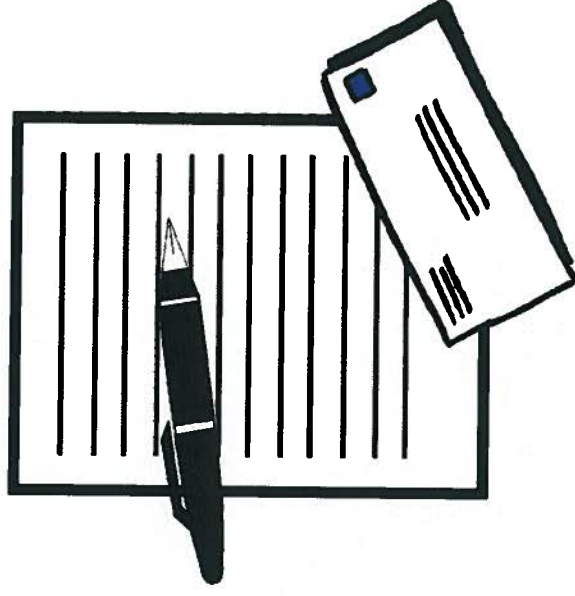
3027 Harvester Road, Suite 400

Burlington, ON L7N 3G7

T: 289-288-0287 Ext. 6834

F: 289-288-0285

E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)



Thank you for attending this PIC

## MINUTES OF MEETING

**CLIENT** : City of Vaughan (City)

**PROJECT** : Environmental Assessment (EA) Study for  
Part A - Portage Parkway Widening, Applewood Crescent to Jane Street  
Part B - Portage Parkway Extension, Jane Street to Creditstone Road

**MEETING #3** : Meeting with Stakeholders Group (SG)

**DATE OF MEETING** : February 2, 2016, 2:00 pm

**LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive,  
2<sup>nd</sup> Floor, Committee Room 245

**SG ATTENDEES** :

**PROJECT TEAM** : City of Vaughan (Marta Roias, Selma Hubjer )  
CIMA Canada Inc. (CIMA+; Stephen Keen, Sonya Kapusin)

**C.C. TO** : attendees  
City of Vaughan (Andy Lee, Brianne Clace, Daniel Rende, Danny Woo,  
Dino Macchiusi, Frank Facchini, Gerardo Paez Alonso, Paul Salerno,  
Saad Yousaf, Selma Hubjer, Tim Simmonds, Vince Suppa)

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**Note:** If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.



---

**DISCUSSION TOPICS****ACTION  
BY**

---

- 1 STUDY STATUS UPDATE**
  - The City welcomed all in attendance and facilitated roundtable introductions. Attendees were asked to sign-in and an agenda was distributed in advance of the meeting.
- 2 DRAFT PUBLIC INFORMATION CENTRE (PIC) BOARDS**
  - The Project Team presented PIC information boards in PowerPoint. The intent of the presentation was to review the PIC content and discuss most of the agenda items.
  - During the presentation (attached), the Project Team clarified that Part A and B are running concurrently, and the limits for Part A include the Jane Street intersection to the west bank of Black Creek.
- 3 SUMMARY OF STUDIES COMPLETED SINCE PIC 1**
  - See PowerPoint Presentation.
- 4 ALTERNATIVE DESIGN CONCEPTS FOR THE RECOMMENDED PLANNING SOLUTION**
  - See PowerPoint Presentation.
- 5 EVALUATION OF ALTERNATIVE DESIGNS AND ALIGNMENTS**
  - See PowerPoint Presentation.
- 6 PRELIMINARY PREFERRED DESIGN FOR WIDENING AND EXTENSION**
  - See Preliminary Preferred Plan.
  - Please note that the stated design for the Creek Crossing was mistakenly stated as being a 35m span bridge – this is wrong and options are being developed and evaluated revisited.
- 8 BENEFITS, IMPACTS AND MITIGATION MEASURES ASSOCIATED WITH PREFERRED DESIGN**
  - Part A implementation is a priority, including interim improvements at the intersection with Jane Street; Part A extends to the west bank of the Black Creek channel. Part B will be implemented as the VMC and development unfolds on the south side.
  - The extension of VMC roads will be shown on the preliminary preferred drawing.
  - Intersection improvements at Applewood Crescent are being considered.
  - The 27m cross-section width was selected at the bridge because trees will not be planted and this width facilitated reduction in property impacts; The 33m cross-section width considers needs at the Jane Street intersection; The reduced cross-section compromises on boulevards.
  - Property acquisition will follow the EA as a separate process, once the EA has been filed on the public record.
  - The northern entranceway at 7941 Jane Street can be addressed with the Project Team; At a minimum, the Region will accommodate a right in right out access on Jane Street.

CIMA+

DISCUSSION TOPICS	ACTION BY
<ul style="list-style-type: none"> <li>Existing access points are being preserved mostly on the north side. Ideally two entrances next to one another should be combined. No access points to _____ property are affected; _____ will advise of concerns when they review the drawing. The City can provide the preliminary preferred drawing in CAD, with the understanding that it is a work in progress.</li> <li>Regarding cycle tracks in cross section, consider whether 33m are needed if there is no connection to cycle tracks.</li> </ul>	
<b>9 NEXT STEPS</b>	
<ul style="list-style-type: none"> <li>Comments on the PIC boards were requested by February 16, 2016. The Project Team will circulate copies of the presentation with the preliminary preferred plan to the Stakeholders Group.</li> </ul>	CIMA+ Stakeholders Group



City of Vaughan  
**Portage Parkway  
Environmental Assessment**  
Stakeholders Group  
Meeting No. 3

February 2, 2016



# Agenda

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- ❑ Study Status Update
- ❑ Draft Public Information Centre (PIC) Boards
- ❑ Summary of Studies Completed Since PIC 1
- ❑ Alternative Design Concepts for the Recommended Planning Solution
- ❑ Evaluation of Alternative Designs and Alignments
- ❑ Preliminary Preferred Design for Widening and Extension
- ❑ Benefits, Impacts and Mitigation Measures Associated with Preferred Design
- ❑ Next Steps – Comments on PIC Boards by February 16, 2016





City of Vaughan

# Portage Parkway Environmental Assessment

Public Information Centre No. 2

March 9, 2016



# Welcome to the Public Information Centre

This Public Information Centre (PIC) is the second of two PICs held for the Environmental Assessment (EA) Study for Portage Parkway Widening and Easterly Extension to Creditstone Road.

The first PIC was held in November, 2015 to obtain public input on:

- Background to the study and existing conditions of the study area
- Transportation problems or opportunities and solutions

At this PIC, we welcome your input on:

- Design concepts for the preferred solutions
- Potential effects of the project and ways to avoid or minimize impacts

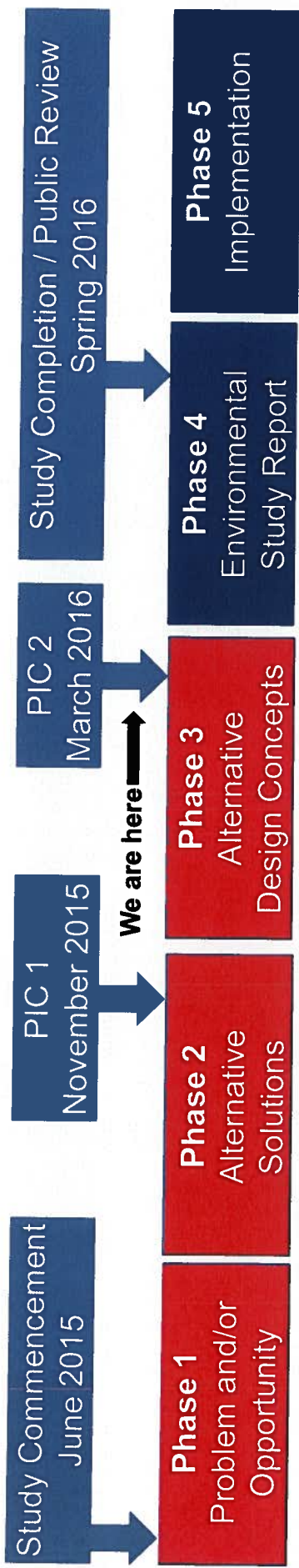
You are invited to:

- Review the information boards
- Ask questions and discuss your comments with the study team
- Fill out and submit a comment sheet
- Sign in to receive future updates on the study
- Visit the project website:

[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

# Class EA Process

- This project is subject to the Municipal Engineers Association **Municipal Class Environmental Assessment** (2000 as amended in 2007 and 2011).
- The Municipal Class EA is a planning and design process approved by the Ministry of Environment and Climate Change to meet the requirements of the *Environmental Assessment Act*.
- This study follows the Class EA process for **Schedule C** projects.

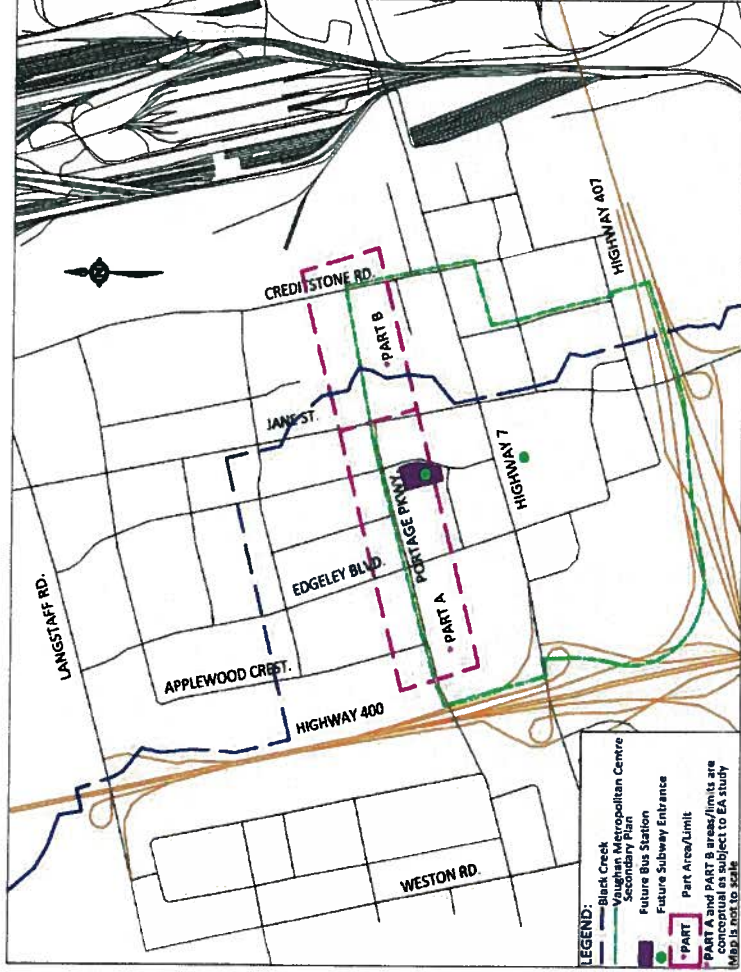


- The City's Transportation Master Plan, having satisfied Phases 1 and 2, initiated the planning and design process for the Portage Parkway Widening and Easterly Extension to Creditstone Road.
- This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
  - Part A - Portage Parkway Widening from Applewood Crescent to Jane Street
  - Part B - Portage Parkway Extension from Jane Street to Creditstone Road.



# Study Area

- Improvements to Portage Parkway are being considered and evaluated as two interrelated Parts:
  - **Widening west of Jane Street** – Widening of Portage Parkway from two to four lanes from Applewood Crescent to Jane Street.
  - **Extension east of Jane Street** – Extension of Portage Parkway from Jane Street crossing the Black Creek Channel to Creditstone Road.



The total distance of the Study Area corridor is approximately 1.5 km.



# Summary of PIC No. 1 - Future Traffic Conditions



**Future conditions  
without widening**



**Future conditions  
with widening**



# Summary of PIC No. 1

## **Problem and Opportunity**

Transportation improvements are needed to support growth within the VMC and surrounding area. Widening and extending Portage Parkway is an opportunity to:

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,
- Address travel demand in the VMC and surrounding area,
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,
- Improve connections to local and regional infrastructure,
- Support transit-oriented nodes and corridors,
- Enhance transit ridership, cycling and walking, and
- Provide an alternate to Highway 7.

## **Preferred Solution**

- Travel Demand Management Initiatives are being implemented by the City as a separate strategy
- Alternative Modes of Transportation, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- Localized Intersection and Operational Improvements (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road (e.g., new 4 lane roadway)

# Summary of Public Comments

- + Add a dedicated truck route to the Portage Parkway Extension
- + Add an advance green or a turning arrow for the left turn from Pennsylvania Avenue to southbound Applewood Crescent, and from eastbound Portage Parkway to northbound Applewood Crescent
- + Consider compatibility of staged intersection improvements and driveways as part of new developments with the ultimate design for Portage Parkway
- + Consider landscaping
- + Consider proximity of the road extension to the existing CN Rail spur
- + Consider the effects of property needs for right of way
  - + Consider functionality of buildings and businesses
  - + Consider property access, circulation and operations
- + Consider the future viability of the area for industry
- + Effects on the VMC subway station and Millway Avenue
- + Extend the sidewalk on Applewood Crescent north of Portage Parkway
- + Extend Portage Parkway to Keele Street over the CN property
- + Study the extension of Langstaff Road from Creditstone Road to Keele Street



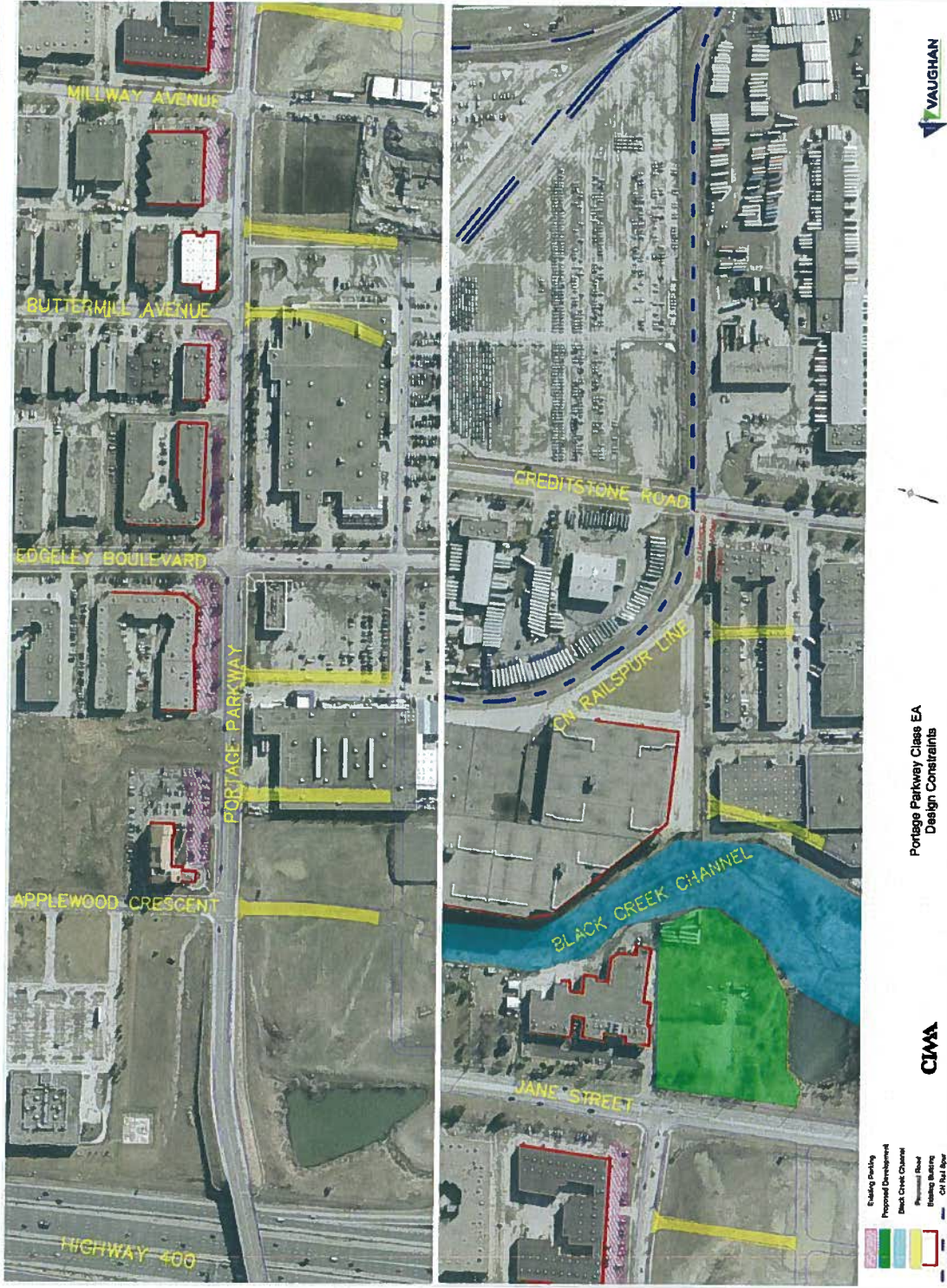
# Summary of Environmental Investigations

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- + Fluvial Geomorphic Assessment
- + Heritage Impact Assessment
- + Natural Environment Site Reconnaissance
- + Phase 1 Environmental Site Assessment
- + Stage 1 Archaeological Assessment
- + Stormwater Constraints



# Design Constraints





# Design Considerations



THE LANDSCAPE AND OPEN SPACE PLAN (2015) FOR CONSIDERATION OF THE PORTAGE PARKWAY CLASS EA DESIGN. THE PLAN SHOWS THE PROPOSED PORTAGE PARKWAY AND THE LANDSCAPE AND OPEN SPACE PLAN (2015) FOR CONSIDERATION OF THE PORTAGE PARKWAY CLASS EA DESIGN. THE PLAN SHOWS THE PROPOSED PORTAGE PARKWAY AND THE LANDSCAPE AND OPEN SPACE PLAN (2015) FOR CONSIDERATION OF THE PORTAGE PARKWAY CLASS EA DESIGN.

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- PEDESTRIAN PRIORITY ZONES AT EDGELEY BOULEVARD



Portage Parkway Class EA Design Considerations

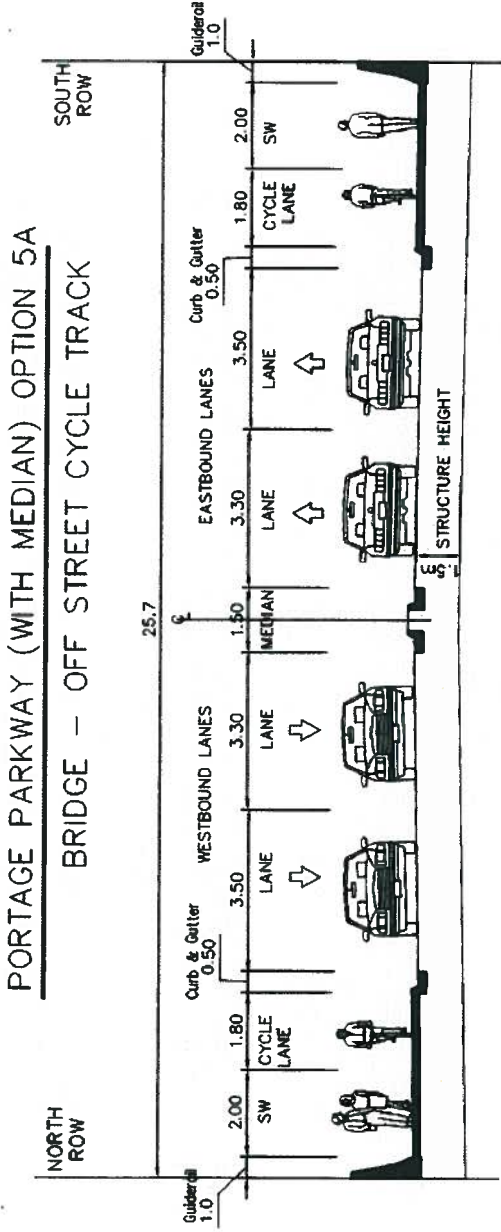
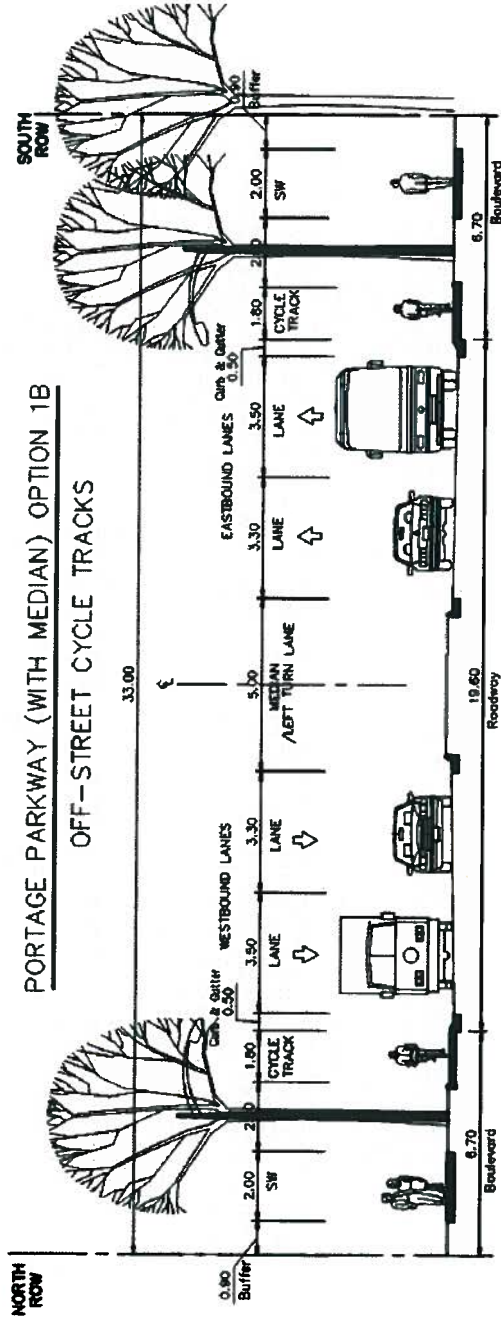


# Design Evaluation Criteria

Criteria considered for assessment of alternative design concepts:

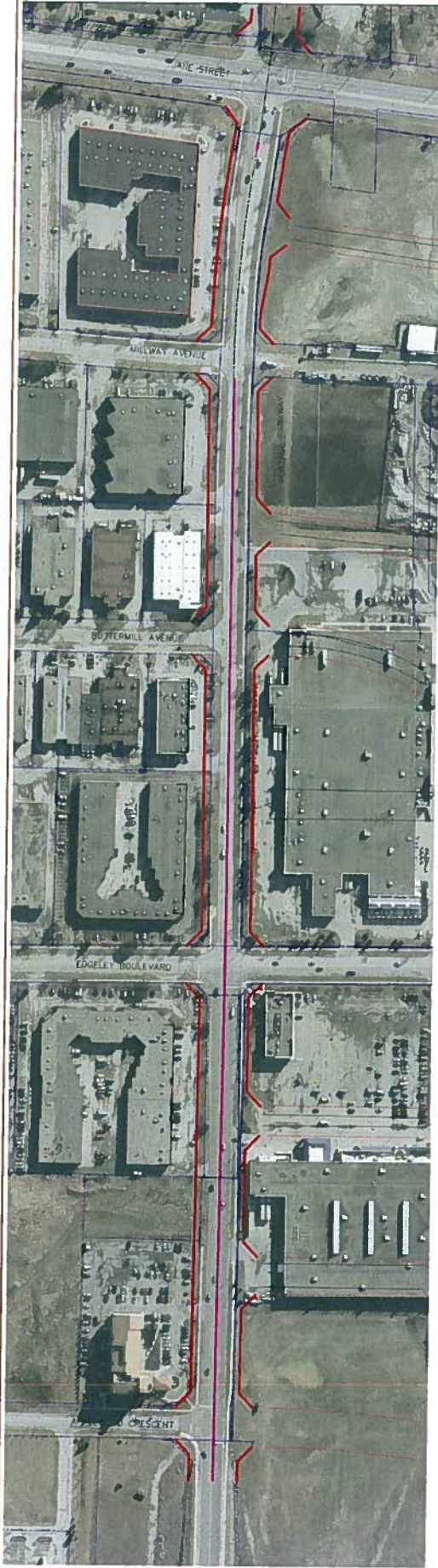
- Cultural**
  - Archaeology
  - Built Heritage and Cultural Landscapes
- Economic**
  - Capital Costs
- Implementation**
  - Regulatory Framework
  - Staging
  - Municipal Servicing Coordination
- Infrastructure Planning**
  - Alternative Transportation
  - Streetscape
  - Level of Service
  - Network Capacity
  - Planning Policy (Official/Secondary Plans, Transportation Master Plans)
  - Stormwater Management
  - Traffic Safety
- Natural**
  - Aquatic
  - Avian and Wildlife
  - Natural Areas
  - Species at Risk
  - Vegetation
  - Watercourses
- Social**
  - Air Quality
  - Businesses
  - Emergency Services
  - Noise
  - Property Access

# Typical Cross-sections

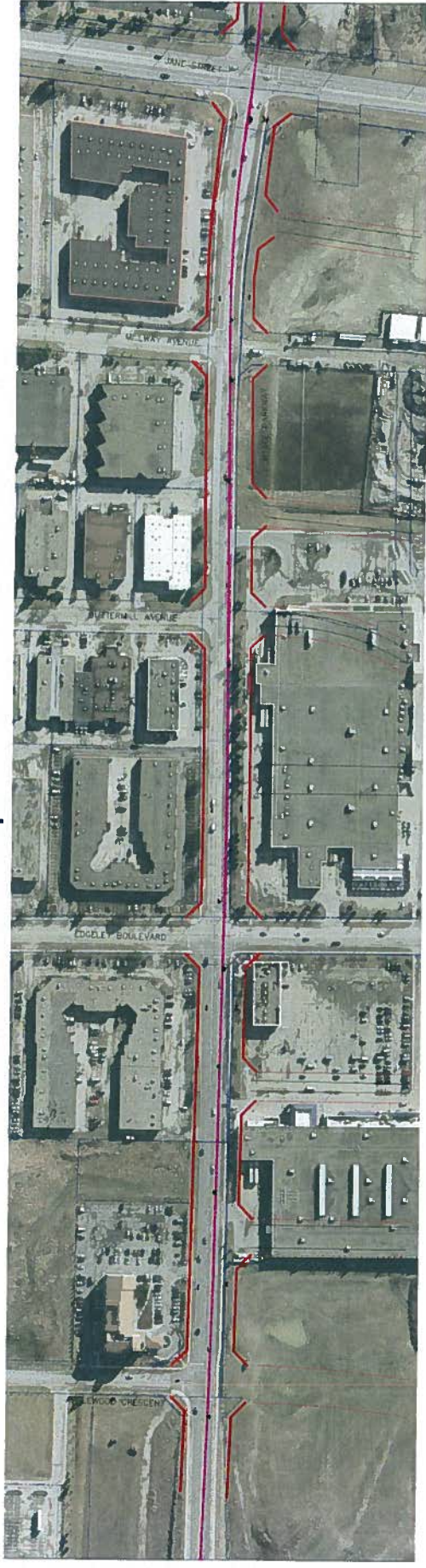




# Portage Parkway Widening Options West of Jane Street



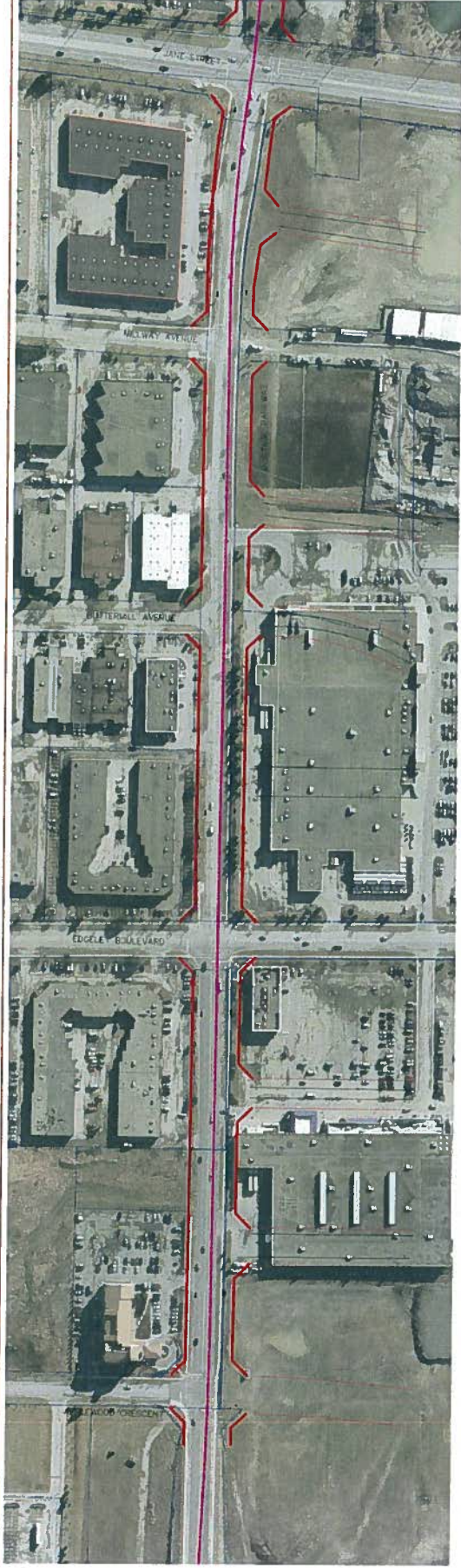
Option 1



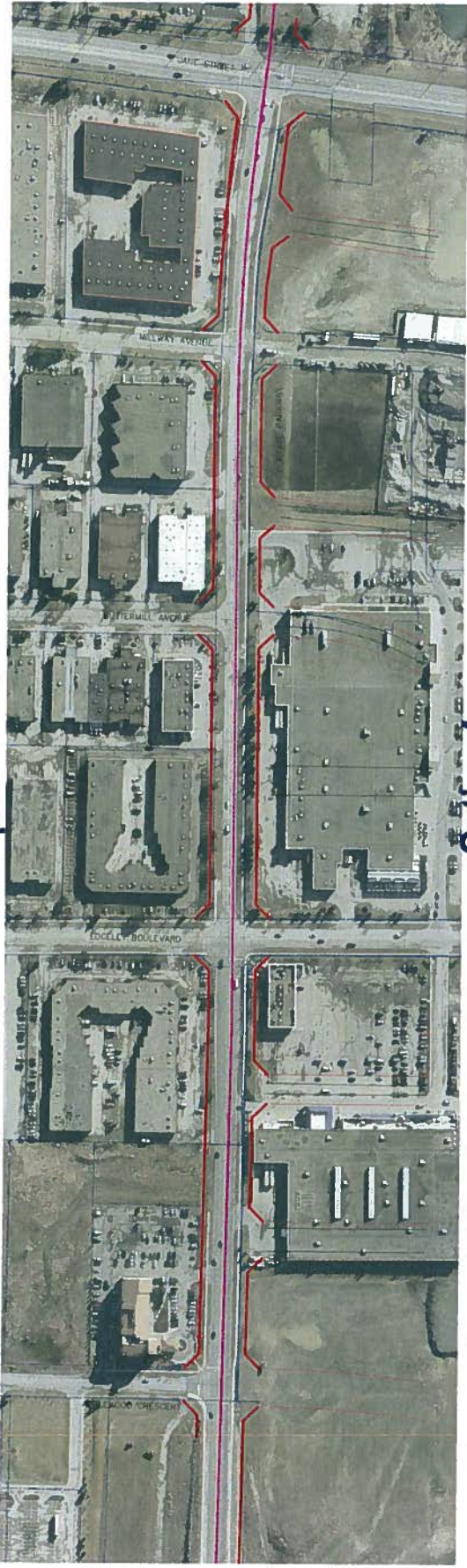
Option 2



# Portage Parkway Widening Options West of Jane Street



Option 3



Option 4



# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
<b>Cultural</b>					
<b>Archaeological Features</b>	●	●	●	●	●
<b>Built Heritage Resources and Cultural Heritage Landscapes</b>	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	◐	◐	◐	◐
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	●	●	●	●
<b>Staging</b>	●	◐	◐	◐	◐
<b>Municipal Servicing Coordination</b>	●	◐	◐	◐	◐

# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
Infrastructure Planning					
Alternative Transportation	○	●	●	●	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, VMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	○	●	●	●	●
Natural					
Aquatic	●	●	●	●	●
Avian and Wildlife	●	●	●	●	●
Natural Areas	●	●	●	●	●
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	●	●	●	●

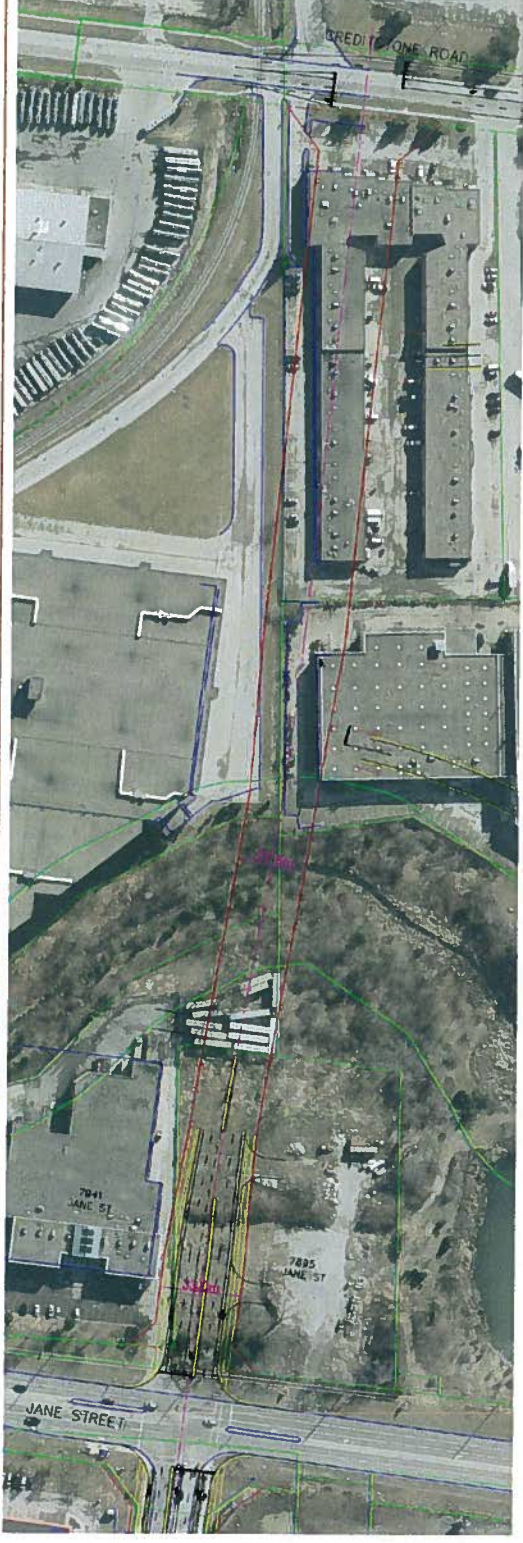


# Evaluation of Widening West of Jane Street

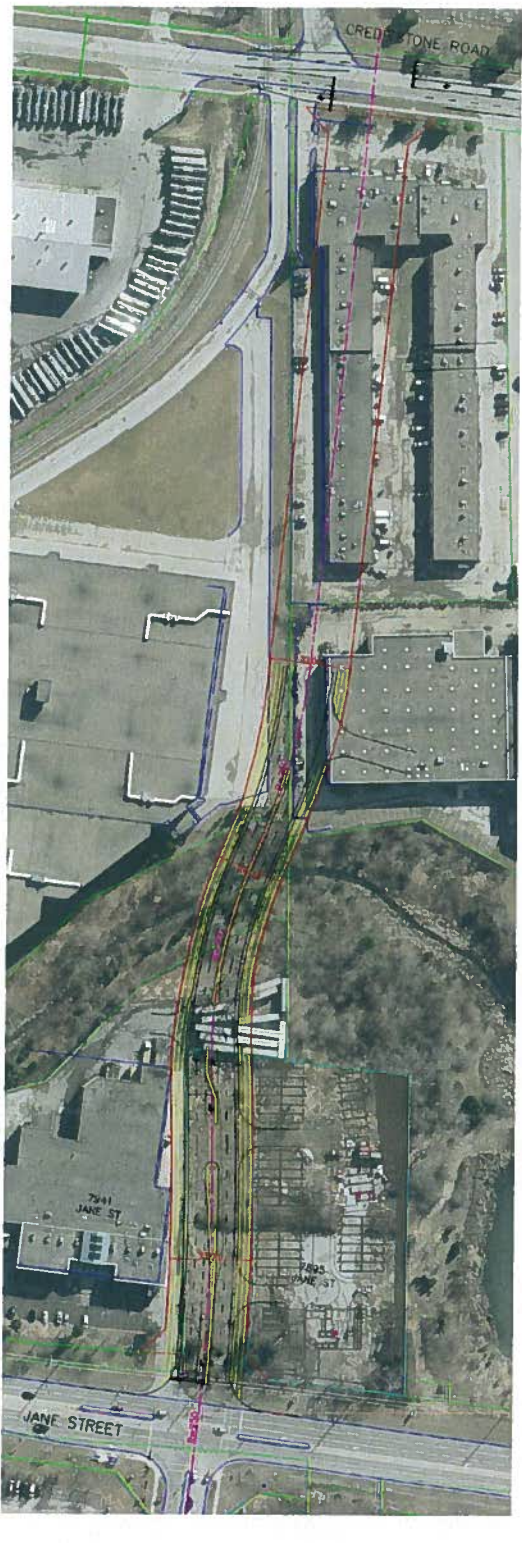
Technical Criteria	Do Nothing	1	2	3	4
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended



# Portage Parkway Road Extension East of Jane Street



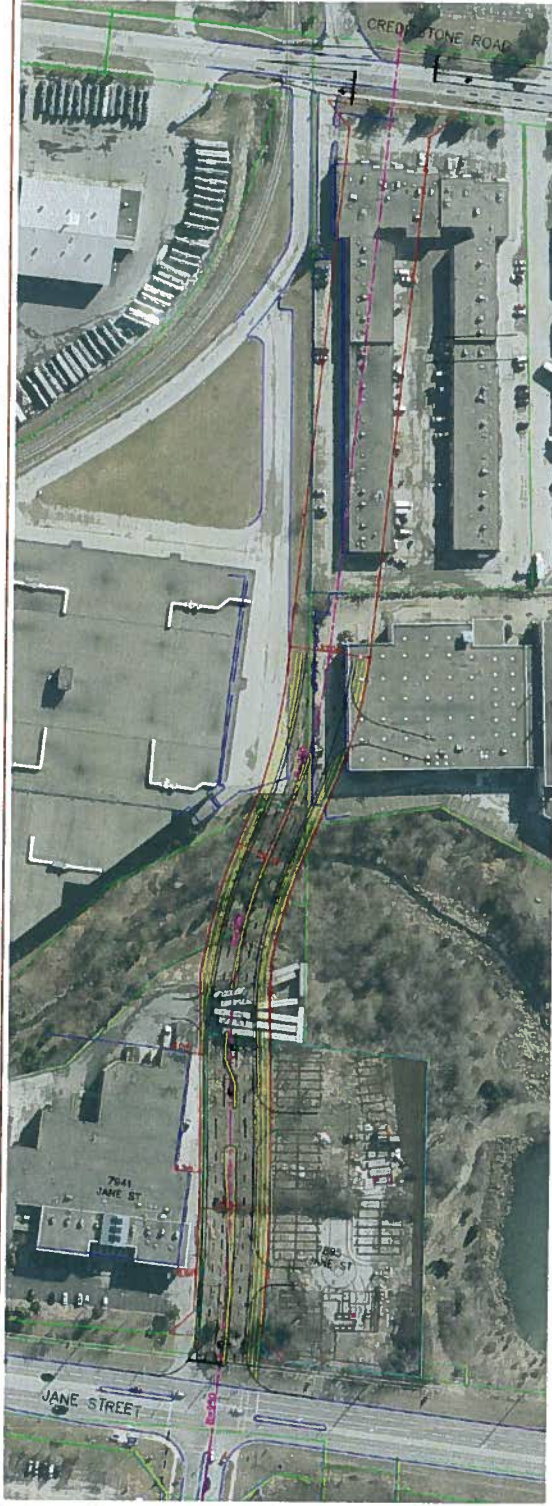
Option 1



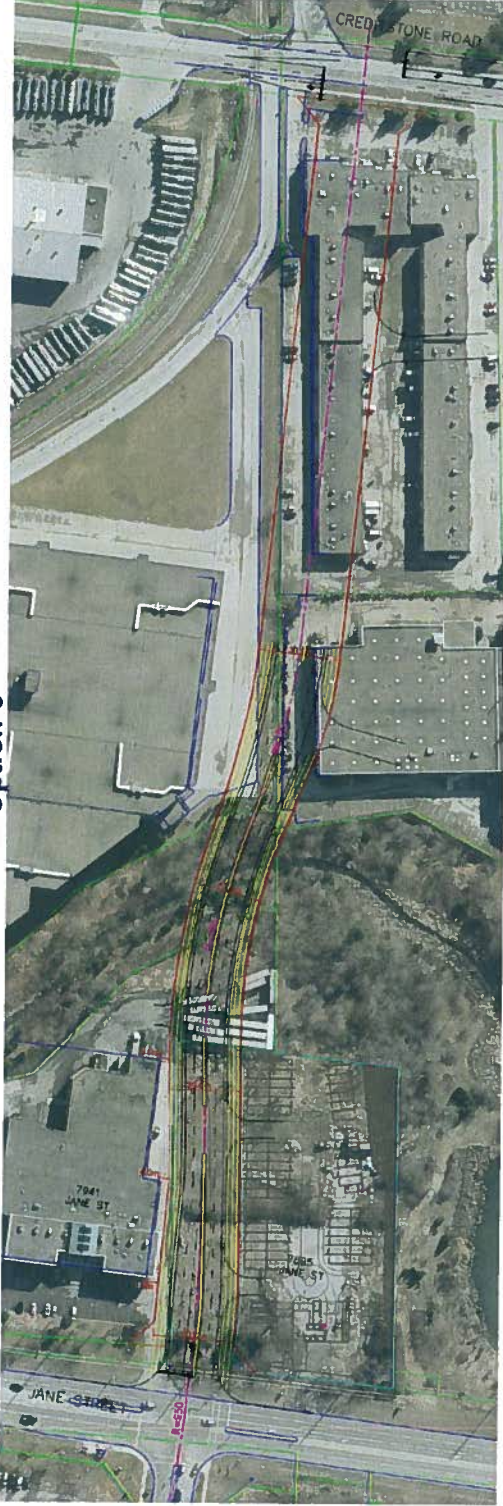
Option 2



# Portage Parkway Road Extension East of Jane Street



Option 3



Option 4



# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Cultural</b>					
<b>Archaeological Features</b>	●	◐	◐	◐	◐
<b>Built Heritage Resources (BHRs) and Cultural Heritage Landscapes (CHLs)</b> BHRs – none CHLs – none	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	◐	◐	◐	◐
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	◐	◐	◐	◐
<b>Staging</b>	●	◐	◐	◐	◐
<b>Municipal Servicing Coordination</b>	●	◐	◐	◐	◐



# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
Infrastructure Planning	○	●	●	◐	●
Alternative Transportation	○	●	●	◐	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, YMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	●	●	●	●	●
<b>Natural</b>					
Aquatic	●	◐	◐	◐	◐
Avian and Wildlife	●	◐	◐	◐	◐
Natural Areas Cultural Meadow (CUM1-1) Deciduous Forest (FOD)	●	◐	◐	◐	◐
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	◐	◐	◐	◐

# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate areas subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended

# Black Creek Channel Crossing

## Creek Crossing Requirements:

### Hydraulic flow of creek

- ❑ A minimum opening of 8.6 m wide x 2.4 m high (100 year storm flow)
- ❑ Regional Storm level 202.41 (approx. 2m below level of bank)

### Geomorphology (meander)

- ❑ ~76 m wide at bridge crossing (not representative of current conditions)
- ❑ Recommended crossing at least 8 m wide (2x average bankfull width)

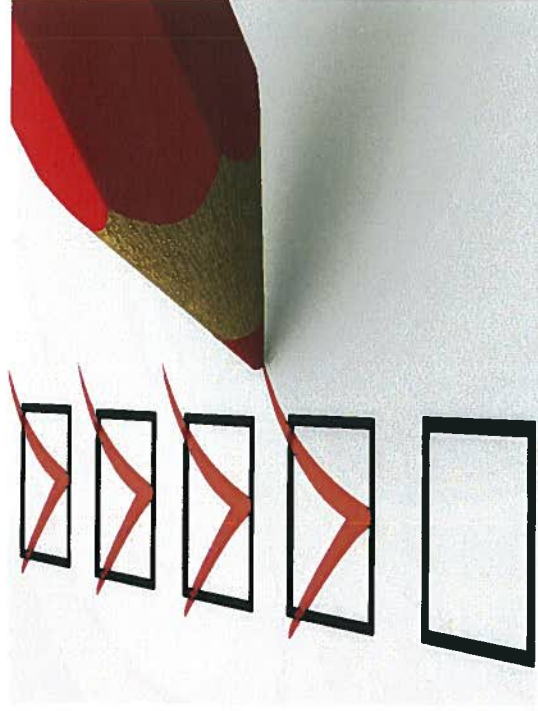
### Profile of Portage Parkway

- ❑ Naturally follows the banks of the creek

### Proposed Structure

- ❑ ~35 m clear span bridge of creek
- ❑ Exceeding hydraulic and geomorphic recommendations.

# Next Steps



- Review your input
- Refine and/or select preferred design concepts
- Prepare preliminary design
- File Environmental Study Report on public record



# What do you think?

Please share your comments with either Project Manager by  
March 23, 2016:

**Marta Roias, RPP**

City of Vaughan Project Manager

2141 Major Mackenzie Drive

Vaughan, ON L6A 1T1

T: 905-832-8585 Ext. 8026

F: 905-832-6145

E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

**Stephen Keen, P.Eng.**

CIMA+ Project Manager

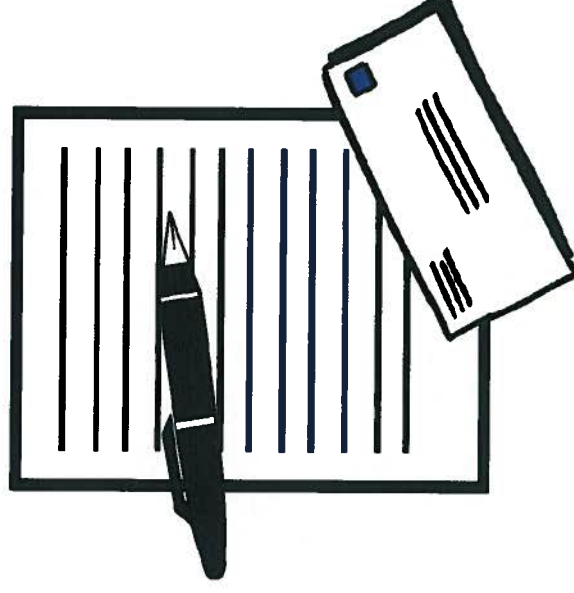
3027 Harvester Road, Suite 400

Burlington, ON L7N 3G7

T: 289-288-0287 Ext. 6834

F: 289-288-0285

E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)



Thank you for attending this PIC

## MINUTES OF MEETING

- CLIENT** : City of Vaughan (City)
- PROJECT** : Environmental Assessment (EA) Study for  
Part A - Portage Parkway Widening, Applewood Crescent to Jane Street  
Part B - Portage Parkway Extension, Jane Street to Creditstone Road
- MEETING #3** : Meeting with Technical Agencies Committee (TAC)
- DATE OF MEETING** : February 2, 2016, 10:00 am
- LOCATION** : City of Vaughan, City Hall, 2141 Major Mackenzie Drive,  
2<sup>nd</sup> Floor, Committee Room 245
- TAC ATTENDEES** : Powerstream (Tony D'Onofrio)  
Toronto Transit Commission (Srdjana Jaksic)  
York Catholic District School Board (Christine Hyde)  
York Region (Shahid Matloob)  
York Region (Vi Bui)  
York Region Rapid Transit Corporation (Stuart Murray)
- City of Vaughan (Saad Yousaf, , Vince Suppa)
- PROJECT TEAM** : City of Vaughan (Marta Roias, Selma Hubjer)  
CIMA Canada Inc. (CIMA+; Stephen Keen, Sonya Kapusin)
- C.C. TO** : Metrolinx (Lisa Salsberg)  
Ministry of Environment and Climate Change (Emilee O'Leary)  
Powerstream (Brian O'Gay, Jiacheng Zhang, Joe Crozier, Patrick Leung)  
Toronto and Region Conservation (Suzanne Bevan)
- York Region (Brian Titherington, Richard Hui)  
York Region Rapid Transit Corporation (Carolyn Ryall)
- City of Vaughan (Andrew Pearce, Daniel Rende, Jennifer Cappola-Logullo, Amy Roots, Gerardo Paez Alonso Danny Woo, Dino Macchiusi, Frank Facchini, , Paul Salerno, Saad Yousaf, Stephen Lue, Steven Dixon, Tim Simmonds)

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Note: If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

---

**DISCUSSION TOPICS****ACTION  
BY**

---

**1 STUDY STATUS UPDATE**

- The City welcomed all in attendance and facilitated roundtable introductions. Attendees were asked to sign-in.
- The City explained the Class EA study has been accelerated for completion by June, 2016.

**2 DRAFT PUBLIC INFORMATION CENTRE (PIC) BOARDS**

- CIMA+ reviewed the agenda, which was distributed to all present, and presented PIC information boards in PowerPoint. The intent of the presentation was to review the PIC content and discuss most of the agenda items.
- During the presentation (attached), the City clarified that Part A and B are running concurrently, and the limits for Part A include the Jane Street intersection to the west bank of Black Creek.

**3 SUMMARY OF STUDIES COMPLETED SINCE PIC 1**

- The Project Team was advised to be aware of mitigation for Butternut Trees. The Toronto Transit Commission (TTC) found a butternut tree south of the study area.

CIMA+

**4 ALTERNATIVE DESIGN CONCEPTS FOR THE RECOMMENDED PLANNING SOLUTION**

- The cross-section for the bridge will show cycle tracks versus cycle lanes.
- CIMA+ explained that a change to the cross-section shown in the Vaughan Metropolitan Centre (VMC) streetscape and open space plan was based on consideration for the north side as an active industrial area and the south side as a changing environment; Making the corridor "citified" may not be appropriate for the industrial area.
- In response to questions, CIMA+ clarified that 3.5m curb lanes are adequate, the centre median will be utilized mostly for back to back turns (i.e., no planting planned for the median), and widening to the north was not a feasible design option.
- Option 2 has impacts to Walmart and Lowe's loading docks.
- The Project Team is working on the wording for descriptions of design alternatives.

CIMA+

**5 EVALUATION OF ALTERNATIVE DESIGNS AND ALIGNMENTS**

- CIMA+ will add a legend to the evaluation matrices.
- The Project Team was advised that stormwater management on Millway Avenue close to Highway 7 is questioning historic assumptions. TMIG is undertaking a comprehensive review of stormwater management in the area and highlighting challenges with an undersized pond. A concern was raised for the potential impact of road widening introducing further constraints to the stormwater management system. The City was requested to take care when determining what may or may not need to happen. The City explained that it is considering the stormwater optimization study and coordination with this Class EA is ongoing. The Project Team will need to meet with the Toronto and Region Conservation Authority (TRCA) in the near future.

CIMA+

**6 PRELIMINARY PREFERRED DESIGN FOR WIDENING AND EXTENSION**

- CIMA+ noted the opportunity to review the preliminary preferred design before the PIC.



DISCUSSION TOPICS	ACTION BY
<ul style="list-style-type: none"> <li>Please note that the stated design for the Creek Crossing was mistakenly stated as being a 35m span bridge – this is wrong and options are being developed and evaluated revisited.</li> </ul>	
<p><b>8 BENEFITS, IMPACTS AND MITIGATION MEASURES ASSOCIATED WITH PREFERRED DESIGN</b></p>	
<ul style="list-style-type: none"> <li>Show extended roads in the VMC area.</li> <li>;.Clarify plans for rapid transitway on Jane Street which may include changes to existing access points; Include statement in the Environmental Study Report (ESR) addressing provision for any potential future restrictions on access to Jane Street north of Portage Parkway as applicable</li> <li>Consider greater signage to help alleviate concerns regarding customer way-finding/access; Identify Millway Avenue alignment and York Region Transit station; TTC will forward information.</li> <li>Land acquisition (subject to separate process) on north property, east of Jane Street, will follow Class EA completion; Land for this section may not need to be purchased for 10 years; The Region would like a traffic control plan for this location to be included in the ultimate condition; Details need to be worked out.</li> <li>Consider how to implement interface between residential and employment land uses on Creditstone Road; Could lead to future conflict between employment and residential areas.</li> </ul>	<p>CIMA+</p> <p>TTC</p>
<p><b>9 NEXT STEPS</b></p>	
<ul style="list-style-type: none"> <li>Comments on the PIC boards were requested by February 16, 2016. The Project Team will circulate copies of the presentation with the preliminary preferred plan to the Technical Agencies Committee.</li> </ul>	<p>CIMA+</p> <p>TAC</p>



City of Vaughan  
**Portage Parkway  
Environmental Assessment**  
Technical Agencies Committee  
Meeting No. 3

February 2, 2016



# Agenda

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- Study Status Update
- Draft Public Information Centre (PIC) Boards
- Summary of Studies Completed Since PIC 1
- Alternative Design Concepts for the Recommended Planning Solution
- Evaluation of Alternative Designs and Alignments
- Preliminary Preferred Design for Widening and Extension
- Benefits, Impacts and Mitigation Measures Associated with Preferred Design
- Next Steps – Comments on PIC Boards by February 16, 2016



City of Vaughan

# Portage Parkway Environmental Assessment

Public Information Centre No. 2

March 9, 2016

**CIWA**  
Partners in excellence



# Welcome to the Public Information Centre

This Public Information Centre (PIC) is the second of two PICs held for the Environmental Assessment (EA) Study for Portage Parkway Widening and Easterly Extension to Creditstone Road.

The first PIC was held in November, 2015 to obtain public input on:

- Background to the study and existing conditions of the study area
- Transportation problems or opportunities and solutions

At this PIC, we welcome your input on:

- Design concepts for the preferred solutions
- Potential effects of the project and ways to avoid or minimize impacts

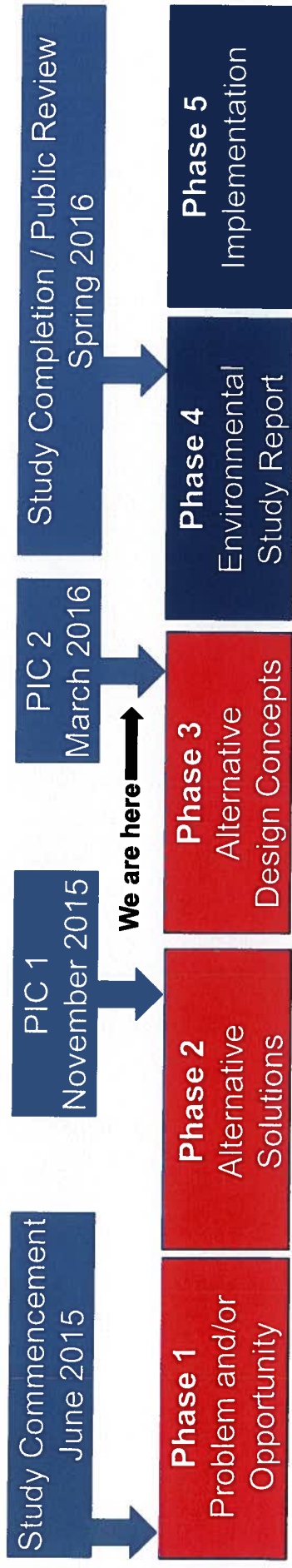
You are invited to:

- Review the information boards
- Ask questions and discuss your comments with the study team
- Fill out and submit a comment sheet
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- Visit the project website:

[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Class-EA-Study-for-Portage-Parkway.aspx)

# Class EA Process

- This project is subject to the Municipal Engineers Association **Municipal Class Environmental Assessment** (2000 as amended in 2007 and 2011).
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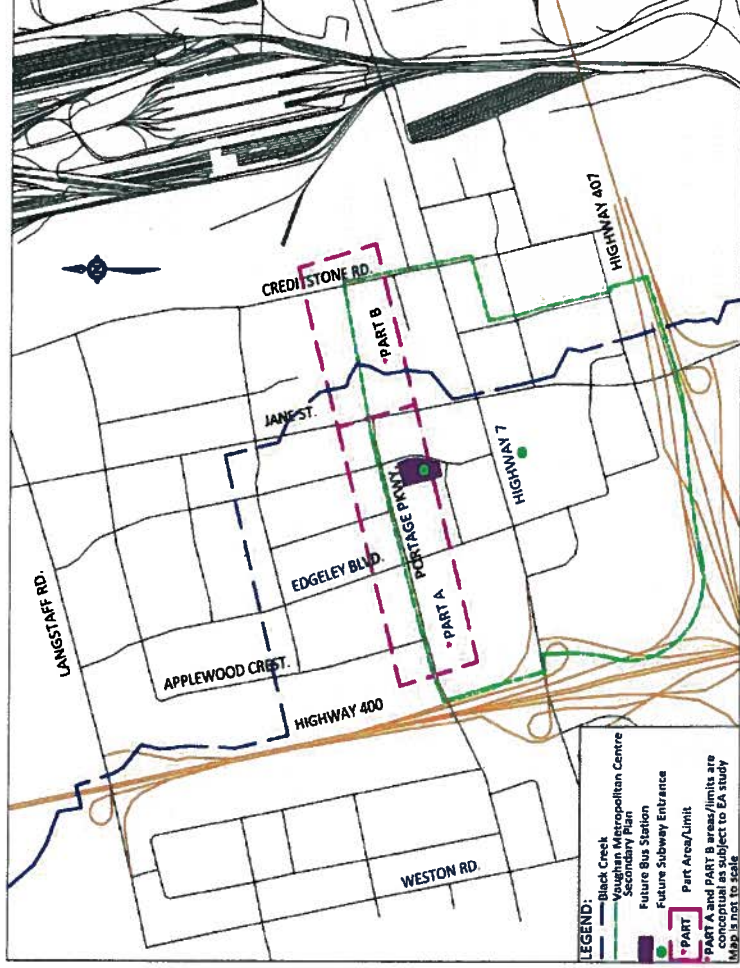


- The City's Transportation Master Plan, having satisfied Phases 1 and 2, initiated the planning and design process for the Portage Parkway Widening and Easterly Extension to Creditstone Road.
- This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
  - Part A - Portage Parkway Widening from Applewood Crescent to Jane Street
  - Part B - Portage Parkway Extension from Jane Street to Creditstone Road.



# Study Area

- Improvements to Portage Parkway are being considered and evaluated as two interrelated Parts:**
- ❑ **Widening west of Jane Street** – Widening of Portage Parkway from two to four lanes from Applewood Crescent to Jane Street.
  - ❑ **Extension east of Jane Street** – Extension of Portage Parkway from Jane Street crossing the Black Creek Channel to Creditstone Road.



**The total distance of the Study Area corridor is approximately 1.5 km.**

# Summary of PIC No. 1 - Future Traffic Conditions



**Future conditions  
without widening**



**Future conditions  
with widening**





# Summary of PIC No. 1

## **Problem and Opportunity**

Transportation improvements are needed to support growth within the VMC and surrounding area. Widening and extending Portage Parkway is an opportunity to:

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,
- Address travel demand in the VMC and surrounding area,
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,
- Improve connections to local and regional infrastructure,
- Support transit-oriented nodes and corridors,
- Enhance transit ridership, cycling and walking, and
- Provide an alternate to Highway 7.

## **Preferred Solution**

- Travel Demand Management Initiatives are being implemented by the City as a separate strategy
- Alternative Modes of Transportation, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- Localized Intersection and Operational Improvements (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road (e.g., new 4 lane roadway)



# Summary of Public Comments

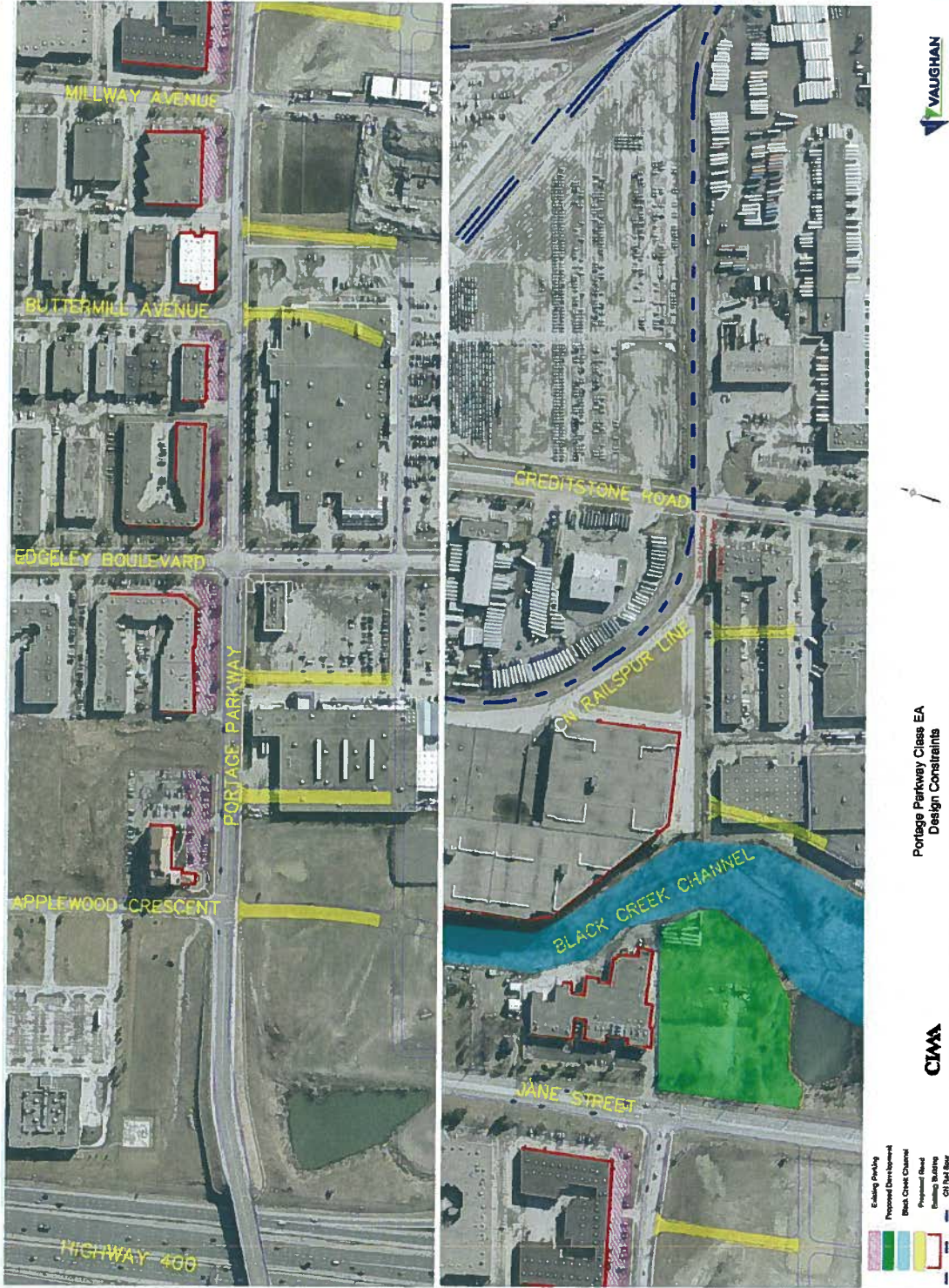
- + Add a dedicated truck route to the Portage Parkway Extension
- + Add an advance green or a turning arrow for the left turn from Pennsylvania Avenue to southbound Applewood Crescent, and from eastbound Portage Parkway to northbound Applewood Crescent
- + Consider compatibility of staged intersection improvements and driveways as part of new developments with the ultimate design for Portage Parkway
- + Consider landscaping
- + Consider proximity of the road extension to the existing CN Rail spur
- + Consider the effects of property needs for right of way
  - + Consider functionality of buildings and businesses
  - + Consider property access, circulation and operations
- + Consider the future viability of the area for industry
- + Effects on the VMC subway station and Millway Avenue
- + Extend the sidewalk on Applewood Crescent north of Portage Parkway
- + Extend Portage Parkway to Keele Street over the CN property
- + Study the extension of Langstaff Road from Creditstone Road to Keele Street

# Summary of Environmental Investigations

---

- + Fluvial Geomorphic Assessment
- + Heritage Impact Assessment
- + Natural Environment Site Reconnaissance
- + Phase 1 Environmental Site Assessment
- + Stage 1 Archaeological Assessment
- + Stormwater Constraints

# Design Constraints





# Design Considerations



VAC STREETSCAPE AND CRED SPACE PLAN (2015) FOR CONSIDERATION OF:  
 -TRANSITIONAL ZONE BETWEEN URBAN CENTRE AND EMPLOYMENT LANDS  
 -PORTAGE PARKWAY AS THE LANDSCAPE EDGE FOR THE VAC  
 -PEDESTRIAN PRIORITY ZONES AT EDGELY BOULEVARD



Proposed Development  
 Black Creek Channel  
 Proposed Road  
 CN Rail Spur

Portage Parkway Class EA  
 Design Considerations

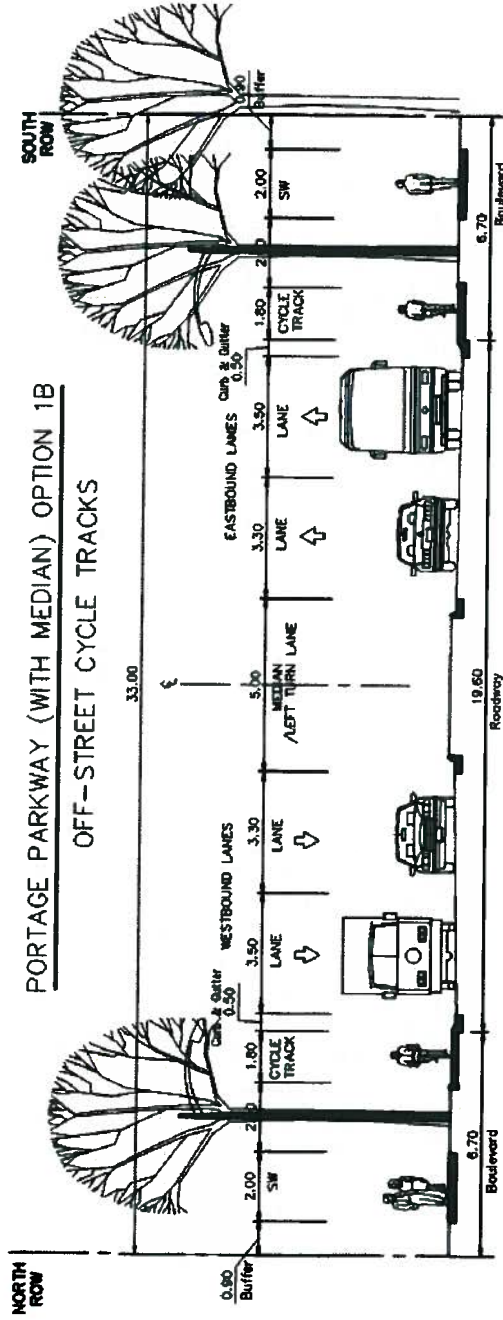


# Design Evaluation Criteria

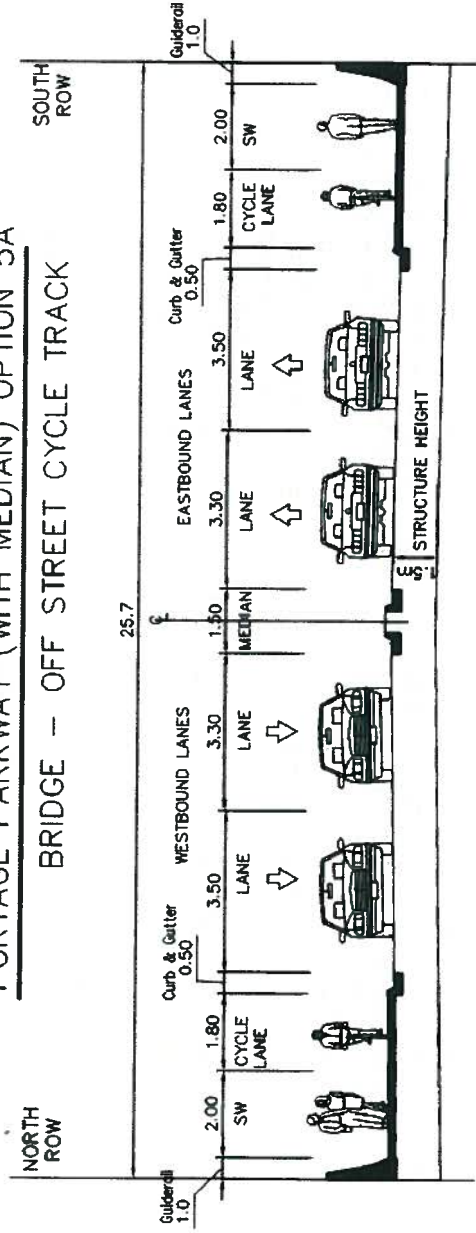
Criteria considered for assessment of alternative design concepts:

- Cultural**
  - Archaeology
  - Built Heritage and Cultural Landscapes
- Economic**
  - Capital Costs
- Implementation**
  - Regulatory Framework
  - Staging
  - Municipal Servicing Coordination
- Infrastructure Planning**
  - Alternative Transportation
  - Streetscape
  - Level of Service
  - Network Capacity
  - Planning Policy (Official/Secondary Plans, Transportation Master Plans)
  - Stormwater Management
  - Traffic Safety
- Natural**
  - Aquatic
  - Avian and Wildlife
  - Natural Areas
  - Species at Risk
  - Vegetation
  - Watercourses
- Social**
  - Air Quality
  - Businesses
  - Emergency Services
  - Noise
  - Property Access

# Typical Cross-sections

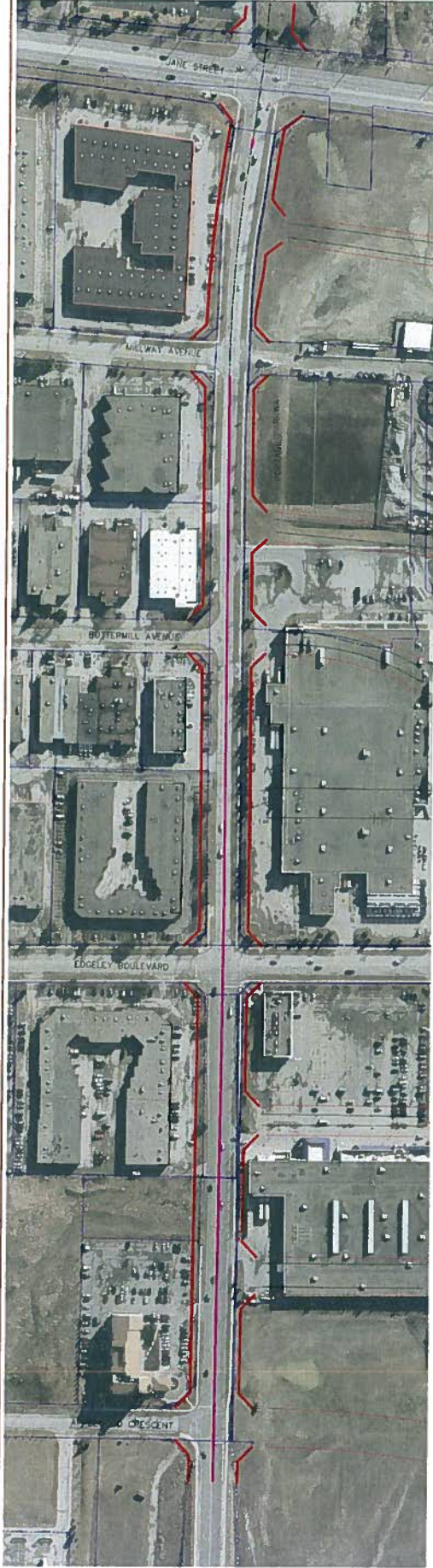


## PORTAGE PARKWAY (WITH MEDIAN) OPTION 5A BRIDGE - OFF STREET CYCLE TRACK

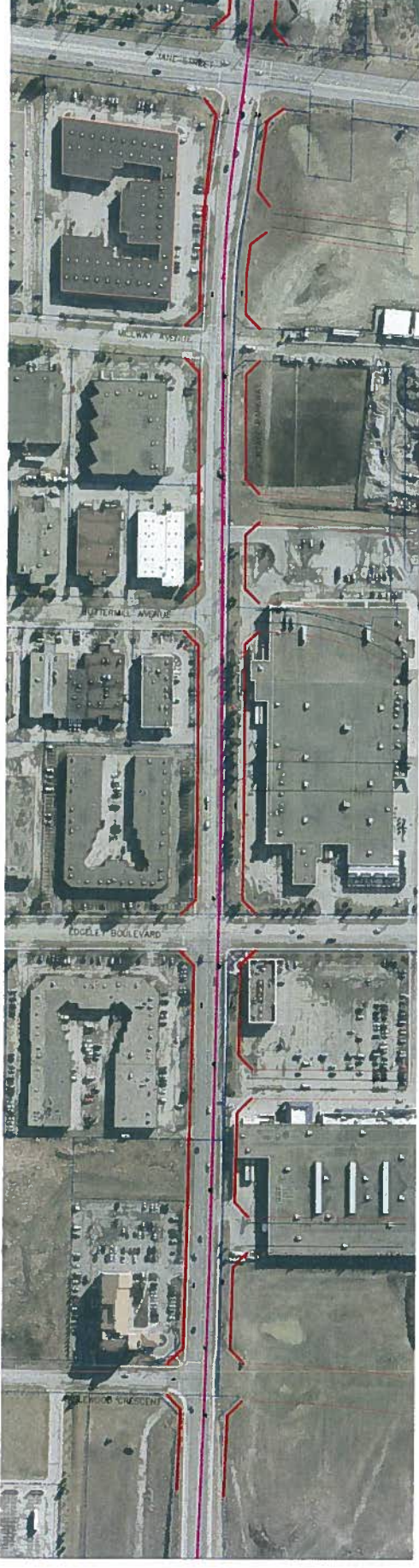




# Portage Parkway Widening Options West of Jane Street



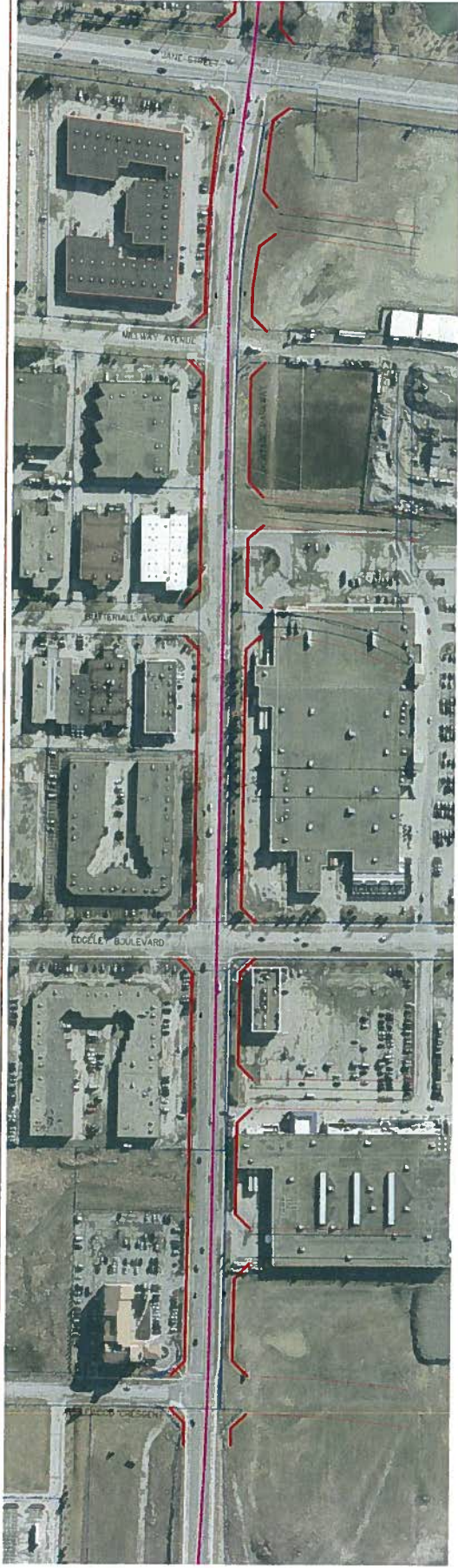
Option 1



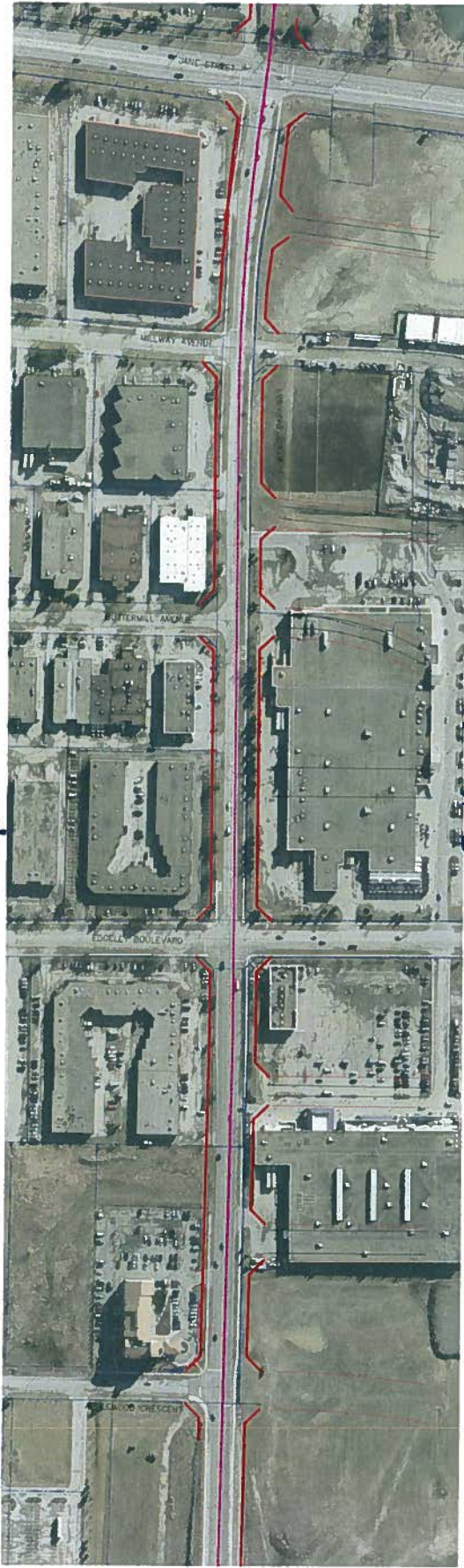
Option 2



# Portage Parkway Widening Options West of Jane Street



Option 3



Option 4



# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
<b>Cultural</b>					
<b>Archaeological Features</b>	●	●	●	●	●
<b>Built Heritage Resources and Cultural Heritage Landscapes</b>	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	◐	◐	◐	◐
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	●	●	●	●
<b>Staging</b>	●	◐	◐	◐	◐
<b>Municipal Servicing Coordination</b>	●	◐	◐	◐	◐



# Evaluation of Widening West of Jane Street

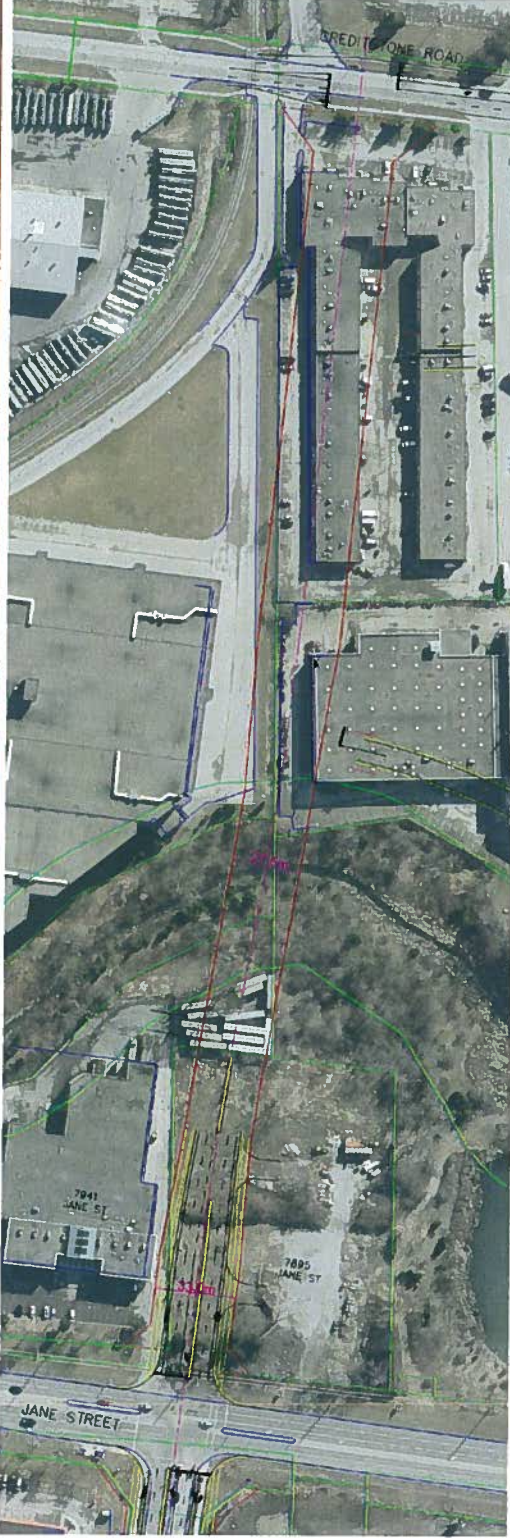
Technical Criteria	Do Nothing	1	2	3	4
Infrastructure Planning					
Alternative Transportation	○	●	●	●	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, VMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	○	●	●	●	●
Natural					
Aquatic	●	●	●	●	●
Avian and Wildlife	●	●	●	●	●
Natural Areas	●	●	●	●	●
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	●	●	●	●

# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended



# Portage Parkway Road Extension East of Jane Street



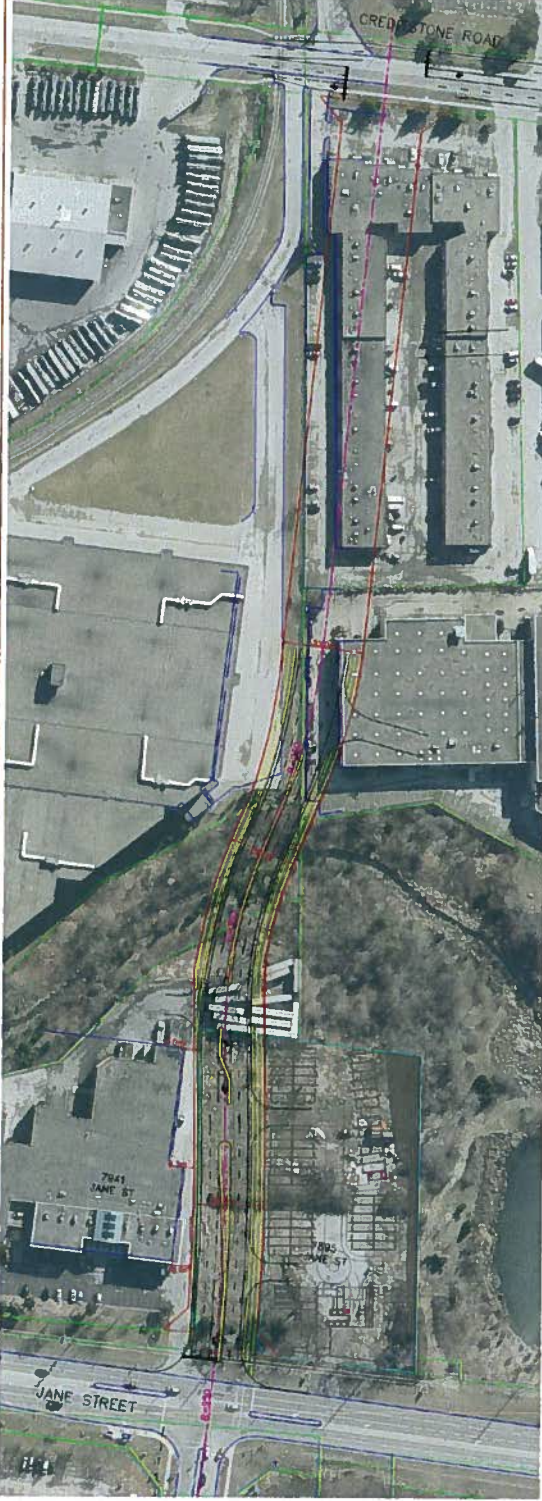
Option 1



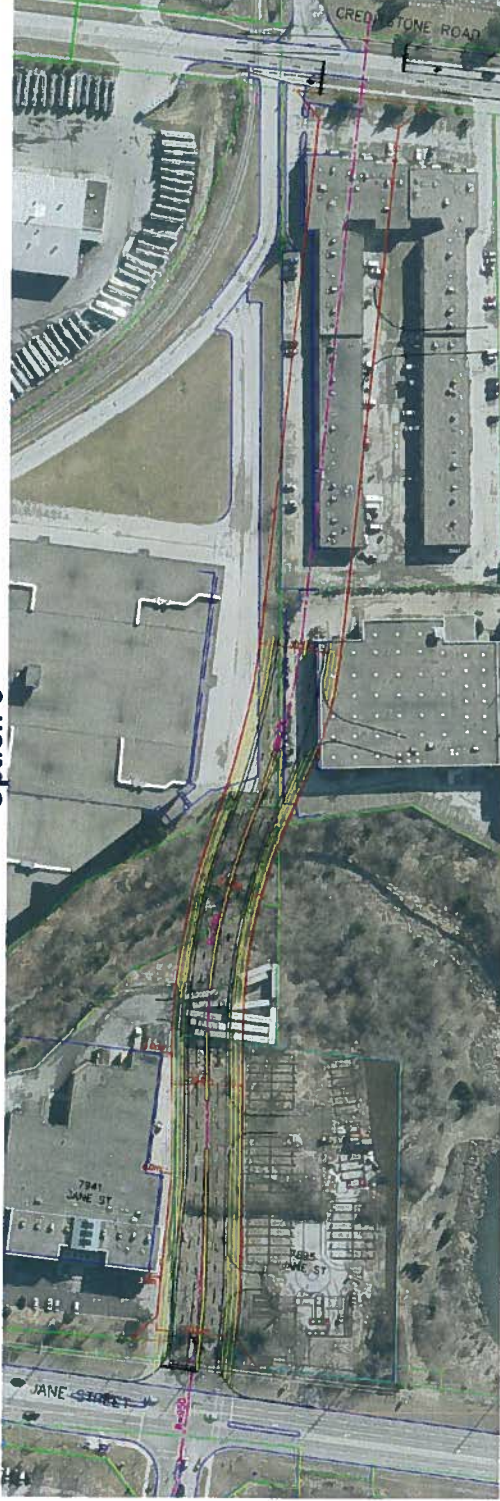
Option 2



# Portage Parkway Road Extension East of Jane Street



Option 3



Option 4

# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Cultural</b>					
<b>Archaeological Features</b>	●	⦿	⦿	⦿	⦿
<b>Built Heritage Resources (BHRs) and Cultural Heritage Landscapes (CHLs)</b> BHRs - none CHLs - none	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	⦿	⦿	⦿	⦿
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	⦿	⦿	⦿	⦿
<b>Staging</b>	●	⦿	⦿	⦿	⦿
<b>Municipal Servicing Coordination</b>	●	⦿	⦿	⦿	⦿



# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
Infrastructure Planning					
Alternative Transportation	○	●	●	◐	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, YMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	●	●	●	●	●
Natural					
Aquatic	●	◐	◐	◐	◐
Avian and Wildlife	●	◐	◐	◐	◐
Natural Areas Cultural Meadow (CUM1-1) Deciduous Forest (FOD)	●	◐	◐	◐	◐
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	◐	◐	◐	◐



# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate areas subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended

# Black Creek Channel Crossing

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## Creek Crossing Requirements:

### Hydraulic flow of creek

- A minimum opening of 8.6 m wide x 2.4 m high (100 year storm flow)
- Regional Storm level 202.41 (approx. 2m below level of bank)

### Geomorphology (meander)

- ~76 m wide at bridge crossing (not representative of current conditions)
- Recommended crossing at least 8 m wide (2x average bankfull width)

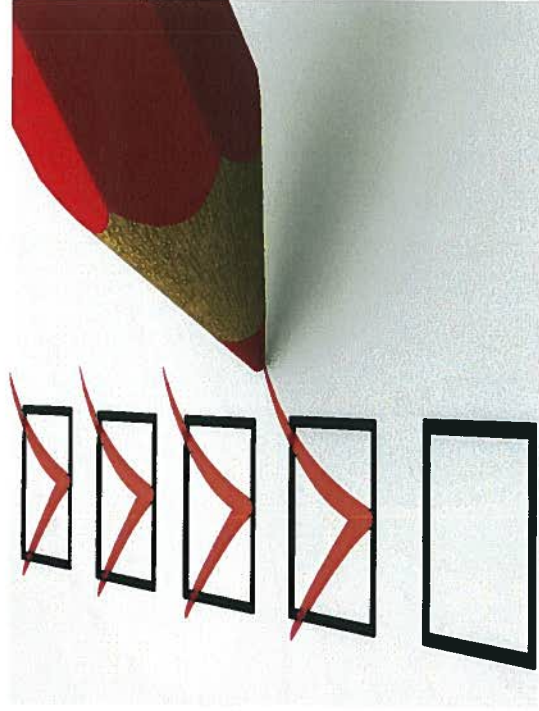
### Profile of Portage Parkway

- Naturally follows the banks of the creek

### Proposed Structure

- ~35 m clear span bridge of creek
- Exceeding hydraulic and geomorphic recommendations.

# Next Steps



- Review your input
- Refine and/or select preferred design concepts
- Prepare preliminary design
- File Environmental Study Report on public record

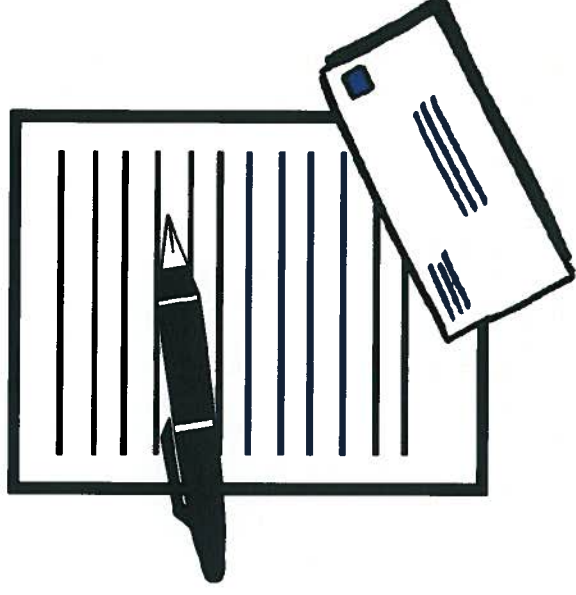


# What do you think?

Please share your comments with either Project Manager by  
March 23, 2016:

**Marta Roias, RPP**  
City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

**Stephen Keen, P.Eng.**  
CIMA+ Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7  
T: 289-288-0287 Ext. 6834  
F: 289-288-0285  
E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)



Thank you for attending this PIC

## MINUTES OF MEETING

**CLIENT** : City of Vaughan

**PROJECT** : Environmental Assessment (EA) Study for  
Part A - Portage Parkway Widening, Applewood Crescent to Jane Street  
Part B - Portage Parkway Extension, Jane Street to Creditstone Road

**MEETING #1** : Meeting with Toronto and Region Conservation Authority (TRCA)

**DATE OF MEETING** : March 07, 2016, 01:30 pm

**LOCATION** : TRCA Head Office (Duffins Room), 101 Exchange Avenue, Vaughan

**TRCA ATTENDEES** : Harsha Gammanpila  
Leslie Piercey  
Suzanne Bevan  
Wilfred Ho

**PROJECT TEAM** : City of Vaughan (City) (Marta Roias, Saad Yousaf, Gerardo Paez Alonso)  
CIMA Canada Inc. (CIMA) (Stephen Keen, Sonya Kapusin, Maram Miri)  
Golder Associate (Golder) (Christopher Davidson, Richard Booth)

**C.C. TO** : Attendees

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**Note:** If you believe that these minutes are lacking in accuracy, please inform the author who will make the necessary changes.

**DISCUSSION TOPICS**

**ACTION  
BY**

1	<b>UPDATE ON STUDY PROGRESS</b>	
	<p>The City provided an update on the Class EA for Portage Parkway. The second Public Information Centre (PIC) for this study was scheduled on March 9, 2016 to present the alternative design concepts for the proposed road widening and extension, including the proposed crossing of the Black Creek Channel. The purpose of this meeting with TRCA was to review the design options under evaluation for the crossing of the Black Creek Channel.</p>	
2	<b>REVIEW OF BACKGROUND STUDIES</b>	
	<p><b>Preliminary Preferred Design</b></p> <ul style="list-style-type: none"> <li>• CIMA provided an overview of the preliminary preferred design for the road widening from Applewood Crescent to Jane Street (Part A) and extension from Jane Street to Creditstone Road (Part B); Reference was made to the preliminary preferred design on a roll plan.</li> </ul> <p><b>Hydraulic Assessment</b></p> <ul style="list-style-type: none"> <li>• Golder presented results of the hydraulic modelling for all design options</li> </ul> <p><b>Natural Environment</b></p> <ul style="list-style-type: none"> <li>• During the meeting, Golder explained the Black Creek crossing in Part B is generally a warmwater watercourse with low flow; The potential for seasonal flooding exists; The walkways may have an effect, but are not a concern for the design options</li> </ul>	
3	<b>REVIEW OF OPTIONS</b>	
	<p>CIMA reviewed the design options for crossing the Black Creek Channel:            (1) Box Culvert (does not meet hydraulic requirements)            (2, 3 and 4) Bridges (1, 2 and 3 Span)            (5) 12m ConSpan Culvert</p> <p>The bridges are estimated to cost \$10.5M with a \$6M premium compared to the culvert options. Golder has assessed Options 2, 3, 4 and 5 as meeting hydraulic requirements.</p> <p>In response to questions, CIMA clarified:</p> <ul style="list-style-type: none"> <li>• Each option can accommodate a minimum 4.5m wide pedestrian trails</li> <li>• The stormwater pond south of the crossing is not assessed as part of this Class EA. Stormwater analysis, hydraulic modelling and plan is being undertaken in context of Black Creek Stormwater Optimization Master Plan as per discussion; meeting was held on May 15, 2015.</li> </ul>	
4	<b>PRELIMINARY EVALUATION</b>	
	<p>CIMA will forward to TRCA for review, an electronic copy of the preliminary evaluation of design options for the Black Creek Channel</p>	CIMA
5	<b>DISCUSSION / NEXT STEPS</b>	
	<p>CIMA and Golder will forward to TRCA for review:</p> <ul style="list-style-type: none"> <li>• Hydraulic Models</li> <li>• Hydraulic, Stormwater Management, and Natural Environment Reports</li> <li>• Preliminary Evaluation of Creek Crossings</li> <li>• Timing of feedback from TRCA will depend on when review documents are received; TRCA requires 20 to 40 business days (approximately two months) for review; TRCA's preference is to review the Draft Environmental Study Report (ESR); TRCA will provide comments in draft form.</li> <li>• It was noted that, a preferred design option for the crossing would not be presented at PIC 2.</li> </ul>	<p>CIMA Golder</p> <p>TRCA</p>







City of Vaughan

# Portage Parkway Environmental Assessment

Public Information Centre No. 2

March 9, 2016



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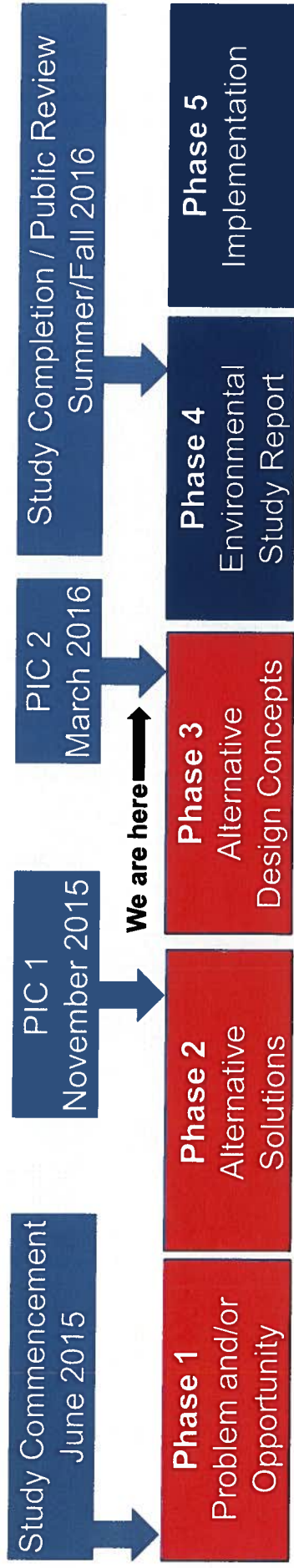
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[www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Class-EA-Study-for-Portage-Parkway.aspx)

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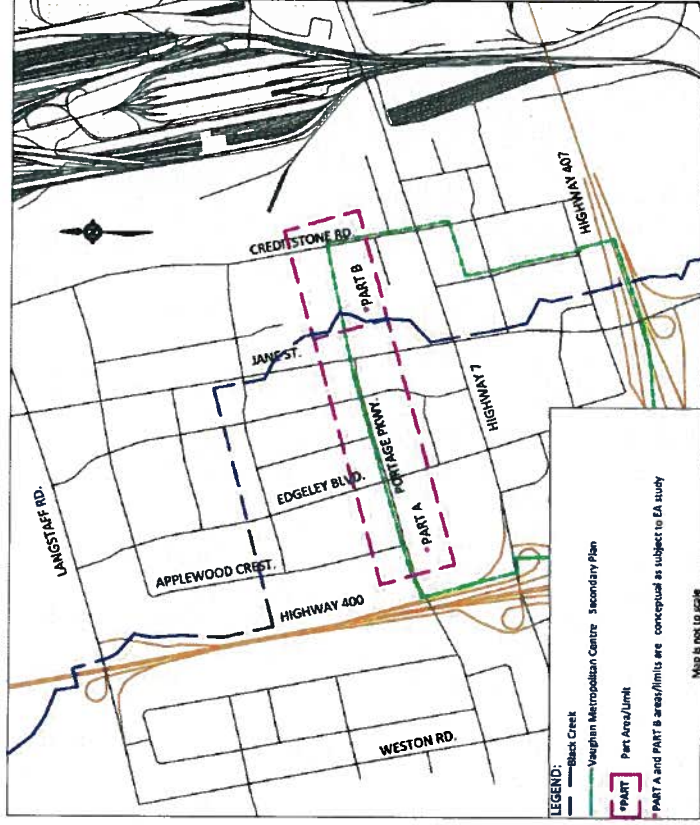
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- ❑ This study, proceeding for two interrelated parts, will complete Phases 3 and 4 for:
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  - ❑ Part A - Portage Parkway Widening from Applewood Crescent to Jane Street.
  - ❑ Part B - Portage Parkway Extension from Jane Street to Creditstone Road.



# Study Area

Completion of the Portage Parkway EA Study is being advanced as two interrelated parts, the limits of which are under refinement through the study process as follows:

- ❑ **Part A\*: Widening west of Jane Street** – Widening of Portage Parkway from two to four lanes from Applewood Crescent to West of the Black Creek Channel.
- ❑ **Part B\*: Extension east of Jane Street** – Extension of Portage Parkway from West of the Black Creek Channel to Creditstone Road.

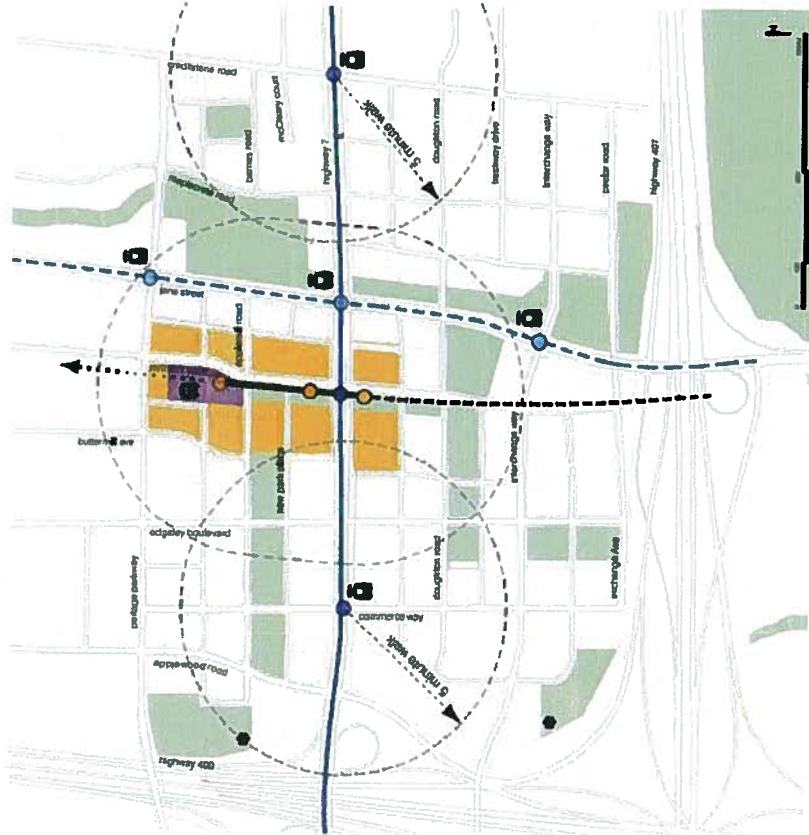


**The total distance of the Study Area corridor is approximately 1.5 km.**

# Vaughan Metropolitan Centre (VMC) Secondary Plan

SCHEDULE B > TRANSIT NETWORK

- LEGEND**
- subway entrances
  - potential Hwy7 rapidway stations
  - potential Jane Street rapidway stations
  - 5 minute walking radii
  - blocks adjacent to subway
  - bus station
  - see Policy 6.3.2
  - spindine subway alignment
  - future spindine subway extension
  - spindine subway station box
  - Highway 7 rapidway
  - potential Jane Street rapidway
  - potential viva stations
  - major parks and open spaces



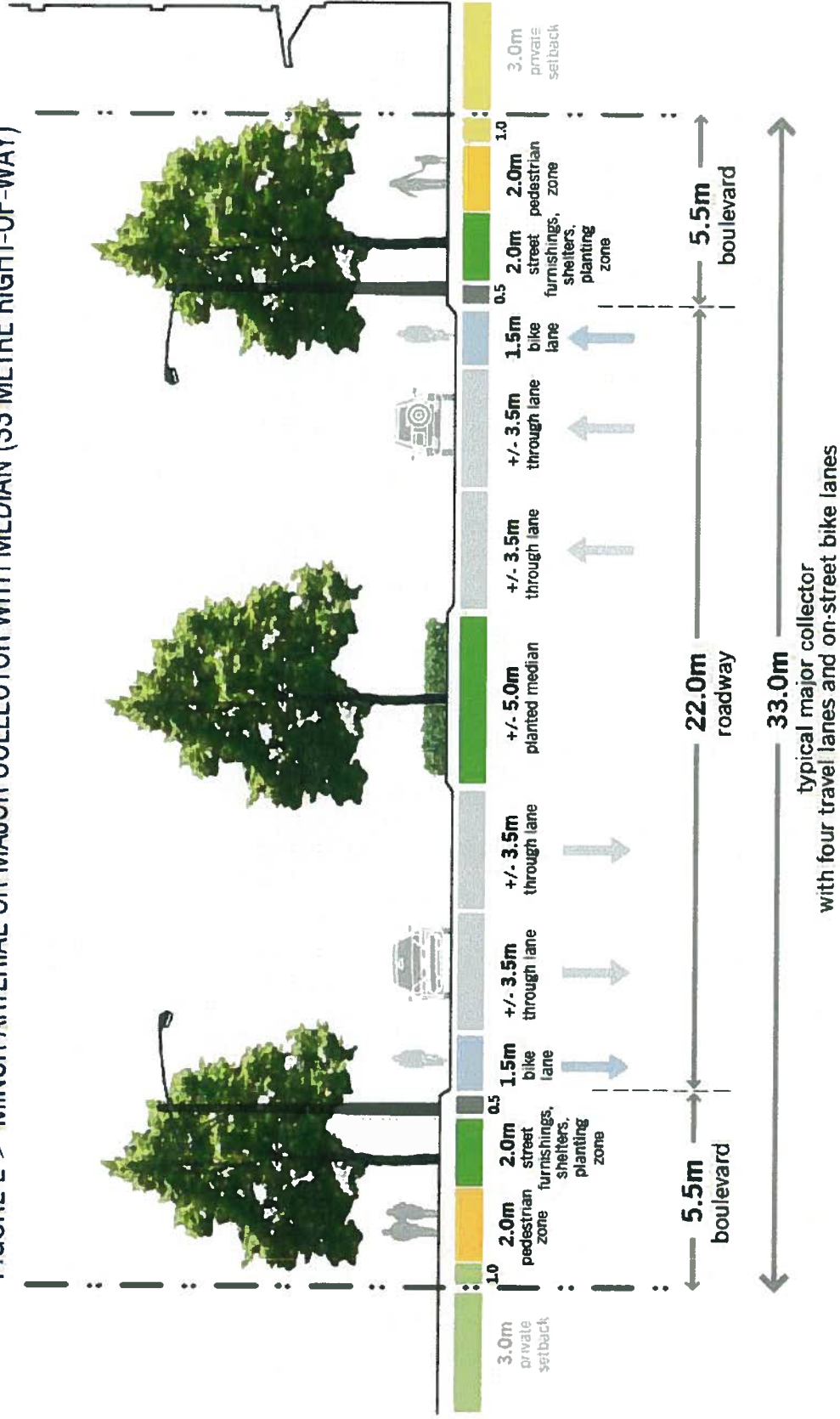
SCHEDULE C > STREET NETWORK

- LEGEND**
- arterials (width to be consistent with Region of York Capital Plan)
  - minor arterials (33 m)
  - major collectors (28-33 m)
  - special collector (33 m)
  - minor collectors (22-26 m)
  - local streets (20-22 m)
  - arterials (width to be consistent with Region of York Capital Plan)
  - Colossus Drive overpass corridor protection area (see Policy 4.3.11)
  - major parks and open spaces
  - see Policy 4.3.2
  - see Policy 4.3.18
  - see Policy 6.3.2
  - Special Study Area 'A' (See Policy 4.3.10)



# Design - VMC Secondary Plan

FIGURE E > MINOR ARTERIAL OR MAJOR COLLECTOR WITH MEDIAN (33 METRE RIGHT-OF-WAY)



The VMC Plan – Secondary Plan – Draft Consolidation Proposed Modifications WITHOUT PREJUDICE, Urban Strategies Inc., September 2015  
 The VMC Plan – Superseded – As Partially Approved by the Ontario Municipal Board on November 18, 2015.



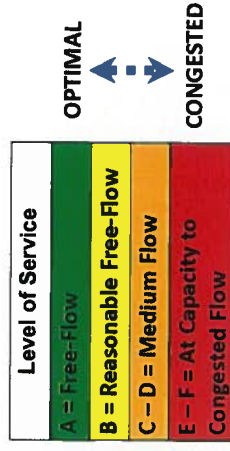
# Summary of PIC No. 1 - Future Traffic Conditions



**Future conditions  
without widening**



**Future conditions  
with widening**



# Summary of PIC No. 1

## **Problem and Opportunity**

Transportation improvements are needed to support growth within the VMC and surrounding area. Widening and extending Portage Parkway is an opportunity to:

- Capitalize on existing transportation infrastructure and strengthen east-west grid network,
- Address travel demand in the VMC and surrounding area,
- Create an alternate route to bypass the VMC including facilitating trucks as part of a broader strategy,
- Improve connections to local and regional infrastructure,
- Support transit-oriented nodes and corridors,
- Enhance transit ridership, cycling and walking, and
- Provide an alternate to Highway 7.

## **Preferred Solution**

- Travel Demand Management Initiatives are being implemented by the City as a separate strategy
- Alternative Modes of Transportation, including the provision for continuous sidewalk, cycling systems, connectivity to the subway extension to Highway 7, and rapid transit on Jane Street
- Localized Intersection and Operational Improvements (e.g., traffic signals, turning lanes)
- Widening Portage Parkway from Applewood Crescent to Jane Street (e.g., 2 to 4 lanes)
- Extending Portage Parkway from Jane Street to Creditstone Road (e.g., new 4 lane roadway)

# Summary of Public Comments

- + Add a dedicated truck route to the Portage Parkway Extension
- + Add an advance green or a turning arrow for the left turn from Pennsylvania Avenue to southbound Applewood Crescent, and from eastbound Portage Parkway to northbound Applewood Crescent
- + Consider compatibility of staged intersection improvements and driveways as part of new developments with the ultimate design for Portage Parkway
- + Consider landscaping
- + Consider proximity of the road extension to the existing CN Rail spur
- + Consider the effects of property needs for right of way
  - + Consider functionality of buildings and businesses
  - + Consider property access, circulation and operations
- + Consider the future viability of the area for industry
- + Effects on the VMC subway station and Millway Avenue
- + Extend the sidewalk on Applewood Crescent north of Portage Parkway
- + Extend Portage Parkway to Keele Street over the CN property
- + Study the extension of Langstaff Road from Creditstone Road to Keele Street

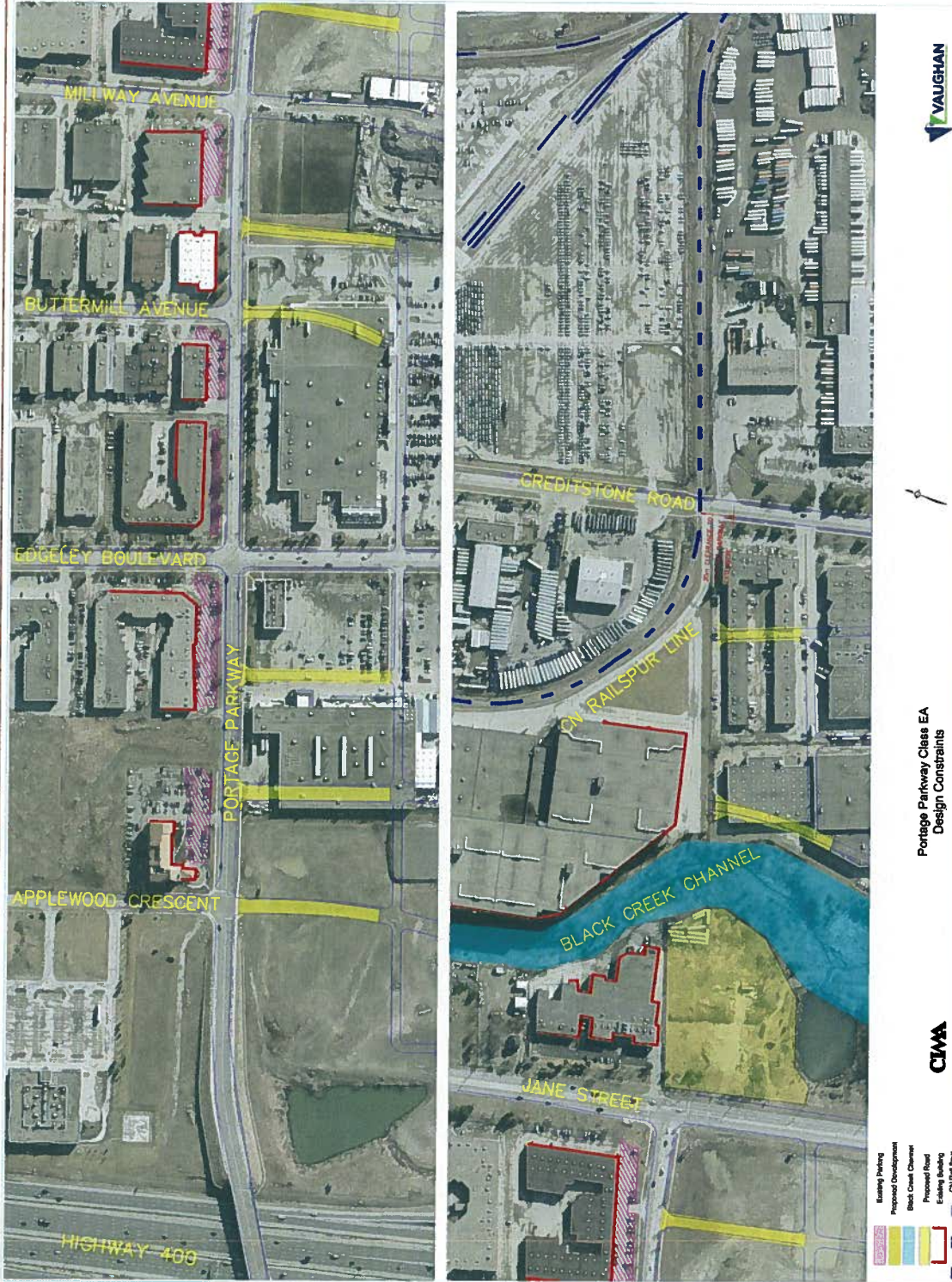


# Summary of Environmental Investigations

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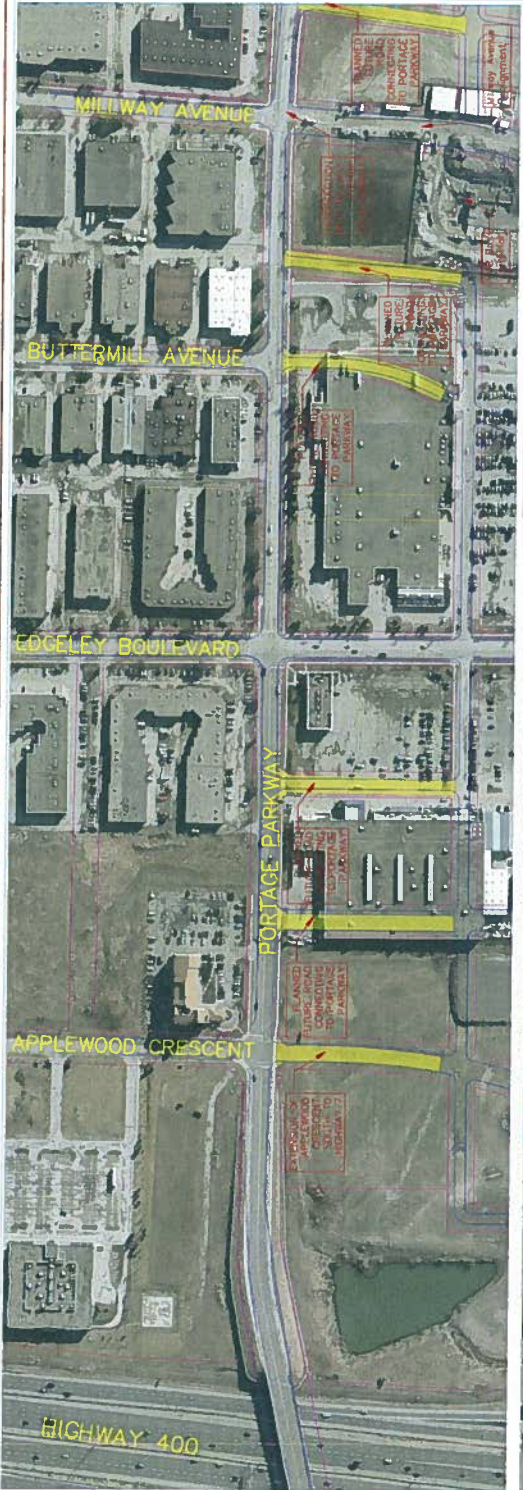
- + Fluvial Geomorphic Assessment
- + Heritage Impact Assessment
- + Natural Environment Site Reconnaissance
- + Phase 1 Environmental Site Assessment
- + Stage 1 Archaeological Assessment
- + Stormwater Constraints

# Design Constraints





# Design Considerations



VAC STREETScape AND OPEN SPACE PLAN (2015) FOR CONSIDERATION OF MOBILITY AND GATEWAY INTERSECTION DESIGN AT PORTAGE PARKWAY WITH APPLEWOOD CREEK CHANNEL. DESIGN CONSIDERATIONS FOR INTERSECTION DESIGN AT REMAINING INTERSECTIONS WITHIN THE STUDY AREA

- TRANSITIONAL ZONE BETWEEN URBAN CENTRE AND EMPLOYMENT LANDS
- PORTAGE PARKWAY AS THE LANDSCAPE EDGE FOR THE VAC
- PEDESTRIAN PRIORITY ZONES AT EDGELY BOULEVARD



Proposed Developments  
 Black Creek Channel  
 Proposed Road  
 CN Rail Spur

Portage Parkway Class EA  
 Design Considerations





# Design Evaluation Criteria

Criteria considered for assessment of alternative design concepts:

## **Cultural**

- Archaeology
- Built Heritage and Cultural Landscapes

## **Economic**

- Capital Costs

## **Implementation**

- Regulatory Framework
- Construction Staging and Phasing
- Municipal Servicing and Utilities Coordination

## **Infrastructure Planning**

- Alternative Transportation
- Streetscape
- Level of Service
- Network Capacity
- Planning Policy (Official/Secondary Plans, Transportation Master Plans)
- Stormwater Management
- Traffic Safety

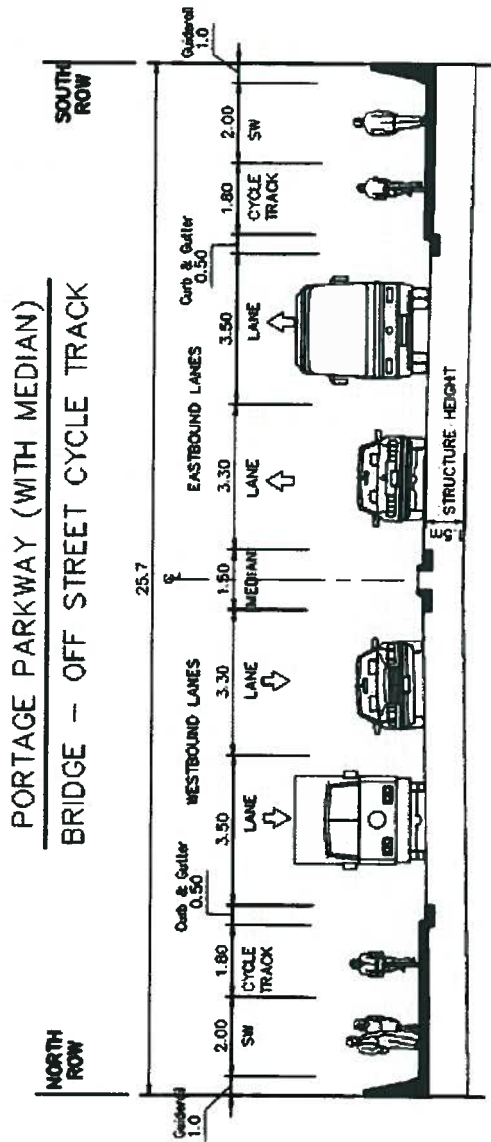
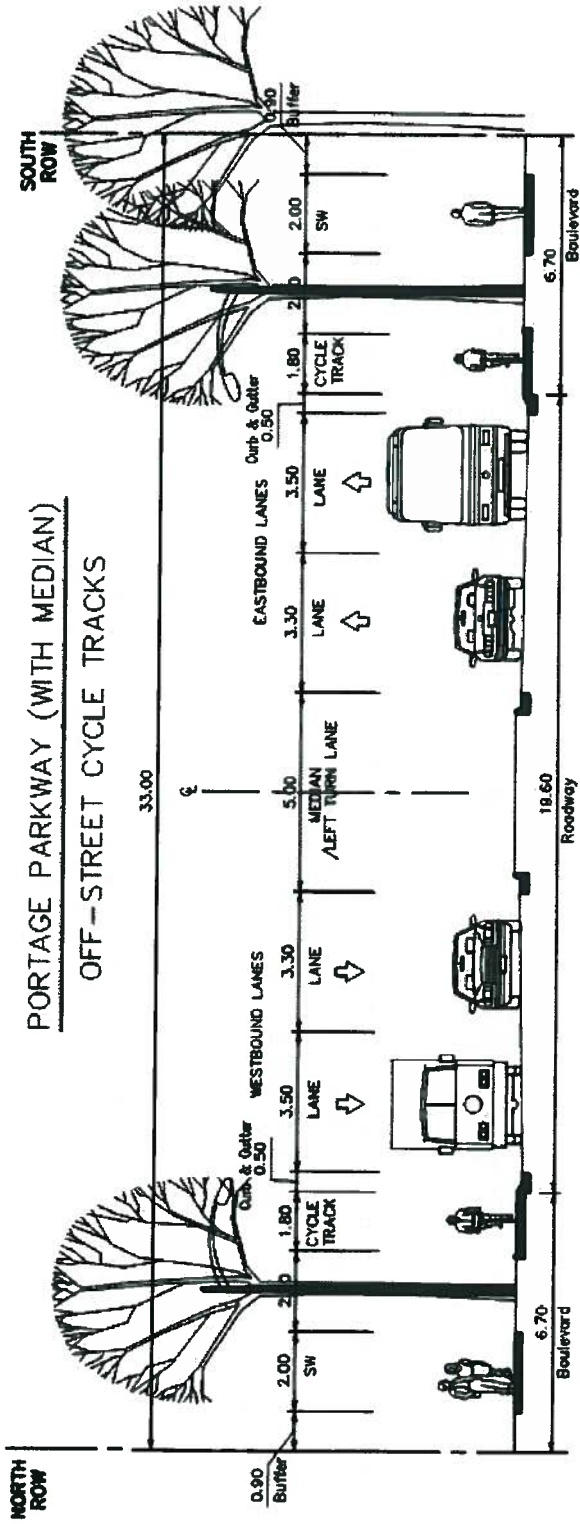
## **Natural**

- Aquatic
- Avian and Wildlife
- Natural Areas
- Species at Risk
- Vegetation
- Watercourses

## **Social**

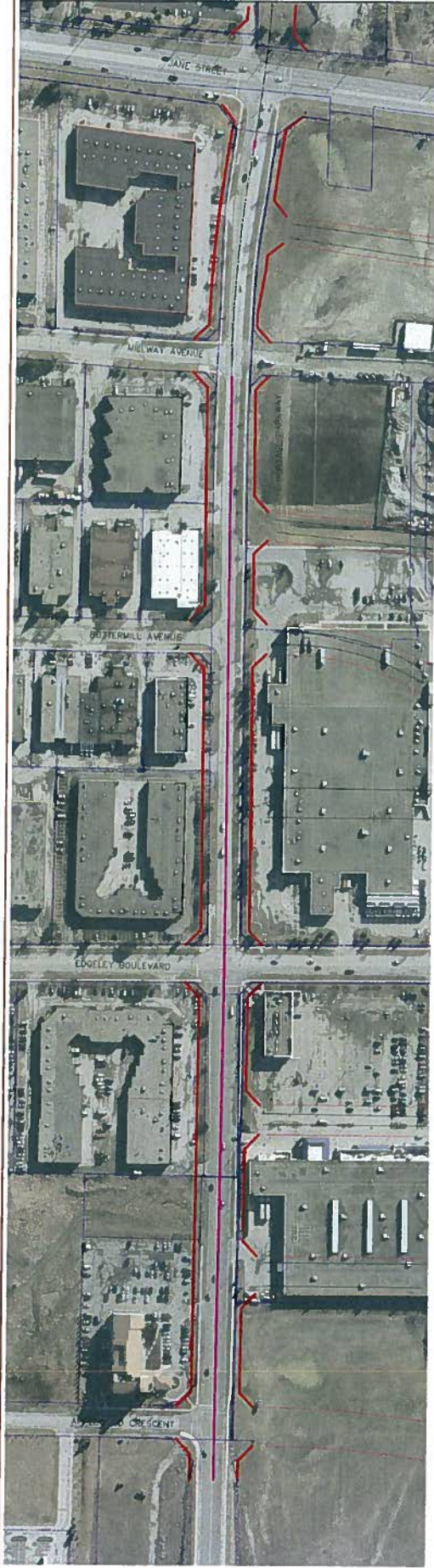
- Air Quality
- Businesses
- Emergency Services
- Noise
- Property Access

# Typical Cross-sections

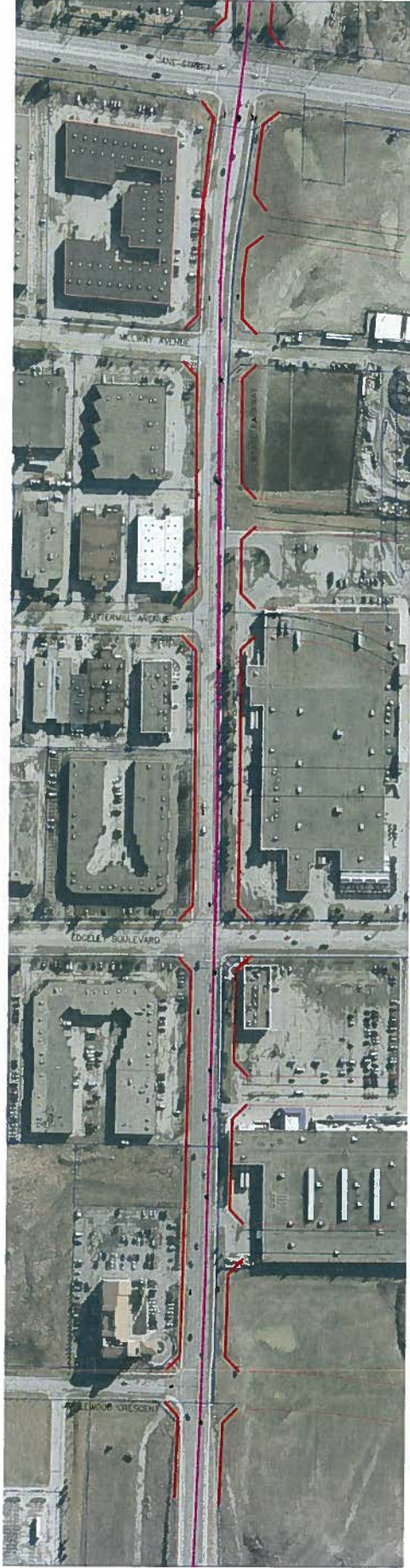




# Portage Parkway Widening Options West of Jane Street



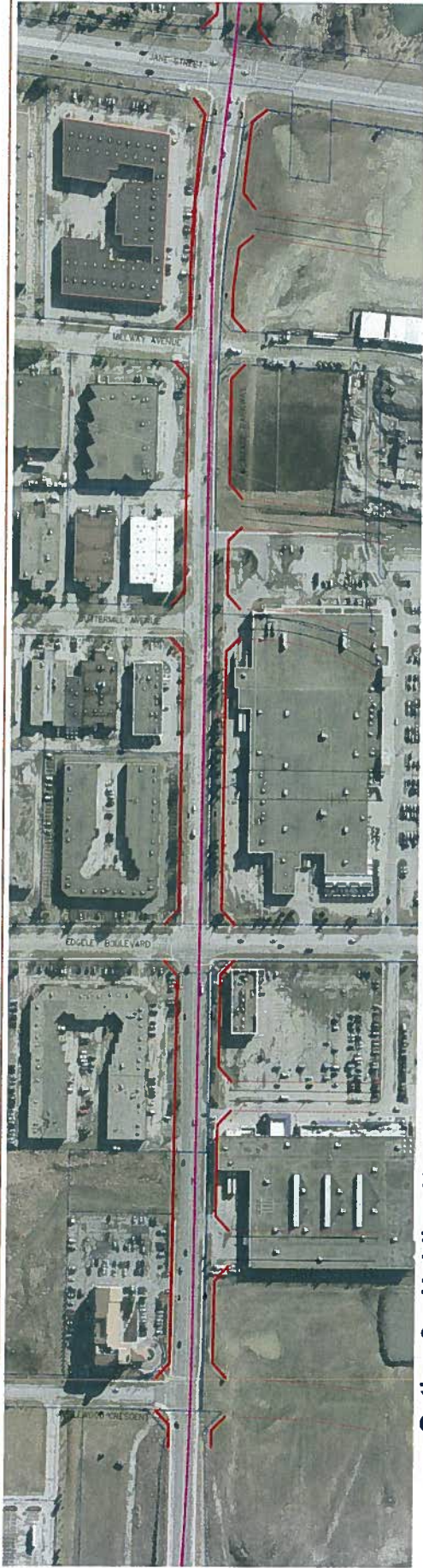
**Option 1 – Widening equally about the centerline removes a row of parking from all properties on the North side.**



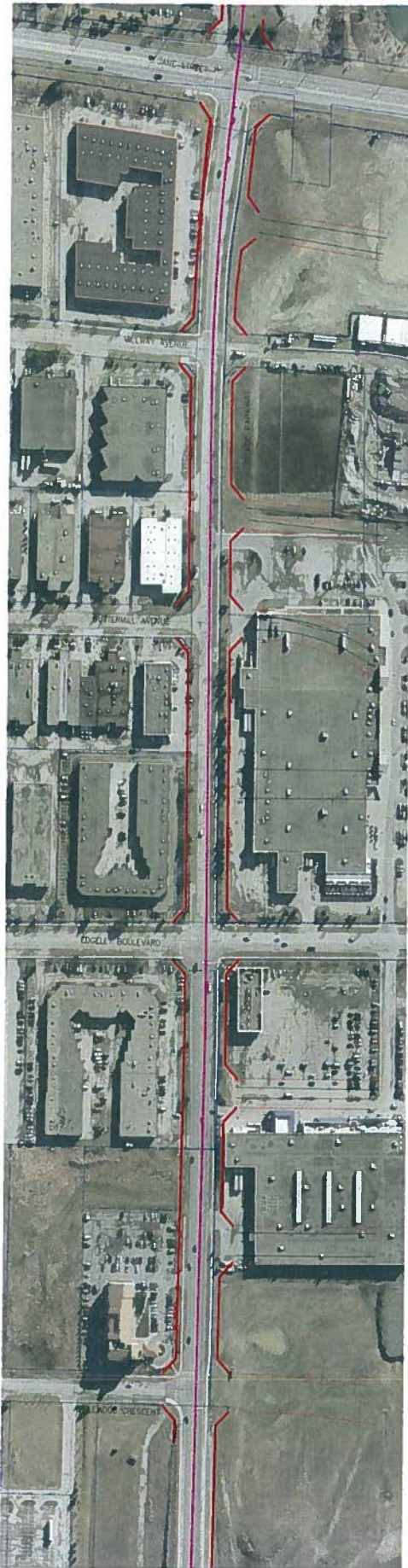
**Option 2 - Widening to the South avoids all North side parking impacts but adversely impacts on parking and loading/unloading operations on the South side.**



# Portage Parkway Widening Options West of Jane Street



**Option 3 – Holding North side right-of-way at its current position again adversely impacts parking and loading/unloading on the South side.**



**Option 4 – Holding the right-of-way to the edge of curb line for parking on North and South sides minimizes impacts on both sides (only affects one North-side property).**



# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
<b>Cultural</b>					
<b>Archaeological Features</b>	●	●	●	●	●
<b>Built Heritage Resources and Cultural Heritage Landscapes</b>	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	◐	◐	◐	◐
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	●	●	●	●
<b>Staging</b>	●	◐	◐	◐	◐
<b>Municipal Servicing Coordination</b>	●	◐	◐	◐	◐

Very Low Impact (Most Positive)	Fairly Low Impact	Medium Impact	Fairly High Impact	Very High Impact (Least Positive)
●	◐	◐	◐	○

# Evaluation of Widening West of Jane Street

Technical Criteria	Do Nothing	1	2	3	4
Infrastructure Planning					
Alternative Transportation	○	●	●	●	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, VMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	○	●	●	●	●
Natural					
Aquatic	●	●	●	●	●
Avian and Wildlife	●	●	●	●	●
Natural Areas	●	●	●	●	●
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	●	●	●	●

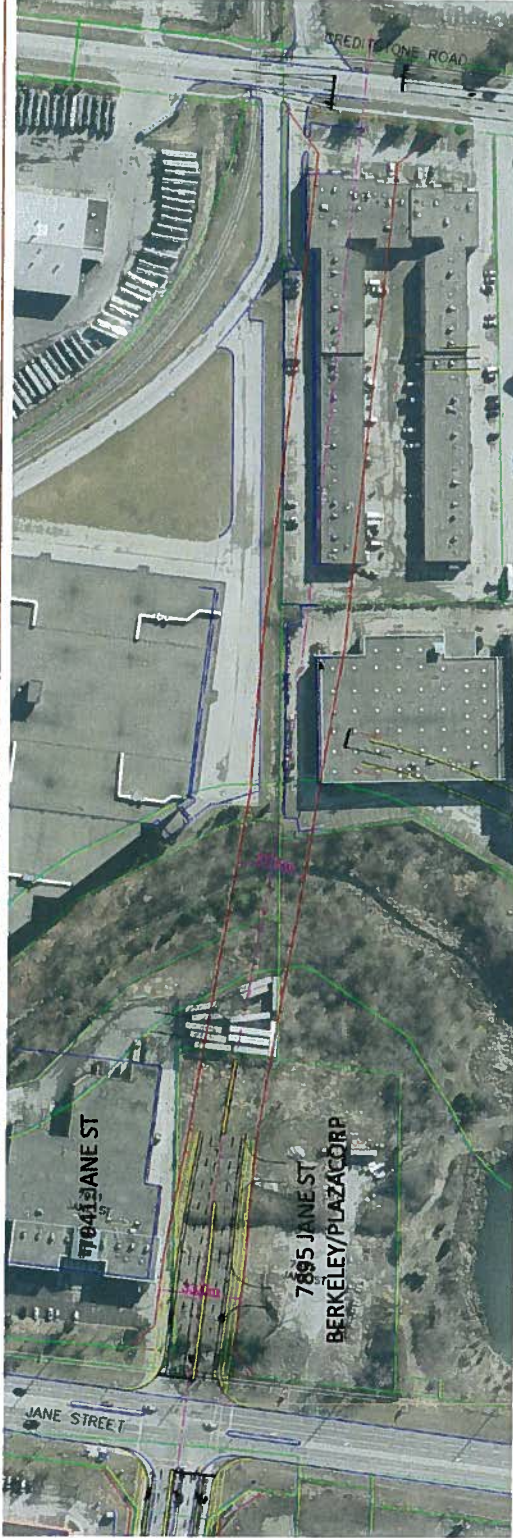


# Evaluation of Widening West of Jane Street

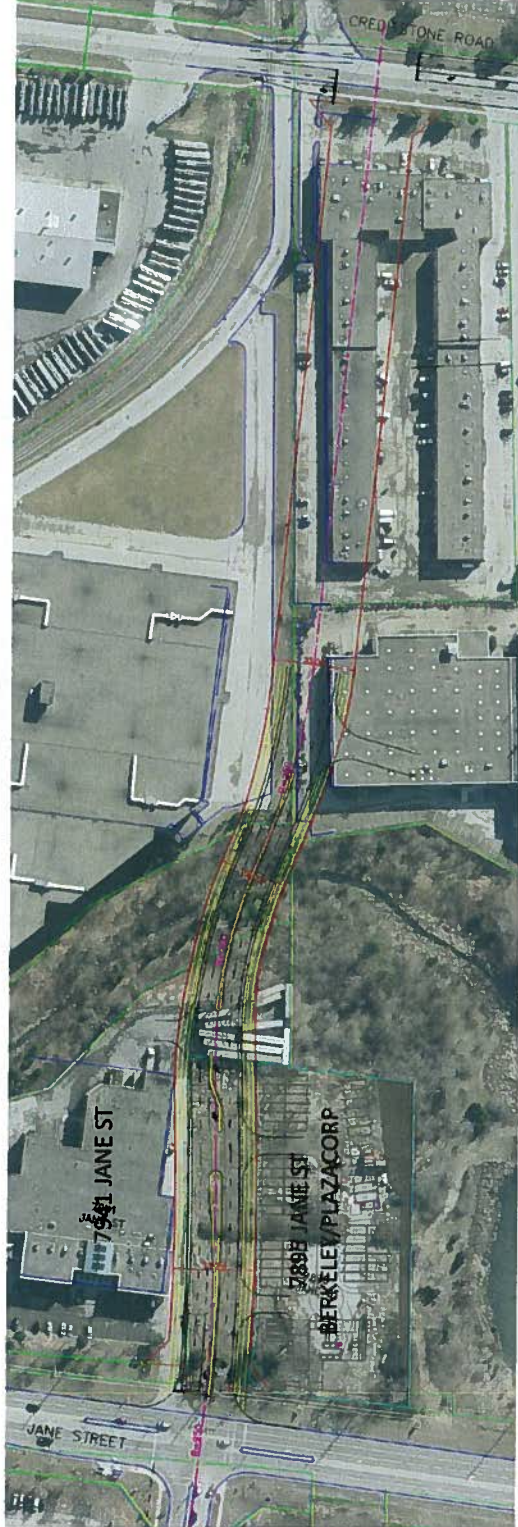
Technical Criteria	Do Nothing	1	2	3	4
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended



# Portage Parkway Road Extension East of Jane Street



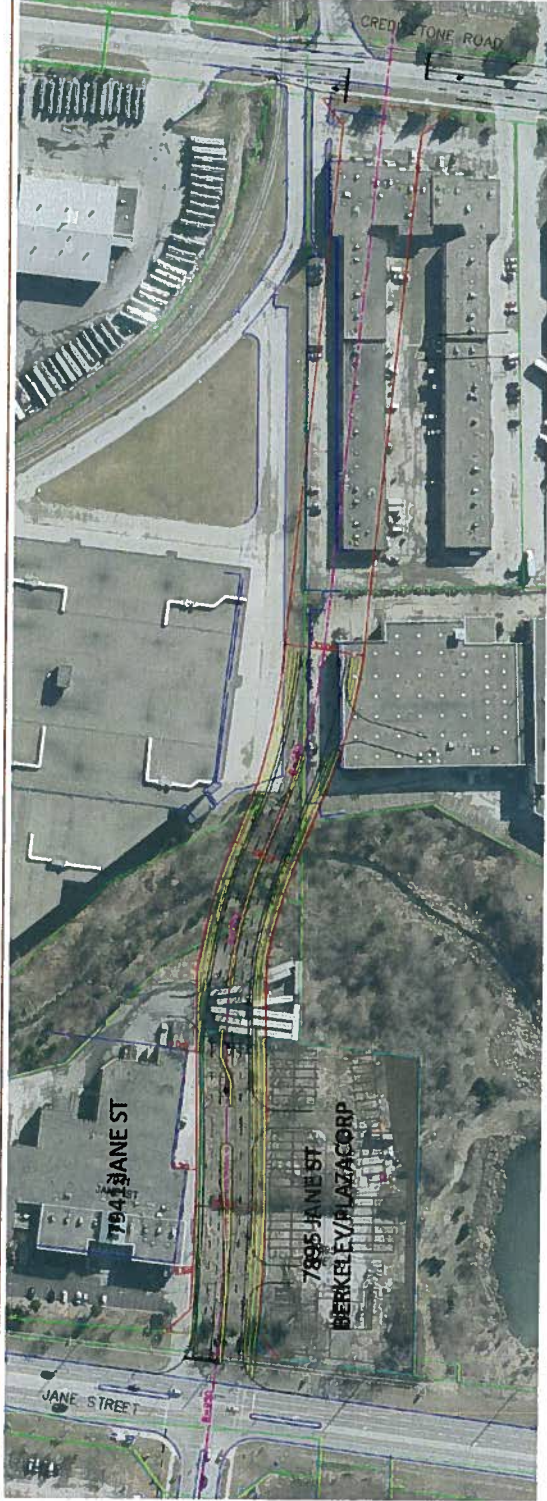
Option A – Southerly Shift East of Jane Street.



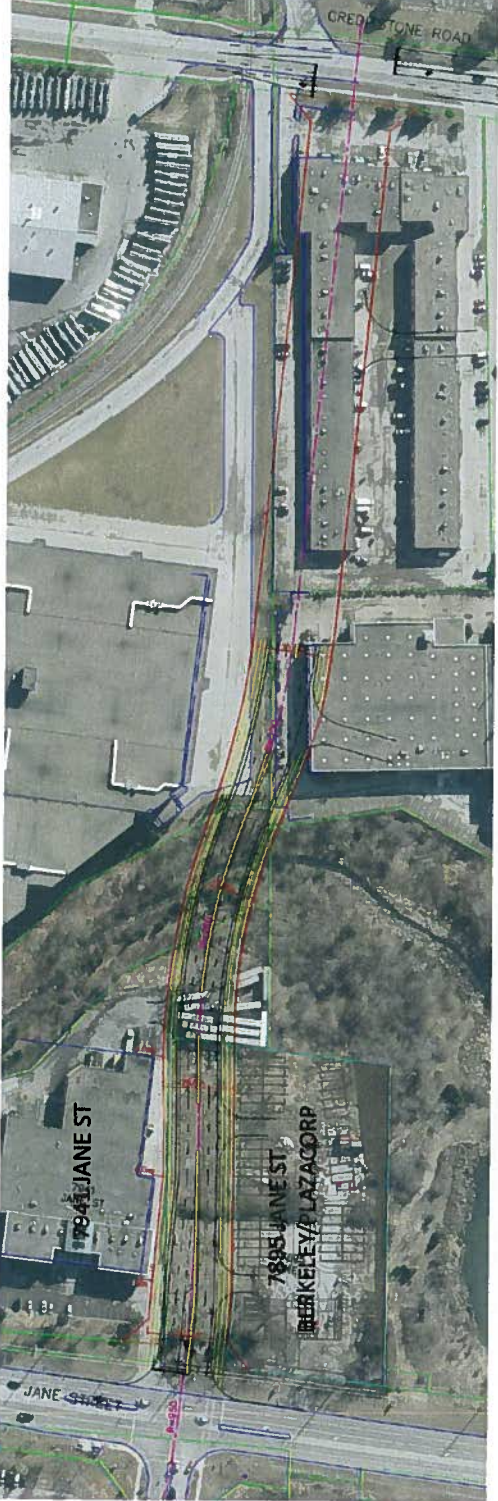
Option B – Northerly Shift East of Jane Street.



# Portage Parkway Road Extension East of Jane Street



Option C – Alignment East of Jane Street with Centre Boulevard



Option D - Alignment East of Jane Street without Centre Boulevard



# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Cultural</b>					
<b>Archaeological Features</b>	●	◐	◐	◐	◐
<b>Built Heritage Resources (BHRs) and Cultural Heritage Landscapes (CHLs)</b> BHRs – none CHLs – none	●	●	●	●	●
<b>Economic</b>					
<b>Capital Costs</b>	●	◐	◐	◐	◐
<b>Implementation</b>					
<b>Regulatory Framework</b>	●	◐	◐	◐	◐
<b>Staging</b>	●	◐	◐	◐	◐
<b>Municipal Servicing Coordination</b>	●	◐	◐	◐	◐

	Fairly Low Impact	Medium Impact	Fairly High Impact	Very High Impact (Least Positive)
●	◐	◐	◐	○
Very Low Impact (Most Positive)	Fairly Low Impact	Medium Impact	Fairly High Impact	Very High Impact (Least Positive)

# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
Infrastructure Planning					
Alternative Transportation	○	●	●	◐	●
Streetscape	●	●	●	●	●
Level of Service and Network Capacity	○	●	●	●	●
Planning Policy (Official Plan, VMC Secondary Plan, Transportation Master Plan)	○	●	●	●	●
Stormwater Management	●	◐	◐	◐	◐
Traffic Safety	●	●	●	●	●
Natural					
Aquatic	●	◐	◐	◐	◐
Avian and Wildlife	●	◐	◐	◐	◐
Natural Areas Cultural Meadow (CUM1-1) Deciduous Forest (FOD)	●	◐	◐	◐	◐
Species at Risk	●	◐	◐	◐	◐
Vegetation	●	◐	◐	◐	◐
Watercourses	●	◐	◐	◐	◐

# Evaluation of Extension East of Jane Street

Technical Criteria	Do Nothing	A	B	C	D
<b>Social</b>					
<b>Air Quality</b>					
<b>Businesses</b>					
<b>Emergency Services</b>					
<b>Property Requirements (approximate areas subject to detailed design)</b>					
<b>Noise Impacts</b>					
<b>Property Access</b>					
<b>Summary</b>					
<b>Overall Findings</b>					
<b>Recommendation</b>	Not carried forward	Not carried forward	Not carried forward	Not carried forward	Recommended



# Black Creek Crossing

## + Key Design Considerations:

- Hydraulic impact on creek flood levels.
- Amount of fill in valley.
- Vegetation Impacts.
- Soil Conditions.
- Protect for Active Transportation connectivity including to VMC Secondary Plan Open Space (i.e. Trail connections underneath and to roadway etc.)
- Financial – Constructability/engineering, costs, etc.



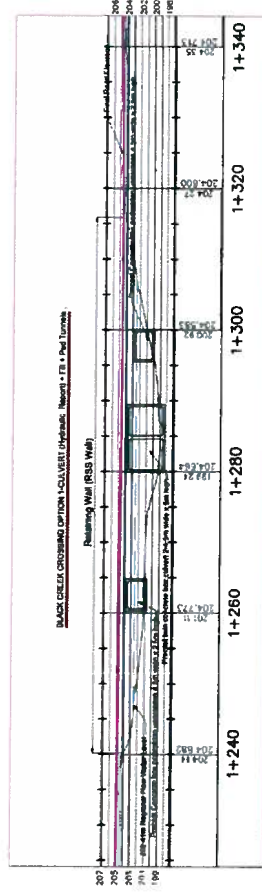
## + Design Options:

- Culverts
- Bridges

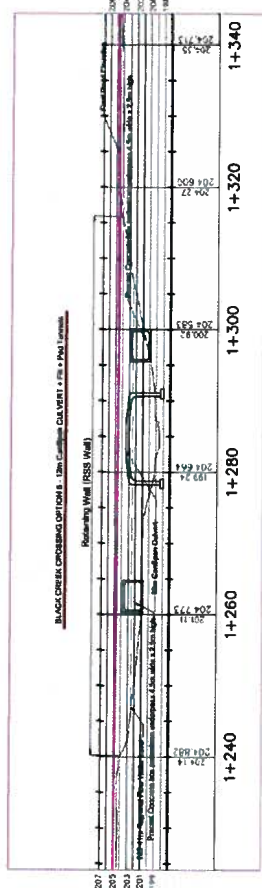
# Black Creek Crossing

## Preliminary Design Concepts Culvert Options

### Box Culvert



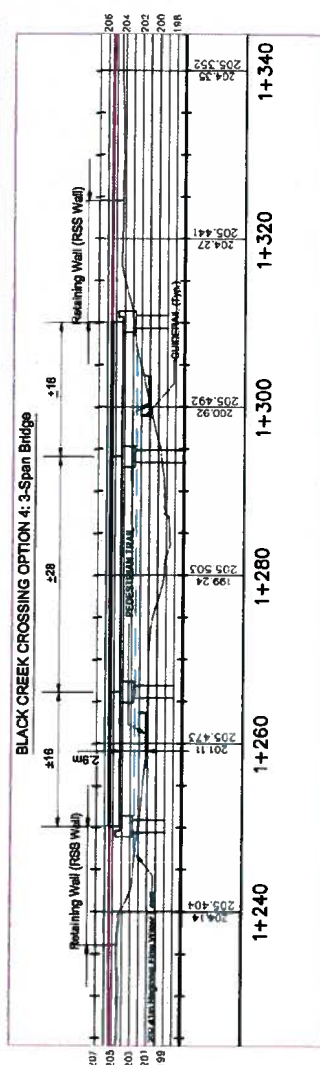
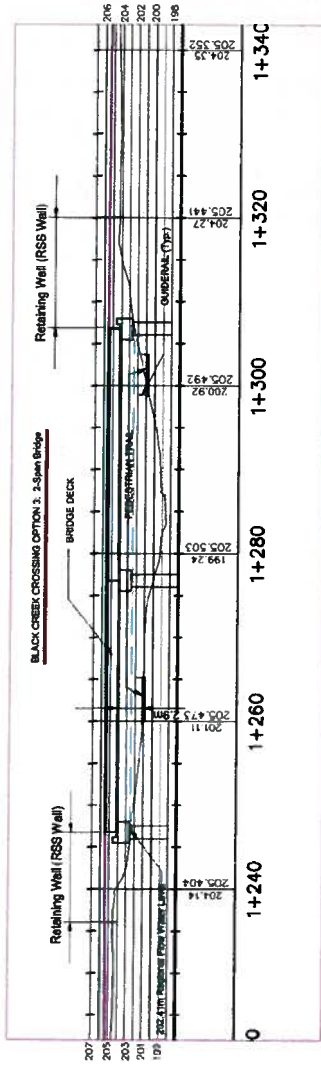
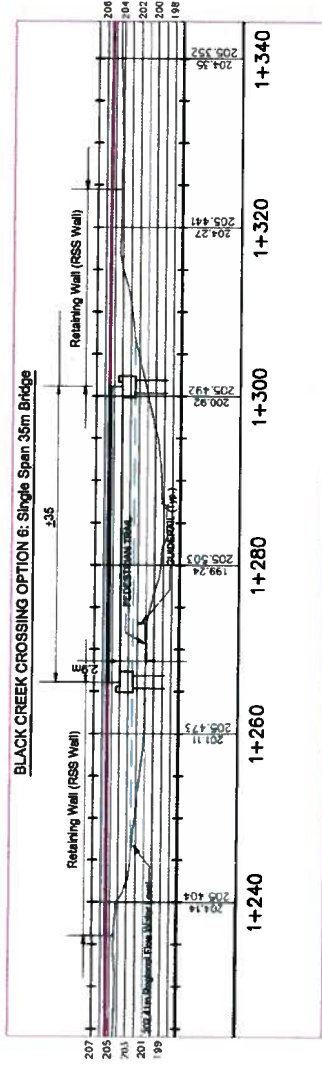
### ConSpan Culvert



Reference: CON/SPAN O-Series. Digital Image. ConTech Engineered Solutions. Web. 9 Mar. 2016.

# Black Creek Crossing

## Preliminary Design Concepts Bridge Options



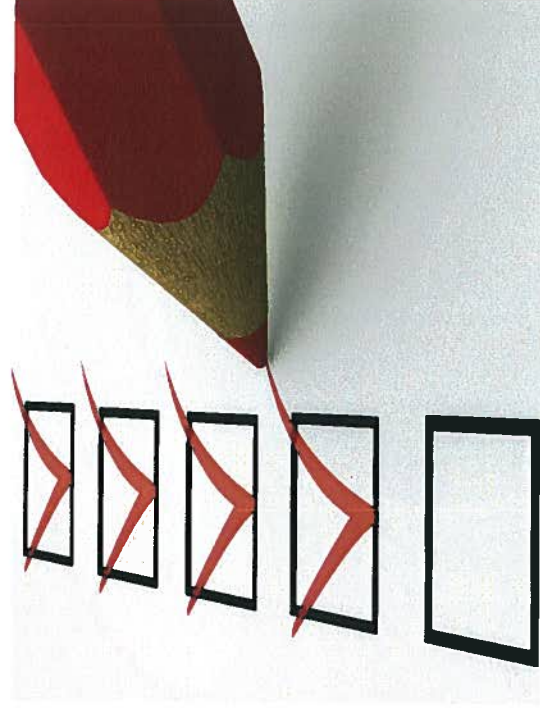
Reference: Bridge. Federal Highway Administration, Route 31, Lyons, New York. Construction of Field-Cast Ultra-High Performance Concrete Connections. Web. 9 Mar. 2016.



# Impacts and Mitigation

Impact	Mitigation
During Detailed Design	
Archaeology	<ul style="list-style-type: none"> <li>▪ Conduct Stage 2 Archaeological Assessment where required.</li> </ul>
Flood Plain	<ul style="list-style-type: none"> <li>▪ Co-ordinate with Toronto and Region Conservation for approvals.</li> </ul>
Property	<ul style="list-style-type: none"> <li>▪ Acquire and secure property (as subject to separate processes) for right-of-way from affected properties/development for widening, extension and improvements.</li> </ul>
Utilities	<ul style="list-style-type: none"> <li>▪ Relocate utilities in co-ordination with utility companies as required.</li> </ul>
During Construction	
Adjacent Land Uses	<ul style="list-style-type: none"> <li>▪ Notify the public regarding construction.</li> </ul>
Erosion and Sedimentation	<ul style="list-style-type: none"> <li>▪ Implement erosion control measures.</li> </ul>
Noise	<ul style="list-style-type: none"> <li>▪ Restore disturbed earth by sodding and planting.</li> </ul>
Vegetation	<ul style="list-style-type: none"> <li>▪ Adhere to the City of Vaughan Noise By-law.</li> <li>▪ Maintain construction equipment and vehicles.</li> </ul>
Soils	<ul style="list-style-type: none"> <li>▪ Implement tree protection measures.</li> <li>▪ Adhere to seasonal constraints for aquatic and wildlife habitat.</li> <li>▪ Replant vegetation with native species.</li> <li>▪ Prepare control and clean-up plans for spills.</li> </ul>

# Next Steps



- Review your input
- Refine and/or select preferred design concepts
- Prepare preliminary design
- File Environmental Study Reports on public record

# What do you think?

Please share your comments with either Project Manager by  
March 23, 2016:

**Marta Roias, RPP**

City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1

T: 905-832-8585 Ext. 8026

F: 905-832-6145

E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

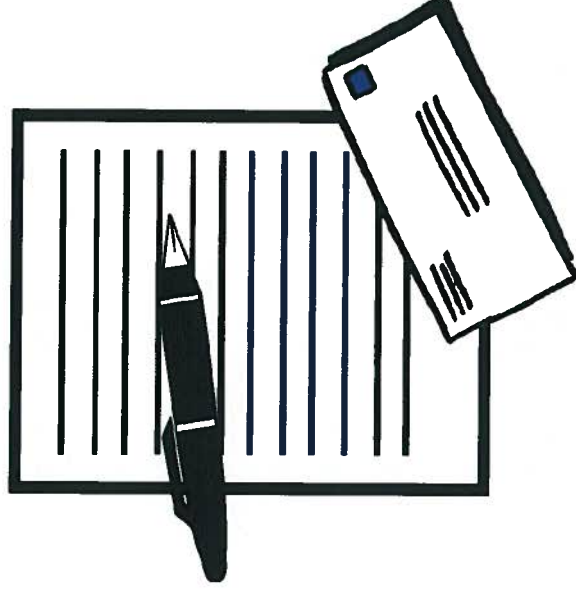
**Stephen Keen, P.Eng.**

CIMA+ Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

T: 289-288-0287 Ext. 6834

F: 289-288-0285

E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)



Thank you for attending this PIC



# **Public and Stakeholders Correspondence**



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: JUNE 17, 2015  
Name: DENNIS BALSDON  
Title: SYSTEM PLANNER  
Agency: ROGERS COMMUNICATIONS INC.  
Address: 244 NEWKIRK ROAD  
RICHMOND HILL Postal Code: L4C 3S5  
Phone: 905-780-7076 Fax: 905-780-7110  
Email: dennis.balsdon@rci.rogers.com

Does your agency wish to be kept informed of the Study? (Circle Yes or No)

Yes No

Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)

Yes No

Part of interest (Circle Part A or Part B or Both):

Part A Part B Both (Part A and Part B)

Comments:  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca

June 18<sup>th</sup> 2015

To whom it may concern,

Thank you for circulating Infrastructure Ontario (IO) on your Notice. Infrastructure Ontario is the strategic manager of the provincial government's real estate with a mandate of maintaining and optimizing value of the portfolio while ensuring real estate decisions reflect public policy objectives of the government.

As you may be aware, *IO is responsible for managing property that is owned by Her Majesty the Queen in Right of Ontario as represented by the Minister of Infrastructure (MOI)*. There is a potential that IO managed lands fall within your study area. As a result, your proposal may impact IO managed properties and/or the activities of tenants present on IO-managed properties. In order to determine if IO property is within your study area, IO requires that the proponent of the project conduct a title search by reviewing parcel register(s) for adjoining lands, to determine the extent of ownership by MOI or its predecessor's ownership (listed below). Please contact IO if any ownership of provincial government lands are known to occur within your study area and are proposed to be impacted. IO managed land can ***include within the title but is not limited to*** variations of the following: Her Majesty the Queen/King, OLC, ORC, Public Works, Hydro One, PIR, MGS, MBS, MOI, MTO, MNR and MEI\*. Please ensure that a copy of your notice is also sent to the ministry/agency on title. As an example, if the study area includes a Provincial Park, then MNR is to also be circulated notices related to your project.

*IO obligates proponents to complete all due diligence for any realty activity on IO managed lands and this should be incorporated into all project timelines.*

### **Potential Negative Impacts to IO Tenants and Lands**

#### *General Impacts*

Negative environmental impacts associated with the project design and construction, such as the potential for dewatering, dust, noise and vibration impacts, impacts to natural heritage features/habitat and functions, etc should be avoided and/or appropriately mitigated in accordance with applicable regulations best practices as well as Ministry of Natural Resources (MNR) and Ministry of the Environment (MOE) standards. Avoidance and mitigation options that characterize baseline conditions and quantify the potential impacts should be present as part of the EA project file. Details of appropriate mitigation, contingency plans and triggers for implementing contingency plans should also be present.

#### *Impacts to Land holdings*

Negative impacts to land holdings, such as the taking of developable parcels of IO managed land or fragmentation of utility or transportation corridors, should be avoided. If the potential for such impacts is present as part of this undertaking, you should contact the undersigned to discuss these issues at the earliest possible stage of your study.

If takings are suggested as part of any alternative, these should be appropriately mapped and quantified within the EA report documentation. In addition, details of appropriate mitigation and or next steps related to compensation for any required takings should be present. IO requests circulation of the draft EA report prior to finalization if potential impacts to IO-managed lands are present as part of this study.



### *Impacts to Cultural Heritage*

Should the proposed activities impact cultural heritage features on IO managed lands, a request to examine cultural heritage features, which can include cultural landscapes, built heritage, and archaeological potential and/or sites, could be required. If the potential for such impacts is present as part of this undertaking, you should contact the undersigned to discuss these issues at the earliest possible stage of your study.

### **Potential Triggers Related to MOI's Class EA**

*IO is required to follow the MOI Public Work Class Environmental Assessment Process for (PW Class EA). The PW Class EA applies to a wide range of realty and planning activities including leasing or letting, planning approvals, disposition, granting of easements, demolition and property maintenance/repair. For details on the PW Class EA please visit the Environment and Heritage page of our website found at*

<http://www.infrastructureontario.ca/Templates/Buildings.aspx?id=2147490336&langtype=1033>

*Please note that completion of any EA process does not provide an approval for MOI's Class EA obligations. Class EA processes are developed and in place to assess undertakings associated with different types of projects. For example, assessing the impacts of disposing of land from the public portfolio is significantly different then assessing the best location for a proposed road.*

IO is providing this information so that adequate timelines and project budgets can consider MOI's regulatory requirements associated with a proposed realty activity in support of a project. Some due diligences processes and studies can be streamlined. For example, prior to any disposition of land, at minimum a Phase I Environmental Site Assessment and a Stage I Archaeological Assessment and the MOI Category B Environmental Assessment should be undertaken.. Deficiencies in any of these requirements could result in substantial project delays and increased project costs.

In summary, the purchase of MOI-owned/IO-managed lands or disposal of rights and responsibilities (e.g. easement) for IO-managed lands triggers the application of the MOI Class EA. If any of these realty activities affecting IO-managed lands are being proposed as part of any alternative, please contact the Sales, Easements and Acquisitions Group through IO's main line (Phone: 416-327-3937, Toll Free: 1-877-863-9672), and also contact the undersigned at your earliest convenience to discuss next steps.

### **Specific Comments**

Please remove IO from your circulation list, with respect to this project, if MOI owned lands are not anticipated to be impacted. In addition, in the future, please send only **electronic copies of notices** for any projects impacting IO managed lands to:  
[Keith.Noronha@infrastructureontario.ca](mailto:Keith.Noronha@infrastructureontario.ca)

Thank you for the opportunity to provide initial comments on this undertaking. If you have any questions I can be reached at the contacts below.

Sincerely,

**Lisa Myslicki**

Environmental Advisor, Environmental Management  
Infrastructure Ontario  
1 Dundas Street West,  
Suite 2000, Toronto, Ontario  
M5G 2L5  
(416) 212-3768  
[lisa.myslicki@infrastructureontario.ca](mailto:lisa.myslicki@infrastructureontario.ca)

\* Below are the acronyms for agencies/ministries listed in the above letter

OLC	Ontario Lands Corporation
ORC	Ontario Realty Corporation
PIR	Public Infrastructure and Renewal
MGS	Ministry of Government Services
MBS	Management Board and Secretariat
MOI	Ministry of Infrastructure
MTO	Ministry of Transportation
MNR	Ministry of Natural Resources
MEI	Ministry of Energy and Infrastructure



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 18, 2015  
Name: Tija Dirks  
Title: Director, Transportation Planning Branch  
Agency: Ministry of Transportation  
Address: 777 Bay St., 30th Floor  
Toronto Postal Code: M7A 2J8  
Phone: 416-585-7238 Fax: \_\_\_\_\_  
Email: tija.dirks@ontario.ca

Does your agency wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca



**Ministry of Tourism,  
Culture and Sport**

Culture Services Unit  
Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON M7A 0A7  
Tel: 416 314 7147  
Fax: 416 212 1802

**Ministère du Tourisme,  
de la Culture et du Sport**

Unité des services culturels  
Direction des programmes et des services  
401, rue Bay, Bureau 1700  
Toronto ON M7A 0A7  
Tél: 416 314 7147  
Télééc: 416 212 1802



June 23, 2015 (EMAIL ONLY)

Marta Roias, RPP  
City of Vaughan  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
E: Marta.Roias@vaughan.ca

**RE: MTCS file #: 0003035**  
**Proponent: City of Vaughan**  
**Subject: Notice of Commencement/Completion**  
**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and Part B: Portage Parkway Extension from Jane Street to Creditstone Road**  
**Location: City of Vaughan, Ontario**

---

Dear Ms Roias:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the Notice of Commencement for your project. MTCS's interest in this EA project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- Archaeological resources, including land-based and marine;
- Built heritage resources, including bridges and monuments; and,
- Cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources.

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Aboriginal communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Aboriginal communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal Heritage Committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

#### **Archaeological Resources**

Your EA project may impact archaeological resources and you should screen the project with the MTCS [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MTCS archaeological sites data are available at [archaeologicalsites@ontario.ca](mailto:archaeologicalsites@ontario.ca). If your EA project area exhibits archaeological potential, then an archaeological assessment (AA) should be undertaken by an archaeologist licenced under the *OHA*, who is responsible for submitting the report directly to MTCS for review.

#### **Built Heritage and Cultural Heritage Landscapes**

The MTCS [Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes](#) should be completed to help determine whether your EA project may impact cultural heritage resources. The Clerk for the City of Vaughan can provide information on property registered or

designated under the *Ontario Heritage Act*. Municipal Heritage Planners can also provide information that will assist you in completing the checklist.

If potential or known heritage resources exist, MTCS recommends that a Heritage Impact Assessment (HIA), prepared by a qualified consultant, should be completed to assess potential project impacts. Our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. Please send the HIA to MTCS for review, and make it available to local organizations or individuals who have expressed interest in heritage.

### **Environmental Assessment Reporting**

All technical heritage studies and their recommendations are to be addressed and incorporated into EA projects. Please advise MTCS whether any technical heritage studies will be completed for your EA project, and provide them to MTCS before issuing a Notice of Completion. If your screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank you for consulting MTCS on this project: please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Dan Minkin  
Heritage Planner  
Dan.Minkin@Ontario.ca

Copied to: Stephen Keen, P. Eng.  
CIMA Canada Inc.

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MTCS makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MTCS be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MTCS if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately and the local police as well as the Cemeteries Regulation Unit of the Ministry of Government and Consumer Services must be contacted. In situations where human remains are associated with archaeological resources, MTCS should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.



Canadian Environmental  
Assessment Agency

Agence canadienne  
d'évaluation environnementale

55 St. Clair Avenue East,  
Room 907  
Toronto ON M4T 1M2

55, avenue St. Clair Est,  
pièce 907  
Toronto ON M4T 1M2

June 19, 2015

Sent by email

Sonya Kapusin  
3027 Harvester Rd., Ste 400  
Burlington, ON L7N3G7  
[Sonya.kapusin@cima.ca](mailto:Sonya.kapusin@cima.ca)

Dear Ms. Kapusin:

**Re: Information on the *Canadian Environmental Assessment Act, 2012***

Thank you for your correspondence of June 12, 2015 regarding the City of Vaughan's planned widening and extension project.

As part of the Government of Canada's plan for Responsible Resource Development, the *Canadian Environmental Assessment Act, 2012* (CEAA 2012) focuses federal environmental reviews on projects that have the potential to cause significant adverse environmental effects in areas of federal jurisdiction.

The CEAA 2012 applies to projects described in the *Regulations Designating Physical Activities* (the Regulations). Based on the information provided, your project does not appear to be described in the Regulations. **Kindly review the Regulations to confirm applicability to the proposed project** including section 1 (h), which relates to federally designated wildlife areas and migratory bird sanctuaries.

According to section 25 (c) of the Regulations the construction, operation, decommissioning and abandonment of a new all-season public highway that requires a total of 50 km or more of new right of way may require a Federal Environmental Assessment.

For more information on CEAA 2012, please access the following links on the Canadian Environmental Assessment Agency's (the Agency) website:

Overview of CEAA 2012

<http://www.ceaa.gc.ca/default.asp?lang=En&n=16254939-1>

Regulations Designating Physical Activities, and  
Prescribed Information for a Description of a Designated Project Regulations  
<http://www.ceaa.gc.ca/default.asp?lang=En&n=9EC7CAD2-1>

.../2



If it appears that CEAA 2012 may apply to your proposed project, you must provide the Agency with a description of the proposed project. Please see the link below to the Agency's guide to preparing a project description.

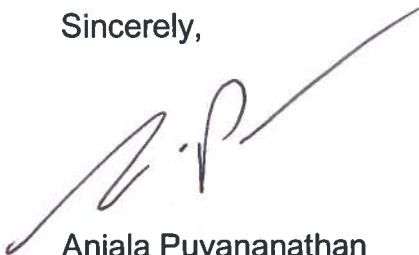
Guide to Preparing a Description of a Designated Project

<http://www.ceaa.gc.ca/63D3D025-2236-49C9-A169->

[DD89A36DA0E6/Guide to Preparing a Description of a Designated Project under CEAA 2012.pdf](http://www.ceaa.gc.ca/63D3D025-2236-49C9-A169-DD89A36DA0E6/Guide%20to%20Preparing%20a%20Description%20of%20a%20Designated%20Project%20under%20CEAA%202012.pdf)

If you believe the project is not subject to a federal environmental assessment, and do not submit a project description, **we kindly request that you remove the Agency from your distribution list.** If you have questions, please get in touch with our office through the switchboard at 416-952-1576.

Sincerely,

A handwritten signature in black ink, appearing to read 'A. P.', with a long, sweeping horizontal line extending to the right.

Anjala Puvananathan  
Director, Ontario Region  
Canadian Environmental Assessment Agency

June 12, 2015

Ms. Anjala Puvananathan  
Regional Director, Ontario Region  
Canadian Environmental Assessment Agency  
55 St. Clair Avenue East, 9th Floor  
Toronto, ON M4T 1M2

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT STUDY FOR:  
PORTAGE PARKWAY WIDENING FROM APPLEWOOD CRESCENT TO JANE STREET  
AND  
PORTAGE PARKWAY EXTENSION FROM JANE STREET TO CREDITSTONE ROAD**

Dear Ms. Puvananathan,

The City of Vaughan has initiated the Class Environmental Assessment (Class EA) Study for the planned widening and extension of Portage Parkway from Applewood Crescent to Creditstone Road. The purpose of this letter is to inform your agency of the Study, and invite a representative of your agency to participate as a member of the Technical Agencies Committee (TAC).

The Study is being undertaken in accordance with and will fulfill requirements under Schedule C of the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011) for two interrelated parts:

- Part A – Portage Parkway Widening from Applewood Crescent to Jane Street
- Part B – Portage Parkway Extension from Jane Street to Creditstone Road

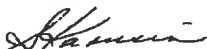
Further information and details regarding the Study are provided in the enclosed Notice of Study Commencement. A representative of your agency is invited to participate as a member of the TAC. The Committee will meet in advance of key decision points to review areas of interest, the potential need for approvals, and the evaluation of alternative solutions and designs.

We kindly ask you to indicate your interest in the Study and/or TAC by completing and returning the enclosed reply form to the undersigned before June 26, 2015. If this Notice has reached you in error, we would appreciate it if you could forward this request to the appropriate contact within your organization or advise CIMA either by returning the reply form or contacting the undersigned directly.

We look forward to your reply. Should you have any questions, please do not hesitate to contact the undersigned at (289) 288-0287, ext. 6812 or [Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca).

Sincerely,

**CIMA Canada Inc.**



Sonya Kapusin, MCIP, RPP  
Consultant EA Planner  
[Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca)

Encl.

cc: Marta Roias, RPP, City of Vaughan  
Stephen Keen, P.Eng., CIMA







## COMMENCEMENT NOTICE ENVIRONMENTAL ASSESSMENT STUDY

**Part A: Portage Parkway Widening from Applewood Crescent to Jane Street and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road**

### THE STUDY

The City of Vaughan Transportation Master Plan (TMP) – A New Path (2013) – identified the need to widen Portage Parkway from Applewood Crescent to Jane Street (Part A) and extend Portage Parkway from Jane Street to Creditstone Road (Part B) as a strategic network improvement and in support of the development in the Vaughan Metropolitan Centre.

### THE PROCESS

The TMP – A New Path, having completed Phases 1 and 2 of the Municipal Class EA process, has identified projects which will improve connections to local and regional infrastructure, provide access to future developments, support transit-oriented nodes and corridors, and enhance transit ridership, cycling and walking.

The TMP identified the need to widen Portage Parkway to four lanes from Applewood Crescent to Jane Street and establish a new four lane road from Jane Street to Creditstone Road and recommended the project be carried forward to Phases 3 and 4 of the Class Environmental Assessment process.

The City has initiated the Class EA Study, which will establish the preferred alternative design for the widening, extension and improvements for:

- Portage Parkway Widening from Applewood Crescent to Jane Street (\*Part A)
- Portage Parkway Extension from Jane Street crossing the Black Creek channel to Creditstone Road (\*Part B)

The Study will consider the technical, environmental, socio-economic and financial criteria as the basis for the assessment. The Study, proceeding for two interrelated parts, is being undertaken in accordance with "Schedule C" of the Municipal Engineers Association Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011). Separate Environmental Study Reports (ESRs) for Part A and B will be prepared and available for public review and comment on study completion.

### CONSULTATION

Consultation with interested stakeholders (public and agencies) is a key component of the study. Public Information Centres (PIC) will be held to present the study, review the scope and discuss issues related to the project including alternative solutions and designs, evaluation criteria, and environmental impacts and mitigation measures. Public Information Centre details will be advertised closer to consultation dates.

For information on study progress and updates, please visit:  
[http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx)

### CONTACTS

If you have any questions/comments or wish to be added to the study mailing list, please contact:

**Marta Roias, RPP**  
City of Vaughan Project Manager  
2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
T: 905-832-8585 Ext. 8026  
F: 905-832-6145  
E: [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca)

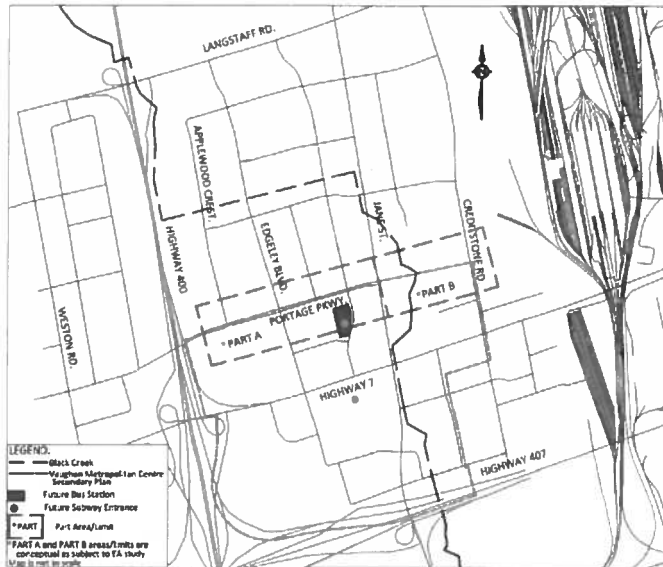
or

**Stephen Keen, P.Eng.**  
CIMA Canada Inc. Consultant Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7  
T: 289-288-0287 Ext. 6834  
F: 289-288-0285  
E: [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca)

*Information is being collected under the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.*

**ANDREW PEARCE**, Director of Development Engineering and Infrastructure Planning Services

This Notice first issued June 12, 2015







REPLY FORM (Please print)

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

**Date:** \_\_\_\_\_  
**Name:** \_\_\_\_\_  
**Title:** \_\_\_\_\_  
**Agency:** \_\_\_\_\_  
**Address:** \_\_\_\_\_  
\_\_\_\_\_ **Postal Code:** \_\_\_\_\_  
**Phone:** \_\_\_\_\_ **Fax:** \_\_\_\_\_  
**Email:** \_\_\_\_\_

**Does your agency wish to be kept informed of the Study? (Circle Yes or No)**

Yes                  No

**Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)**

Yes                  No

**Part of interest (Circle Part A or Part B or Both):**

Part A                  Part B                  Both (Part A and Part B)

**Comments:** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Please return this form to the contact below by June 26, 2015:**

**Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)**

With the exception of personal information, all comments will become part of the public record.







REPLY FORM (Please print)

Rec'd June 29/15

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 22 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: \_\_\_\_\_

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)





REPLY FORM (Please print)

Rec'd June 29/15

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 22, 2016  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of Interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
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Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 25/15  
Name: Christine Hyde  
Title: Planner  
Agency: York Catholic District School Board  
Address: 320 Bloomington Rd. W. Aurora, ON L4G 0M1  
Postal Code: \_\_\_\_\_  
Phone: 905-713 1211 x 12360 Fax: \_\_\_\_\_  
Email: christine.hyde@ycdsb.ca

Does your agency wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)

Yes  No

Part of Interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
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Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca

REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 27, 2015  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)





REPLY FORM (Please print)

Rec'd June 29/15

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: JUNE 22, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

FAX Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 22, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of Interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0267  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 23, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A      Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



## Maram Miri

---

**From:** [Redacted]  
**Sent:** Wednesday, June 24, 2015 12:09 PM  
**To:** Sonya Kapusin  
**Cc:** 'Marta.Roias@vaughan.ca'; Andrew Pearce (Andrew.Pearce@vaughan.ca)  
**Subject:** Environmental Assessment Study for Portage Parkway  
**Attachments:** Scanned from a Xerox Multifunction Device.pdf

Dear Ms. Kapusin:

We are the solicitors for [Redacted] and [Redacted] On behalf of our clients, please find attached our correspondence and four (4) completed "Reply Forms" relating to our clients' confirmation that they wish to participate as members in the Stakeholders Group related to both Parts A and B of the proposed EA for Portage Parkway.

Kindly confirm receipt of this e-mail and the attachments.

Should you require any additional information, please feel free to contact me.

Best regards,

June 24, 2015

**VIA E-MAIL**

Sonya Kapusin  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington ON L7N 3G7

Dear Ms. Kapusin:

**RE: City of Vaughan  
Environmental Assessment for Portage Parkway**

We are the solicitors for \_\_\_\_\_ and \_\_\_\_\_

Our clients are jointly involved in a proposed redevelopment of the Property and have filed planning applications with the City of Vaughan.

We are in receipt of your letter dated June 12, 2015. We are writing to confirm our clients' interest in the Environmental Assessment Study for both the Portage Parkway Widening from Applewood Crescent to Jane Street (Part A), and the Portage Parkway Extension from Jane Street to Creditstone Road (Part B) (the "Study"). Our clients hereby confirm their request to participate as members in the Stakeholders Group for both Parts A and B of the Study.

We have enclosed four completed Reply Forms for the following:

1. \_\_\_\_\_
2. \_\_\_\_\_
3. \_\_\_\_\_
4. \_\_\_\_\_

Please ensure that all future communications in respect of the Study are sent to each of the above-noted individuals.

Should you require any further information, please contact the undersigned.

Yours truly,

Per:

Encl.  
c. **Marta Roias  
Andrew Pearce**



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 24, 2015  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of Interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
Please see cover letter dated June 24, 2015.  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)





REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 24, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A      Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_

Please see cover letter dated June 24, 2015

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
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3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 24, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes      No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes      No

Part of Interest (Circle Part A or Part B or Both):

Part A      Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_

Please see cover letter dated June 24, 2015.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca



REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 24, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_

Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of Interest (Circle Part A or Part B or Both):

Part A      Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_

Please see cover letter dated June 24, 2015.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca





## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Wednesday, June 24, 2015 2:02 PM  
**To:**  
**Cc:** Sonya Kapusin; Pearce, Andrew; Stephen Keen; Hubjer, Selma  
**Subject:** FW: Environmental Assessment Study for Portage Parkway  
**Attachments:** Scanned from a Xerox Multifunction Device.pdf

Greetings

Thank you. **Following up to below...** Please accept this as confirmation of receipt of email and the attachments. The contacts and contact information provided has been added to the Stakeholder Group list.

Looking forward ...

Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Wednesday, June 24, 2015 12:09 PM  
**To:** 'sonya.kapusin@cima.ca'  
**Cc:** Roias, Marta; Pearce, Andrew  
**Subject:** Environmental Assessment Study for Portage Parkway

Dear Ms. Kapusin:

We are the solicitors for On behalf of our clients,  
please find attached our correspondence and four (4) completed "Reply Forms" relating to our clients' confirmation that  
they wish to participate as members in the Stakeholders Group related to both Parts A and B of the proposed EA for  
Portage Parkway.

**Kindly confirm receipt of this e-mail and the attachments.**

Should you require any additional information, please feel free to contact me.

Best regards,

REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 25, 2015  
Name: [REDACTED]  
Affiliation (if applicable): [REDACTED]  
Address: [REDACTED]  
Postal Code: [REDACTED]  
Phone: [REDACTED] Fax: [REDACTED]  
Email: [REDACTED]

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both)

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



## Maram Miri

---

**From:**  
**Sent:** Friday, June 26, 2015 12:45 PM  
**To:** Sonya Kapusin  
**Subject:** EA for Portage Parkway Extension, Vaughan  
**Attachments:** Vaughan environmental assesement letter June 12.15.pdf; Vaughan environmental assesement reply June 26.15.pdf

Ms. Kapusin,

We have recently become aware of the proposal by the City of Vaughan to extend Portage Parkway easterly to Jane Street. Our company, \_\_\_\_\_ owns the property at \_\_\_\_\_ which is leased to \_\_\_\_\_ who operates a rather large document storage facility at this site.

We now have your letter of June 12, 2015 and are submitting the attached Reply. We wish to be kept informed and involved in this project. We suspect that our tenant, \_\_\_\_\_ may also want to be informed and we are forwarding your letter of June 12 and this email to them for direct reply.

Thank you.

REPLY FORM (Please print)

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

Date: \_\_\_\_\_  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

**Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)**

*Rec'd June 29 / 15*

**REPLY FORM (Please print)**

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

**Date:** JUNE 24, 2015  
**Name:** \_\_\_\_\_  
**Affiliation (if applicable):** \_\_\_\_\_  
**Address:** \_\_\_\_\_  
**Postal Code:** \_\_\_\_\_  
**Phone:** \_\_\_\_\_ **Fax:** \_\_\_\_\_  
**Email:** \_\_\_\_\_

**Do you wish to be kept informed of the Study? (Circle Yes or No)**

**Yes**       **No**

**Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)**

**Yes**       **No**

**Part of interest (Circle Part A or Part B or Both):**

**Part A**       **Part B**       **Both (Part A and Part B)**

**Comments:** \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
DIRECTLY AFFECTED BY THE EASTERLY  
EXTENSION OF PORTAGE PARKWAY.  
\_\_\_\_\_  
\_\_\_\_\_

**Please return this form to the contact below by June 26, 2015:**

**Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)**





REPLY FORM (Please print)

ASSESSMENT (EA) FOR:  
DENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: June 26/15

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

*Recd June 29/15*

## Maram Miri

---

**From:** Stephen Keen  
**Sent:** Friday, June 26, 2015 10:50 AM  
**To:** Sonya Kapusin  
**Cc:** Jaime Garcia  
**Subject:** FW: Enviromental Assessment Study notice at Portage Parkway

fyi

Stephen Keen  
CIMA+  
Burlington, Ontario  
Tel:289-288-0287 x6834

---

**From:**  
**Sent:** June-26-15 10:50 AM  
**To:** Roias, Marta  
**Cc:** Stephen Keen;  
**Subject:** RE: Enviromental Assessment Study notice at Portage Parkway

Thank you for the reply. One other thing to look at for safety is to extend the sidewalk on Applewood north of Portage. The sidewalk stops around Pennsylvania and people end up walking on the street.

Regards,

---

**From:** Roias, Marta [<mailto:Marta.Roias@vaughan.ca>]  
**Sent:** Thursday, June 25, 2015 9:05 AM  
**To:**  
**Cc:** Stephen Keen ([Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca))  
**Subject:** RE: Enviromental Assessment Study notice at Portage Parkway

Greetings

Thank you for expressing an interest in the [Environmental Assessment Study for Portage Parkway](#) and your detailed comments. With respect to your request;

- by copy , your email has been forwarded to the Transportation Services and Parks and Forestry Operations department re: request to look into traffic conditions at the intersection of Pennsylvania Avenue and Applewood Crescent
- the Environmental Assessment Study for Portage Parkway will be assessing safety and intersection performance and identify and consider intersection improvements at intersecting streets including Applewood Crescent. Your name and contact information are on the study mailing list and you will receive future Notices as the study progresses

Regards,  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Tuesday, June 23, 2015 5:03 PM  
**To:** Roias, Marta  
**Subject:** Enviromental Assessment Study notice at Portage Parkway

Good day Marta,

This is in regards to the notice of the assessment study in this area and such would like to ask if you can look at the traffic conditions at Applewood Ave and Pennsylvania Ave and also East bound Portage Pkwy Applewood.

- To make a left hand turn from Pennsylvania onto southbound Applewood has become risky and potential of serious accidents (especially with large trucks in the area). Maybe adding a 3 way stop sign or stop lights at Pennsylvia and Applewood would help this corner become safer.
- To make a left hand turn from East bound Portage Prkwy to North bound Applewood is also dangerous. There are traffic lights at the corner already and adding an advance green or a turning arrow for the left turn would help traffic move quicker and also safer.

The above are a few suggestions as we use these streets regularly and hopefully the little information given will help with the study . Please forward to the appropriate department if this is not part of this study.

Regards,

---

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.



## Maram Miri

---

**From:** Sonya Kapusin  
**Sent:** Monday, June 29, 2015 9:43 AM  
**To:**  
**Cc:** 'Marta.Roias@vaughan.ca'; Andrew Pearce (Andrew.Pearce@vaughan.ca); Stephen Keen  
**Subject:** RE: Environmental Assessment Study for Portage Parkway

Thank you This reply is to confirm receipt of your email and attachments.

### Sonya Kapusin

Tel: 289-288-0287 x 6812  
Fax: 289-288-0285

---

**From:**  
**Sent:** June-24-15 12:09 PM  
**To:** Sonya Kapusin  
**Cc:** 'Marta.Roias@vaughan.ca'; Andrew Pearce (Andrew.Pearce@vaughan.ca)  
**Subject:** Environmental Assessment Study for Portage Parkway

Dear Ms. Kapusin:

We are the solicitors for . On behalf of our clients, please find attached our correspondence and four (4) completed "Reply Forms" relating to our clients' confirmation that they wish to participate as members in the Stakeholders Group related to both Parts A and B of the proposed EA for Portage Parkway.

Kindly confirm receipt of this e-mail and the attachments.

Should you require any additional information, please feel free to contact me.

Best regards,

## Sonya Kapusin

---

**From:** Sonya Kapusin  
**Sent:** Friday, July 03, 2015 3:24 PM  
**To:**  
**Cc:** Roias, Marta (Marta.Roias@vaughan.ca); Selma Hubjer (selma.hubjer@vaughan.ca); Stephen Keen  
**Subject:** RE: City of Vaughan - Environmental Assessment

Hi \_\_\_\_\_,

We will add \_\_\_\_\_ to our mailing list to receive notices by mail.

Regards,

### Sonya Kapusin

Tel: 289-288-0287 x 6812  
Fax: 289-288-0285

---

**From:**  
**Sent:** June-30-15 10:22 AM  
**To:** Sonya Kapusin  
**Subject:** RE: City of Vaughan - Environmental Assessment

Is he on the list now? How will he receive the information?

Thanks

---

**From:**  
**Sent:** 23-Jun-15 1:25 PM  
**To:** 'sonya.kapusin@cima.ca'  
**Subject:** City of Vaughan - Environmental Assessment

Please find attached your form to enlist \_\_\_\_\_ in being kept informed of the Environmental Assessment for Portage Parkway Widening.

Please do not hesitate to contact me if you have any further questions or comments.

Thanking you in Advance,

REPLY FORM (Please print)

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

Date: June 30<sup>th</sup>, 2015  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A      Part B       Both (Part A and Part B)

Comments: see attached email  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)



**Maram Miri**

---

**From:**  
**Sent:** Tuesday, June 30, 2015 2:32 PM  
**To:** Sonya Kapusin; marta.roias@vaughan.ca; Stephen Keen  
**Subject:** Portage Parkway Extension  
**Attachments:** Scanned from a Xerox multifunction device.pdf

Good afternoon,

Please see attached completed form, confirming interest in the environmental assessment for the Portage Parkway Extension, in proximity to CN's MacMillan Yard (the largest rail classification in Canada) and to the City of Vaughan's vast industrial park to the west of the Yard. While I do note the June 26<sup>th</sup> deadline, I did not receive this notice until this morning.

There is an entire array of policies at the provincial, regional and municipal level seeking to promote and protect employment lands and transportation corridors in Vaughan. However, real estate speculation in proximity to the Vaughan Metropolitan Centre is also threatening the integrity and long-term viability of these employment lands and of vital transportation facilities. Accordingly, the inclusion of a dedicated truck route in the Portage Parkway Extension, along with other complementary interventions, can help the City of Vaughan meet its own planning objectives, as well as those contained in the Provincial Policy Statement.

and its partners look forward to participating in The Study.

Regards,

\*\*\*\*\*  
*Le contenu du présent courriel est privilégié, confidentiel et soumis à des droits d'auteur. Il est interdit de l'utiliser ou de le divulguer sans autorisation.*

*This e-mail message is privileged, confidential and subject to copyright. Any unauthorized use or disclosure is prohibited.*  
\*\*\*\*\*



REPLY FORM (Please print)

Rec'd July 13, 2015

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: July 7, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Monday, July 20, 2015 10:55 AM  
**To:**  
**Cc:** Stephen Keen; Sonya Kapusin; Hubjer, Selma  
**Subject:** RE: Transport Canada grade crossing standards - Portage Parkway extension

Thanks for attending and your follow up below to last week's meeting.

Looking forward...  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Monday, July 20, 2015 10:40 AM  
**To:** Roias, Marta  
**Cc:**  
**Subject:** Transport Canada grade crossing standards - Portage Parkway extension

Marta,

This is a link to the Transport Canada (TC) setback standard I mentioned last week:  
<https://www.tc.gc.ca/eng/railsafety/grade-crossings-standards-317.htm>

This link leads to the TC webpage which contains the new 2015 crossing regulations:  
<https://www.tc.gc.ca/eng/railsafety/grade-crossings-standards.htm>

Any proposal for a new grade crossing must comply with these standards, and the proposal must be reviewed by the appropriate departments at ; it is important to note that each new grade crossing constitutes a new safety risk, and as such does not support the creation of new crossings, unless the intent is to close an existing crossing to create a safer one. As I mentioned at last week's meeting, additional constraints may preclude a new crossing of active industrial lead on the west side of MacMillan Yard, linking the future extension of Portage Parkway to Creditstone Road.

Furthermore, wishes to reiterate 2 other major concerns:

- 1) While we note that the intent of the project is create a human-scale roadway that will facilitate transit-oriented development in the Vaughan Metropolitan Centre (VMC) all while ensuring that roadway can serve as vital truck route, we do not understand how both functions can coexist in a safe and coherent manner;



- 2) Linking Portage Parkway to Creditstone will encourage further rezoning and land conversion beyond the limits of the VMC. The City of Vaughan has not provided explanations on how employment lands on the west side of CN's MacMillan Yard will be preserved, in conformity with the Provincial Policy Statement (1.3.2). As such, sensitive land uses may be permitted at insufficient distances from MacMillan Yard and the surrounding industrial uses, and may result in a number of issues stemming from this land use incompatibility.

In light of these major concerns and limitations, and in consideration for the safety and well-being of future occupants, is respectfully requesting that the issues raised above be adequately addressed. Please keep us apprised of the developments in the EA and in the Portage Parkway extension in general.

Regards,

\*\*\*\*\*  
*Le contenu du présent courriel est privilégié, confidentiel et soumis à des droits d'auteur. Il est interdit de l'utiliser ou de le divulguer sans autorisation.*

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REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: July 7, 2015  
Name: ARAD MOHAGHEGH  
Title: PROJECT SPECIALIST, Subways  
Agency: York Region Rapid Transit Corp  
Address: 3601 HWY 7, 12<sup>th</sup> Floor  
Markham Postal Code: L3R 0M3  
Phone: (905) 505-1675 Fax: (905) 886-6969  
Email: ARAD.MOHAGHEGH@York.ca

Does your agency wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)

Yes  No

Part of interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

July 8, 2015

File No.: EA01-06-05

Ms. Sonya Kapusin  
Sonya.Kapusin@cima.ca

RE: City of Vaughan  
Environmental Assessment Study for:  
Portage Parkway Widening from Applewood Crescent to Jane Street,  
and  
Portage Parkway Extension from Jane Street to Creditstone Road

Dear Ms. Kapusin,

This letter is our response to the Notice of Study Commencement for the above noted project. This response acknowledges that the City of Vaughan has indicated that its study is following the approved environmental planning process for a Schedule C project under the Municipal Engineers Association Municipal Class Environmental Assessment (Class EA).

Based on the information submitted, we have identified the following areas of interest with respect to the proposed undertaking:

- Ecosystem Protection and Restoration
- Surface Water
- Groundwater
- Air Quality, Dust and Noise
- Servicing and Facilities
- Contaminated Soils
- Mitigation and Monitoring
- Planning and Policy
- Class EA Process
- Aboriginal Consultation

We are providing the following general comments to assist your team in effectively addressing these areas of interest:

### **Ecosystem Protection and Restoration**

- Any impacts to ecosystem form and function must be avoided where possible. The Environmental Study Report (ESR) should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- All natural heritage features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. Our records confirm that the following sensitive environmental features are located within or adjacent to the study area:
  - Rare Species of flora or fauna
  - Watercourses

We recommend consulting with the Ministry of Natural Resources (MNR), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or

additional study will be necessary to preserve and protect these sensitive features.

### **Surface Water**

- The ESR must include a sufficient level of information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry's *Stormwater Management Planning and Design Manual (2003)* should be referenced in the ESR and utilized when designing stormwater control methods. We recommend that a Stormwater Management Plan should be prepared as part of the Class EA process that includes:
  - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that adequate (enhanced) water quality is maintained
  - Watershed information, drainage conditions, and other relevant background information
  - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
  - Information on maintenance and monitoring commitments.

### **Groundwater**

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the ESR.
- If the potential construction or decommissioning of water wells is identified as an issue, the ESR should refer to Ontario Regulation 903, Wells, under the *Ontario Water Resources Act*.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the ESR. In particular, a Permit to Take Water (PTTW) under the *Ontario Water Resources Act* will be required for any water takings that exceed 50,000 litres per day.



## **Air Quality, Dust and Noise**

- Any potential air quality impacts should be assessed and used in the evaluation of alternatives for the proposed project. Appropriate mitigation measures of any potential effects should be identified.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The ESR should consider the potential impacts of increased noise levels during the operation of the undertaking due to potentially higher traffic volumes resulting from this project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

## **Contaminated Soils**

- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act (EPA)* and Ontario Regulation 153/04, Records of Site Condition, which details the new requirements related to site assessment and clean up. We recommend contacting the ministry's York Durham District Office in Ajax for further consultation if contaminated sites are present.
- The location of any underground storage tanks should be investigated in the ESR. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- Any current or historical waste disposal sites should be identified in the ESR. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the *Environmental Protection Act* may be required for land uses on former disposal sites.
- The ESR should identify any underground transmission lines in the study area. The owners should be consulted to avoid impacts to this infrastructure, including potential spills.

## **Mitigation and Monitoring**

- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- All waste generated during construction must be disposed of in accordance with ministry requirements.

Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the ESR and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly. The proponent's construction and post-construction monitoring plans

should be documented in the ESR.

## **Planning and Policy**

- Parts of the study area are subject to the *Growth Plan for the Greater Golden Horseshoe*. The ESR should demonstrate how the proposed study adheres to the relevant policies in these plans.
- The Provincial Policy Statement contains policies that protect Ontario's natural heritage and water resources, including designated vulnerable areas mapped in source water protection assessment reports under the *Clean Water Act*. Applicable policies should be referenced in the ESR, and the proponent should demonstrate how this proposed project is consistent with these policies. Assessment reports can be found on the Conservation Ontario website at: <http://www.conservation-ontario.on.ca/uncategorised/143-otherswpreionsindex>.

## **Class EA Process**

- The ESR should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making. The ESR must also demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all public consultation efforts undertaken during the planning process. Additionally, the ESR should identify all concerns that were raised and how they have been addressed throughout the planning process. The Class EA also directs proponents to include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment. The ESR should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments) such that all potential impacts can be identified and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the ESR.
- Please include in the ESR a list of all subsequent permits or other approvals that may be required for the implementation of the preferred alternative, including Permits to Take Water, Environmental Compliance Approvals, approval under the *Canadian Environmental Assessment Act* (CEAA), and conservation authority permits.
- Please note that ministry guidelines and other information related to the issues noted above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy> under the publications link. We encourage you to review all the available guides and to reference any relevant information in the ESR.

## **Aboriginal Consultation**

- Your proposed project may have the potential to affect Aboriginal communities who hold or claim Aboriginal or treaty rights protected under Section 35 of Canada's Constitution Act 1982. The Crown has a duty to consult First Nation and Métis communities when it knows about established or credibly asserted Aboriginal or treaty rights, and contemplates decisions or actions that may adversely affect them.
- Although the Crown remains responsible for ensuring the adequacy of consultation with potentially affected Aboriginal communities, it may delegate procedural aspects of the

consultation process to project proponents.

- The environmental assessment process requires proponents to consult with interested persons and government agencies, including those potentially affected by the proposed project. This includes a responsibility to conduct adequate consultation with First Nation and Métis communities.
- The ministry relies on consultation conducted by proponents when it assesses the Crown's obligations and directs proponents during the regulatory process.
- Where the Crown's duty to consult is triggered in relation to your proposed project, the Ontario Ministry of the Environment and Climate Change is delegating the procedural aspects of rights-based consultation to you through this letter.
- Steps that you may need to take in relation to Aboriginal consultation for your proposed project are outlined in the attached "Aboriginal Consultation Information" document. Please complete the checklist contained there, and keep related notes as part of your consultation record. Doing so will help you assess your project's potential adverse effects on Aboriginal or treaty rights.
- You must contact the Director, Environmental Approvals Branch if you have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right, consultation has reached an impasse has been submitted. The ministry will then assess the extent of any Crown duty to consult in the circumstances, and will consider whether additional steps should be taken and what role you will be asked to play in them.

Thank you for the opportunity to comment on this project. A draft copy of the ESR should be sent to this office prior to the filing of the final draft, allowing approximately 30 days review time for the ministry's reviewers to provide comments. Please also forward our office the Notice of Completion and ESR when completed. Should your team have any questions regarding the above, please contact me at 416-326-3469.

Yours sincerely,



Nisha Shirali, M.Pl., MCIP RPP  
Environmental Resource Planner and EA Coordinator  
Air, Pesticides and Environmental Planning

- c. Celeste Dugas, Manager, York Durham District Office, MOECC  
Central Region EA File  
A & P File

## ABORIGINAL CONSULTATION INFORMATION

### ***Consultation with Interested Persons under the Ontario Environmental Assessment Act***

Proponents subject to the Ontario *Environmental Assessment Act* are required to consult with interested persons, which may include First Nations and Métis communities. In some cases, special efforts may be required to ensure that Aboriginal communities are made aware of the project and are afforded opportunities to provide comments. Direction about how to consult with interested persons/communities is provided in the Code of Practice: Consultation in Ontario's Environmental Assessment Process available on the Ministry's website:

<https://www.ontario.ca/environment-and-energy/consultation-ontarios-environmental-assessment-process>

As an early part of the consultation process, proponents are required to contact the Ontario Ministry of Aboriginal Affairs' Consultation Unit and visit Aboriginal Affairs and Northern Development Canada's Aboriginal and Treaty Rights Information System (ATRIS) to help identify which First Nation and Métis communities may be interested in or potentially impacted by their proposed projects.

ATRIS can be accessed through the Aboriginal Affairs and Northern Development Canada website:

[http://sidait-atris.aadnc-aandc.gc.ca/atris\\_online/](http://sidait-atris.aadnc-aandc.gc.ca/atris_online/)

For more information in regard Aboriginal consultation as part of the Environmental Assessment process, refer to the Ministry's website:

[www.ontario.ca/government/environment-assessments-consulting-aboriginal-communities](http://www.ontario.ca/government/environment-assessments-consulting-aboriginal-communities)

You are advised to provide notification directly to all of the First Nation and Métis communities who may be interested in the project. You should contact First Nation communities through their Chief and Band Council, and Metis communities through their elected leadership.

### ***Rights-based consultation with First Nation and Métis Communities***

Proponents should note that, in addition to requiring interest-based consultation as described above, certain projects may have the potential to adversely affect the ability of First Nation or Métis communities to exercise their established or credibly asserted Aboriginal or treaty rights. In such cases, Ontario may have a duty to consult those Aboriginal communities.

Activities which may restrict or reduce access to unoccupied Crown lands, or which could result in a potential adverse impact to land or water resources in which harvesting rights are exercised, may have the potential to impact Aboriginal or treaty rights. For assistance in determining whether your proposed project could affect these rights, please refer to the attached "Preliminary Assessment Checklist: First Nation and Métis Community Interest."

If there is likely to be an adverse impact to Aboriginal or treaty rights, accommodation may be required to avoid or minimize the adverse impacts. Accommodation is an outcome of consultation and includes any mechanism used to avoid or minimize adverse impacts to Aboriginal or treaty rights and traditional uses. Solutions could include mitigation such as adjustments in the timing or geographic location of the proposed activity. Accommodation may in certain circumstances involve the provision of financial compensation, but does not necessarily require it.

For more information about the duty to consult, please see the Ministry's website at:

[www.ontario.ca/government/duty-consult-aboriginal-peoples-ontario](http://www.ontario.ca/government/duty-consult-aboriginal-peoples-ontario)

The proponent must contact the Director, Environmental Approvals Branch if a project may adversely affect an Aboriginal or treaty right, consultation has reached an impasse, or if a Part II Order or an



elevation request is anticipated; the Ministry will then determine whether the Crown has a duty to consult.

The Director of the Environmental Approvals Branch can be notified either by email with the subject line "Potential Duty to Consult" to [EAASIBgen@ontario.ca](mailto:EAASIBgen@ontario.ca) or by mail or fax at the address provided below:

<b>Email:</b>	<a href="mailto:EAASIBGen@ontario.ca">EAASIBGen@ontario.ca</a> Subject: Potential Duty to Consult
<b>Fax:</b>	416-314-8452
<b>Address:</b>	Environmental Approvals Branch 135 St. Clair Avenue West, 1 <sup>st</sup> Floor Toronto, ON, M4V 1P5

### ***Delegation of Procedural Aspects of Consultation***

Proponents have an important and direct role in the consultation process, including a responsibility to conduct adequate consultation with First Nation and Métis communities as part of the environmental assessment process. This is laid out in existing environmental assessment codes of practice and guides that can be accessed from the Ministry's environmental assessment website at

[www.ontario.ca/environmentalassessments](http://www.ontario.ca/environmentalassessments)

The Ministry relies on consultation conducted by proponents when it assesses the Crown's obligations and directs proponents during the regulatory process. Where the Crown's duty to consult is triggered, various additional procedural steps may also be asked of proponents as part of their delegated duty to consult responsibilities. In some situations, the Crown may also become involved in consultation activities.

Ontario will have an oversight role as the consultation process unfolds but will be relying on the steps undertaken and information you obtain to ensure adequate consultation has taken place. To ensure that First Nation and Métis communities have the ability to assess a project's potential to adversely affect their Aboriginal or treaty rights, Ontario requires proponents to undertake certain procedural aspects of consultation.

The proponent's responsibilities for procedural aspects of consultation include:

- Providing notice to the elected leadership of the First Nation and/or Métis communities (e.g., First Nation Chief) as early as possible regarding the project;
- Providing First Nation and/or Métis communities with information about the proposed project including anticipated impacts, information on timelines and your environmental assessment process;
- Following up with First Nation and/or Métis communities to ensure they received project information and that they are aware of the opportunity to express comments and concerns about the project. If you are unable to make the appropriate contacts (e.g. are unable to contact the Chief) please contact the Environmental Assessment and Planning Coordinator at the Ministry's appropriate regional office for further direction.
- Providing First Nation and/or Métis communities with opportunities to meet with appropriate proponent representatives to discuss the project;
- Gathering information about how the project may adversely impact the relevant Aboriginal and/or Treaty rights (for example, hunting, fishing) or sites of cultural significance (for example, burial grounds, archaeological sites);
- Considering the comments and concerns provided by First Nation and/or Métis communities and providing responses;
- Where appropriate, discussing potential mitigation strategies with First Nation and/or Métis communities;

- Bearing the reasonable costs associated with these procedural aspects of consultation, which may include providing support to help build communities' capacity to participate in consultation about the proposed project.
- Maintaining a Consultation Record to show evidence that you, the proponent, completed all the steps itemized above or at a minimum made meaningful attempts to do so.
- Upon request, providing copies of the Consultation Record to the Ministry. The Consultation Record should:
  - summarize the nature of any comments and questions received from First Nation and/or Métis communities
  - describe your response to those comments and how their concerns were considered
  - include a communications log indicating the dates and times of all communications; and
  - document activities in relation to consultation.

Successful consultation depends, in part, on early engagement by proponents with First Nation and Métis communities. Information shared with communities must be clear, accurate and complete, and in plain language where possible. The consultation process must maintain sufficient flexibility to respond to new information, and we trust you will make all reasonable efforts to build positive relationships with all First Nation and Métis communities contacted.

If you need more specific guidance on Aboriginal consultation steps in relation to your proposed project, or if you feel consultation has reached an impasse, please contact the Environmental Assessment and Planning Coordinator at the Ministry's appropriate regional office.

**Preliminary Assessment Checklist: First Nation and Métis Community Interests and Rights**

In addition to other interests, some main concerns of First Nation and Métis communities may pertain to established or asserted rights to hunt, gather, trap, and fish – these activities generally occur on Crown land or water bodies. As such, projects related to Crown land or water bodies, or changes to how lands and water are accessed, may be of concern to Aboriginal communities.

Please answer the following questions and keep related notes as part of your consultation record. “Yes” responses will indicate a potential adverse impact on Aboriginal or treaty rights.

Where you have identified that your project may trigger rights-based consultation through the following questions, you should arrange for a meeting between you and the Environmental Assessment and Planning Coordinator at the Ministry's appropriate regional office to provide an early opportunity to confirm whether Ontario's duty to consult is triggered and to discuss roles and responsibilities in that event.

	YES	NO
1. Are you aware of concerns from First Nation and Métis communities about your project or a similar project in the area?  The types of concerns can range from interested inquiries to environmental complaints, and even to land use concerns. You should consider whether the interest represents on-going, acute and/or widespread concern.		
2. Is your project occurring on Crown land, or is it close to a water body? Might it change access to either?		
3. Is the project located in an open or forested area where hunting or trapping could take place?		
4. Does the project involve the clearing of forested land?		
5. Is the project located away from developed, urban areas?		

<p>6. Is your project close to, or adjacent to, an existing reserve? Projects in areas near reserves may be of interest to the First Nation and Métis communities living there.</p>		
<p>7. Will the project affect First Nations and/or Métis ability to access areas of significance to them?</p>		
<p>8. Is the area subject to a land claim? Information about land claims filed in Ontario is available from the Ministry of Aboriginal Affairs; information about land claims filed with the federal government is available from Aboriginal Affairs and Northern Development Canada.</p>		
<p>9. Does the project have the potential to impact any archaeological sites?</p>		

## Air Quality Impact Assessment Guidance for Schedule C Road Improvements Class EAs

### 1. Study Area

The scope of the AQIA should be determined by the proponent and clearly outlined in the AQIA document based on the number and nature of scenarios/alternatives being considered, for example, the routes under consideration.

The focus should be on defining the “worst case scenario”, whether it is the length of roadway with the highest traffic volumes in close proximity to sensitive receptors or sections of roadways with on and off ramps and overpasses. The end result should be a defined study area.

### 2. Sensitive Receptors

All key and potentially sensitive receptors located in the surrounding area must be identified and included in the modeling. The AQIA should then assess the local air quality impacts at these sensitive receptors.

According to the Ministry of Transportations' *Environmental Guide for Assessing and Mitigating the Air Quality Impacts and Greenhouse Gas Emissions of Provincial Transportation Projects (June, 2012)*, "...local air quality impacts are assumed to be limited to a distance of approximately 500 m from the transportation facility, in each direction." Therefore, the Cartesian grid system used to easily model concentrations at each receptor typically has a grid limit of approximately 500 m from the edge of the subject road.

### 3. List of Parameters

The list of parameters should focus mainly on the key pollutants released from mobile sources such as, but not limited to, the following:

- CO
- NO<sub>x</sub> (with a focus on NO and NO<sub>2</sub>)
- TSP
- PM<sub>10</sub>
- PM<sub>2.5</sub>
- Selected VOCs (benzene, 1-3 Butadiene, formaldehyde, acetaldehyde and acrolein).

Although the focus of these assessments has traditionally been on the 24 hour average concentrations, 1 hour and 8 hour (for CO) averages should also be considered.

### 4. Applicable Guidelines

The applicable guidelines with which to compare modeled concentrations may include:

- MOECC Ambient Air Quality Criteria (AAQCs)
- Canadian Ambient Air Quality Standards (CAAQs)

### 5. Background Data

Background data representative of the study area is generally summarized for the most recent 5 years from the nearest or most representative MOECC AQI and/or NAPS stations so that cumulative impacts are determined at the most impacted receptor.

This data should address 1 and 24 hour averages, 1 hour and 24 hour 90<sup>th</sup> percentile values, and 1 hour and 24 hour maximums as appropriate. However, this will vary depending on the AAQC averaging time for each parameter, for example, the 8 hour averaging time for CO.

### 6. Emission Estimates

Emission estimates are based on current and proposed future traffic counts where either MOVES or Mobile 6.2C is used to generate emission factors.



## 7. Traffic Data

Traffic data including fleet distribution and characteristics, road type, traffic signals, idling conditions, or roundabouts/stop signs may be considered or incorporated into the assessment

## 8. Dispersion Modelling

Dispersion modelling, typically using CAL3QHCR, is conducted to determine maximum pollutant concentrations resulting from implementation of the project and the resulting air quality impacts at the most impacted sensitive receptors for the different scenarios. At a minimum, two modelling scenarios are to be conducted to determine the incremental difference between the current conditions (base case) and future scenario. The timing of the future scenario should be defined and take into consideration projected population growth and traffic/emissions impacts.

As necessary a new link must be defined when the road width, traffic volume, speed, alignment or type of flow changes. The CAL model is able to calculate the contribution from all of the links to each individual receptor so the impact of the entire roadway can be determined.

The five most recent years of meteorological data should be used for dispersion modeling. However, under certain conditions, one year of continuous data may suffice. Surface data can be obtained from facilities such as Pearson International Airport, Toronto Island, Buttonville or site-specific and upper air data obtained from Buffalo, New York.

All supporting documentation and assumptions that are inputted into the models should be summarized as Appendices. A sample electronic CAL3QHCR dispersion model input and output files must be submitted for the ministry's review.

## 9. Results

The predicted results obtained from the dispersion modeling exercise are to be presented in detail in the AQIA and summarized in the ESR. This should include an analysis and discussion of the results and potential air quality impacts of the project.

Results for each contaminant should be discussed separately and should depict predicted maximum 24 hour concentrations at key receptors, the overall maximum predicted concentrations and the combined effect concentrations by adding the 90<sup>th</sup> percentile background concentration to the modeled value. It may also be relevant to discuss receptor specific results.

## 10. Cumulative Impacts

Cumulative effects that the proposed roadway expansion/construction may potentially have on the existing air quality can be readily achieved by adding the modeled results to an estimated "background" concentration for each pollutant.

Comparisons of the cumulative impacts are to be compared with the relevant guidelines. If exceedances or non-conformances are predicted, then a discussion of possible mitigation measures should be discussed. Typically these comprise of coniferous landscaping surrounding the areas of most impacted receptors.

A section on potential air quality impacts from the construction of the proposed undertaking should also be discussed in the AQIA and the ESR.

The ministry is currently preparing draft guidance documents to address cumulative effects in EAs. In the interim, please use the following federal EA resources as references for addressing cumulative effects:

- Cumulative Effects Assessment Practitioners' Guide  
<https://www.ceaa-acee.gc.ca/default.asp?lang=En&n=43952694-1&offset=&toc=hide>

- Reference Guide: Addressing Cumulative Environmental Effects  
<https://www.ceaa-acee.gc.ca/default.asp?lang=En&n=9742C481-1&offset=&toc=hide>

## 11. Climate Change

Where possible, the EA should consider climate change. For example, the EA should examine how the project interrelates with climate change over time. It is recommended that the consideration of climate change in terms of mitigation and adaptation (i.e. the ministry is interested in seeing designs or mitigation in stormwater management to meet extreme weather events which may be beyond the 1:100 year storm events, if applicable), potential effects (i.e. increased traffic or impacts from greenhouse gas emissions) and overall benefits be integrated into the EA methods, as well as the evaluation criteria and indicators.

The ministry is currently preparing draft guidance documents to address climate change in EAs. In the interim, please use the following federal EA resources as references for addressing climate change:

- Operational Policy Statement Assessing Cumulative Environmental Effects under the *Canadian Environmental Assessment Act, 2012*  
<https://www.ceaa-acee.gc.ca/default.asp?lang=En&n=A41F45C5-1&offset=3&toc=show>
- Incorporating Climate Change Considerations in Environmental Assessment: General Guidance for Practitioners  
<https://www.ceaa-acee.gc.ca/default.asp?lang=En&n=1DA9E048-1>

## 12. Regional Impacts

The AQIA should include a section on regional impacts from the proposed undertaking compared with the provincial greenhouse gases totals reported by Environment Canada.

## 13. Summary and Mitigation Measures

The AQIA and ESR should summarize the key conclusions of the study based on the results as provided. In addition, general mitigation measures should be discussed, including those mitigation measures that will be implemented during construction to minimize off-site impacts.

For example, best management practices should be applied to mitigate any air quality impacts caused by construction dust. Please note that the ministry recommends that non-chloride dust suppressants be applied.

For a comprehensive list of fugitive dust prevention and control measures, please refer to *Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities*. Report prepared for Environment Canada. March 2005.

<http://www.bieapfrempp.org/Toolbox%20pdfs/EC%20-%20Final%20Code%20of%20Practice%20-%20Construction%20%20Demolition.pdf>

## 14. Further Guidance

For further guidance, including additional references and information such as prediction of emissions from re-entrained road dust and silt loading factors, please refer to the Ministry of Transportations' *Environmental Guide for Assessing and Mitigating the Air Quality Impacts and Greenhouse Gas Emissions of Provincial Transportation Projects (June, 2012)*.

July 9, 2015

CFN 53390

**BY E-MAIL ONLY** ([Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca))

Ms. Marta Roias, RPP  
City of Vaughan  
2141 Major Mackenzie Drive  
Vaughan, Ontario  
L6A 1T1

Dear Ms. Marta Roias:

**Re: Response to Notice of Commencement  
Portage Parkway Widening from Applewood Crescent to Jane Street  
and Portage Parkway Extension from Jane Street to Creditstone Road  
Municipal Class Environmental Assessment - Schedule C  
Don Watershed; City of Vaughan; Regional Municipality of York**

Toronto and Region Conservation Authority (TRCA) staff received the Notice of Commencement for the above noted Environmental Assessment (EA) on June 17, 2015.

It is our understanding that this undertaking will examine the preferred alternative design for the widening of Portage Parkway from Applewood Crescent to Jane Street (Part A) and the extension of Portage Parkway from Jane Street to Creditstone Road (Part B).

### **TRCA Areas of Interest**

Staff has identified the following Areas of Interest within the study area:

#### TRCA Regulated Areas

- Regulation Limit
- Meander Belt
- Regulatory Flood Plain
- Watercourses

#### TRCA Program and Policy Areas

- Aquatic Species and Habitat
- Aquifers and Hydrogeological Features
- Living City Programs:
  - Renewable Energy
  - Sustainable Communities
- Terrestrial species and Habitat

Please contact the Ministry of Natural Resources to confirm if there are program interests related to this project for:

- Areas of Natural and Scientific Interest
- Provincially Significant Wetlands
- Provincially Endangered Species

Please contact the relevant federal agency to confirm if there are issues related to:

- Asian Long-horned Beetle Regulated Area
- Federally Endangered Species

There may be additional consultation with other federal and provincial agencies to ensure that the requirements of such legislation are met. This list is not inclusive and the onus is on the proponent and its consultants to consult with other agencies as required.

Available mapping and program information regarding these Areas of Interest will be sent under separate cover for your reference. Please ensure that the status, potential impacts and opportunities for enhancement related to these Areas of Interest are documented and assessed through a review of background material, technical study, field assessment and detailed evaluation, as appropriate.

### **Selection of Alternatives**

In consideration of TRCA's *The Living City Policies for Planning and Development in the Watersheds of the Toronto and Region Conservation Authority*, Ontario Regulation 166/06, and TRCA's other programs and policies, staff requires that the preferred alternative meets the following criteria:

1. Prevents the risk associated with flooding, erosion or slope instability.
2. Protects and rehabilitates existing landforms, features and functions.
3. Provides for aquatic, terrestrial and human access.
4. Minimizes water/energy consumption and pollution.
5. Addresses TRCA property and heritage resource concerns.

TRCA staff recommends that a summary of detailed design commitments be included in the EA as a Pre-design Brief. This summary should include, but not be limited to:

- a. An aerial photo indicating the study area, regulated area, existing conditions and preferred solution/design;
- b. Text indicating the preferred alternative solution/design;
- c. A Reference list of alternative solutions and designs considered;
- d. A synopsis of all TRCA requirements and technical commitments.

It is intended that the proponent and their consultants, as well as TRCA, would use the Pre-design Brief during the preliminary stages of detailed design. In the Pre-design Brief, commitments made during the EA would be clearly articulated in order to facilitate a 90 % detailed design submission to TRCA for all required permits. TRCA staff would then be able to review the required studies, reports or plans; and confirm any additional study requirements or revisions to the submitted materials. Ideally, the completion of the Pre-Design Brief will result in a more timely and streamlined permit approval process in the future.

### **TRCA Review**

Prior to selecting the preferred alternative solution and design, please arrange a meeting to discuss issues that relate to TRCA Areas of Interest. In addition, please add TRCA's Don and Highland Watershed, Project Manager, Arlen Leeming, to the project mailing list to receive any public information updates.



A copy of the TRCA Environmental Assessment Review Program Service Delivery Standards, and a summary chart is enclosed for your reference. We recommend you refer to these submission standards during the study to facilitate TRCA review. Please provide the following submissions to expedite TRCA review:

- Notices of public meetings and display material and handouts
- Four hard copies of the Draft EA Document
- One hard copy of the Final EA Document.

Please include a digital copy of all submitted material.

TRCA has been experiencing significant increase in growth area planning and infrastructure implementation review across our jurisdiction. All municipalities that are moving forward with major growth related Environmental Assessments are required to provide support to TRCA for technical review and coordination either through a negotiated service agreement or fee based review. Please be advised that the Authority adopted a new fee schedule on January 2, 2012. Pursuant to Resolution #A287/07, the fee for reviewing this Schedule C project is \$18,480.

Should you have any questions, please contact me at extension 5759 or at [sbevan@trca.on.ca](mailto:sbevan@trca.on.ca).

Yours truly,



Suzanne Bevan  
Senior Planner, Environmental Assessment Planning  
Planning and Development

Encl.: TRCA Areas of Interest Summary Table  
Service Delivery Standards - Recommended TRCA Contact Points

**BY E-MAIL**

cc:

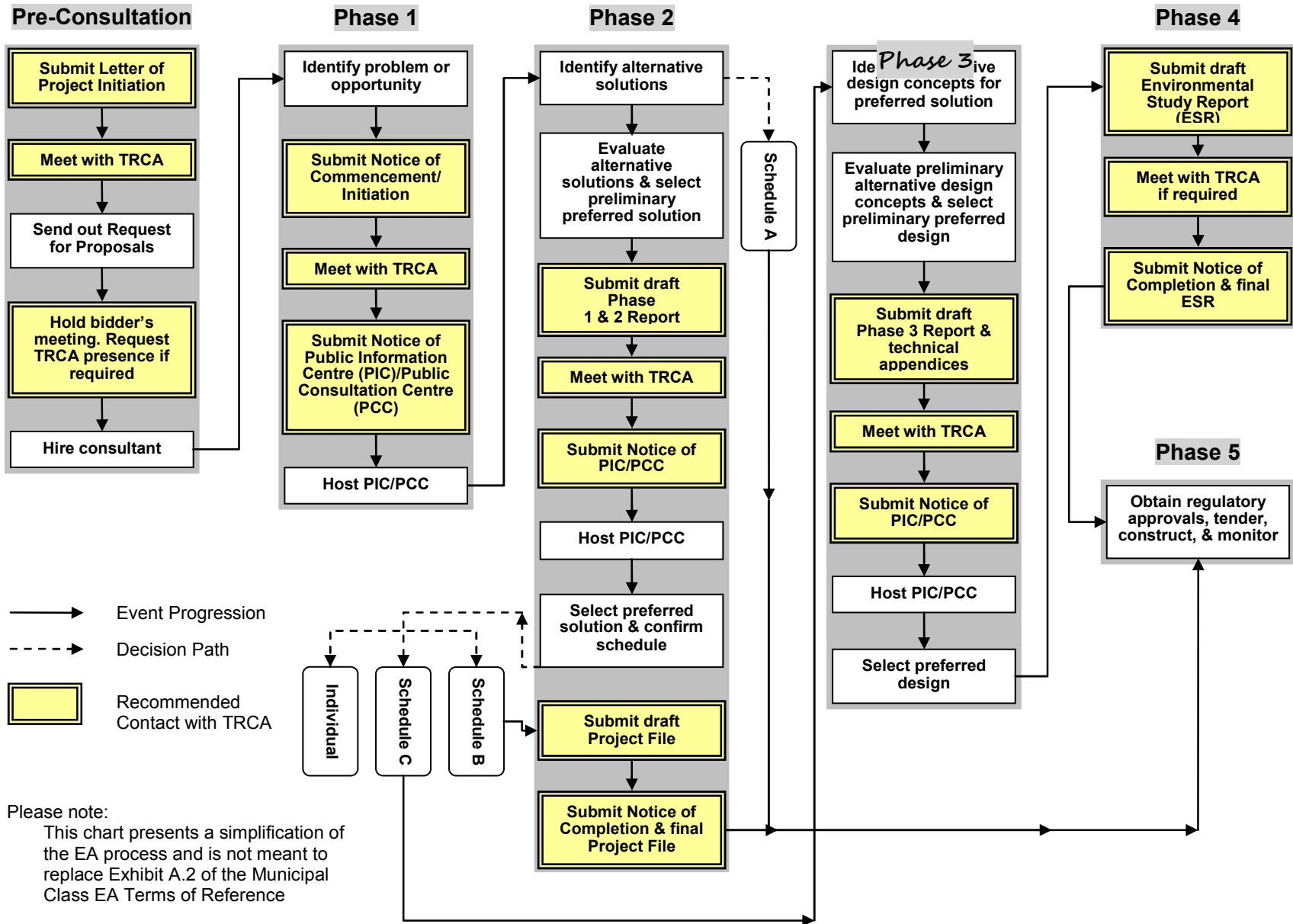
Consultant: Sonya Kapusin, CIMA Canada (Sonya.Kapusin@cima.ca)  
TRCA: Beth Williston, Associate Director, Environmental Assessment Planning  
June Little, Senior Manager, Planning and Development  
Arlen Leeming, Don and Highland Watershed

EA Requirements

Document and assess the status, potential impacts and opportunities for enhancement that relate to the following Areas of Interest through a review of background material, technical study, field assessment and detailed evaluation, as appropriate. Make reference to the applicable Program and Policy documents. Include in the EA Document appendices any minutes, structure summary sheets for watercourses or wetlands, or other material collected through meetings with TRCA staff. Natural features may need to be confirmed on site by TRCA staff.

Area of Interest / Data Availability	Program and Policy Concerns
<b>TRCA REGULATED AREAS</b>	
Regulation Limit <b>GIS data available</b>	<p>In accordance with Ontario Regulation 166/06 (Development, Interference with Wetlands and Alterations to Shorelines and Watercourses), a permit is required from the TRCA prior to any development (e.g. construction) if, in the opinion of TRCA, the control of flooding, erosion, dynamic beaches or pollution or the conservation of land may be affected. The Regulation Limit defines the greater of the natural hazards associated with Ontario Regulation 166/06 (listed below).</p> <p>NOTE: The Regulation Limit provides a geographical screening tool for determining if Ontario Regulation 166/06 will apply to a given proposal. Through site assessment or other investigation, it may be determined that areas outside of the defined Regulation Limit require permits under Ontario Regulation 166/06. In these instances, it is the text of the regulation that will prevail; modifications to the regulation line may be required.</p> <p>Any development within the Regulation Limit must comply with the applicable sections of TRCA's <i>Valley and Stream Corridor Management Program</i>.</p>
Meander Belt	<p>Channel migration has a significant impact on infrastructure, structures and property located near river systems. Determining channel stability is important to ensure that damage from erosion, down-cutting or other natural channel processes is avoided.</p> <p>TRCA may require a meander belt delineation study or fluvial geomorphology analysis to confirm that any development does not conflict with natural channel processes.</p>
Regulatory Flood Plain <b>Engineered maps may be available</b>	<p>The Regulatory Flood Plain is the approved standard used in a particular watershed to define the limit of the flood plain for regulatory purposes. Within TRCA's jurisdiction, the Regulatory Flood Plain is based on the greater of the regional storm, Hurricane Hazel, and the 100 year flood.</p> <p>Any development or alterations to existing structures within the Regulatory Flood Plain may introduce risk to life or property, and may not be compatible with existing natural features. TRCA's framework for Flood Plain Management is the <i>Valley and Stream Corridor Management Program</i>.</p> <p>TRCA may require a flood study or hydraulic update to confirm that there will be no impacts to the storage or conveyance of flood waters.</p>
Watercourses <b>Partial GIS data available</b>	<p>Typically, watercourses are associated with aquatic species and habitat. Any alteration or interference to a watercourse (e.g. straightening, diverting, realigning, altering baseflow) has the potential to impact fish communities, but may also affect the Regulatory Flood Plain, erosion or other natural channel processes. TRCA may require an environmental study or site confirmation of watercourse locations.</p>
<b>TRCA PROGRAM AND POLICY AREAS</b>	
<i>Note: Additional program and policy information may be available at <a href="http://www.trca.on.ca">www.trca.on.ca</a>, or by request.</i>	
Aquatic Species and Habitat <b>GIS data available</b>	<p>TRCA has prepared watershed plans or strategies, as well as watershed-based fisheries management plans for some of its watersheds in partnership with Aurora District MNR. TRCA may require an assessment of the existing aquatic system, together with an evaluation as to how the proposal will meet the objectives articulated in the watershed and watershed-based fisheries</p>

	<p>management plans, as well as prevent negative impacts to the aquatic system.</p> <p>If requested, TRCA will provide an opinion as to whether the project and its implementation will cause <i>serious harm</i> to fish. If <i>serious harm</i> to fish could result, then works will need to be reviewed and authorized by Fisheries and Oceans Canada (DFO).</p>
Aquifers and Hydrogeological Features	<p>The extraction and discharge of groundwater has the potential to negatively impact surrounding natural features. Even small amounts of groundwater extraction may reduce contributions to groundwater dependent features such as wetlands, springs, or fish spawning habitat. In addition, the discharge of groundwater must be controlled to avoid impacts to watercourses and fish habitat from erosion, sedimentation and water quality concerns.</p> <p>TRCA may require geotechnical or hydrogeological investigations to confirm dewatering and discharge requirements, and to identify appropriate mitigation measures with respect to potential impacts to natural features (i.e., wetlands, watercourses, natural features and aquatic habitat).</p>
Living City Programs	<p>The Living City is a vision adopted by TRCA for a new kind of community, where human settlement can flourish forever as part of nature's beauty and diversity. The key objectives of the Living City are: healthy rivers and shorelines; regional biodiversity; sustainable communities; and business excellence.</p> <p>Programs associated with TRCA's Living City include: trails enhancement, renewable energy, sustainable communities, and the <i>Sustainable Technologies Evaluation Program (STEP)</i>.</p>
Terrestrial Natural Heritage System Strategy <b>GIS data available for the refined watershed system</b>	<p>TRCA has identified the need to improve both the quality and quantity of terrestrial habitat. TRCA's <i>Terrestrial Natural Heritage System Strategy</i> sets measurable targets for attaining a healthier natural system by creating an expanded and targeted land base. It includes strategic directions for stewardship and securement of the land base, a land use policy framework to help achieve the target system, and other implementation mechanisms.</p>
Terrestrial Species and Habitat <b>GIS data available</b>	<p>The terrestrial system includes landscape features, vegetation communities and flora and fauna species. Terrestrial species and habitat should be assessed based on their conservation status according to sensitivity to disturbance and specialized ecological needs, as well as rarity.</p> <p>TRCA may require a site assessment and terrestrial inventory to confirm impacts to these resources. TRCA's <i>Terrestrial Natural Heritage Strategy</i> may be applicable to any work that impacts terrestrial species and habitat. In addition, relevant legislation (e.g. <i>Migratory Bird Convention Act</i>, <i>Species at Risk Act</i>) should be applied.</p>



**Service Delivery Standards Recommended TRCA Contact Points in the Municipal Class EA Planning & Design Process**





REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) STUDY FOR:  
Part A: PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
Part B: PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: July 20, 2015  
Name: Dave Mowat  
Title: Consultation Specialist  
Agency: Mississaugas of Scugog Island First Nation  
Address: 22521 Island Road  
Port Perry ON Postal Code: L9L 1B6  
Phone: 905 985 3337 Fax: 905 985 8828  
Email: dmowat@scugogfirstnation.com

Does your agency wish to be kept informed of the Study? (Circle Yes or No)

Yes  No

Does your agency wish to participate as a member of the Technical Agencies Committee?  
(Circle Yes or No)

Yes  No

Part of Interest (Circle Part A or Part B or Both):

Part A  Part B  Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca

## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Tuesday, July 21, 2015 3:19 PM  
**To:**  
**Cc:** Stephen Keen; Sonya Kapusin  
**Subject:** RE: CoV Portage Parkway

Greetings

Further to below... the presentation will be distributed before the end of this week, together with meeting notes from the Technical Agencies Committee and Stakeholders Group jointly held meeting Thursday July 16, 2015.

Regards,  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Monday, July 20, 2015 11:50 AM  
**To:** Roias, Marta  
**Subject:** CoV Portage Parkway

Hello,

Just wanted to follow up, as we have not as yet received the presentation, will it be sent out shortly?

Thanks

REPLY FORM (Please print)

**RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

Date: July 23 2015  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

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Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

## Sonya Kapusin

---

**From:**  
**Sent:** Friday, July 24, 2015 4:03 PM  
**To:** Sonya Kapusin  
**Cc:** marta.roias@vaughan.ca  
**Subject:** FW: Vaughan - Portage Pkwy EA - TAC and Stakeholders Group Meeting No. 1

Hi Sonya,  
Please could you add me to the circulation list. Thank you

---

**From:** Sonya Kapusin [<mailto:Sonya.Kapusin@cima.ca>]  
**Sent:** Thursday, July 23, 2015 3:48 PM  
**To:** Sonya Kapusin  
**Cc:** [Marta.Roias@vaughan.ca](mailto:Marta.Roias@vaughan.ca); Stephen Keen  
**Subject:** B000541\_Vaughan - Portage Pkwy EA\_TAC and Stakeholders Group Meeting No. 1

Good afternoon,

Please find attached notes of our first meeting with the Technical Agencies Committee and Stakeholders Group for the Portage Parkway Class EA, held on July 16. We would appreciate it if you could forward these notes internally to the appropriate contacts within your respective organizations. Please reply to [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca) to advise of any errors or omissions.

Best regards,

---

**Sonya Kapusin, MCIP, RPP**  
Project Manager  
Environmental Planning

### **CIMA+**

**Partners in excellence**  
3027 Harvester Road, Suite 400  
Burlington Ontario L7N 3G7  
CANADA

Tel. : 289 288-0287 # 6812  
Cell. : 905 630-6667  
Fax : 289 288-0285



Do you really need to print this email? Let's protect the environment!

Warning: This email, and any attachment thereof, is the property of CIMA+ and is strictly intended to be used by the person to whom it is addressed. If this email is not



**Ministry of Aboriginal Affairs**

160 Bloor St. East, 9<sup>th</sup> Floor  
Toronto, ON M7A 2E6  
Tel: (416) 326-4740  
Fax: (416) 325-1066  
[www.aboriginalaffairs.gov.on.ca](http://www.aboriginalaffairs.gov.on.ca)

**Ministère des Affaires Autochtones**

160, rue Bloor Est, 9<sup>e</sup> étage  
Toronto ON M7A 2E6  
Tél. : (416) 326-4740  
Télééc. : (416) 325-1066  
[www.aboriginalaffairs.gov.on.ca](http://www.aboriginalaffairs.gov.on.ca)



Reference: EA#2015-172

*Received July 27, 2015*

Sonya Kapusin, MCIP, RPP  
3027 Harvester Road, Ste 400  
Burlington, ON L7N 3G7

**Re: City of Vaughan  
Environmental Assessment Study for:  
Portage Parkway Widening from Applewood Crescent to Jane Street and  
Portage Parkway Extension from Jane Street to Credistone Road**

Dear Sonya Kapusin:

Thank you for informing the Ministry of Aboriginal Affairs (MAA) of your project. Please note that MAA treats all letters, emails, general notices, etc. about a project as a request for information about which Aboriginal communities may have rights or interests in the project area.

As a member of the government review team, the Ministry of Aboriginal Affairs (MAA) identifies First Nation and Métis communities who may have the following interests in the area of your project:

- reserves;
- land claims or claims in litigation against Ontario;
- existing or asserted Aboriginal or treaty rights, such as harvesting rights; or
- an interest in the area of the project.

MAA is not the approval or regulatory authority for your project, and receives very limited information about projects in the early stages of their development. In circumstances where a Crown-approved project may negatively impact a claimed Aboriginal or treaty right, the Crown may have a duty to consult the Aboriginal community advancing the claim. The Crown often delegates procedural aspects of its duty to consult to proponents. Please note that the information in this letter should not be relied on as advice about whether the Crown owes a duty to consult in respect of your project, or what consultation may be appropriate. Should you have any questions about your consultation obligations, please contact the appropriate ministry.

You should be aware that many First Nations and/or Métis communities either have or assert rights to hunt and fish in their traditional territories. For First Nations, these territories typically include lands and waters outside of their reserves.

In some instances, project work may impact aboriginal archaeological resources. If any Aboriginal archaeological resources could be impacted by your project, you should contact your regulating or approving Ministry to inquire about whether any additional Aboriginal communities should be contacted. Aboriginal communities with an interest in archaeological

resources may include communities who are not presently located in the vicinity of the proposed project.

With respect to your project, and based on the brief materials you have provided, we can advise that the project appears to be located in an area where First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation, that could be impacted by your project. Contact information is below:

Chippewas of Georgina Island R.R. #2, P.O. Box N-13 Sutton West, Ontario L0E 1R0	Chief Donna Big Canoe (705) 437-1337 (Fax) 437-4597 <a href="mailto:dbigcanoe@georginaisland.com">dbigcanoe@georginaisland.com</a>
Beausoleil First Nation (Christian Island) 11 O-Gema Miikaan Christian Island, ON L9M 0A9	Chief Roland Monague (705) 247-2051 (Fax) 247-2239 <a href="mailto:bfchief@chimnissing.ca">bfchief@chimnissing.ca</a>
Chippewas of Rama 5884 Rama Road, Suite 200 Rama, Ontario L0K 1T0	Chief Rodney Noganosh (705) 325-3611 (Fax) 325-0879 <a href="mailto:chief@ramafirstnation.ca">chief@ramafirstnation.ca</a>
Mississaugas of the New Credit First Nation 2789 Mississauga Rd., R.R. #6 HAGERSVILLE, Ontario N0A 1H0	Chief Bryan LaForme (905) 768-1133 (Fax) 768-1225 <a href="mailto:bryanlaforme@newcreditfirstnation.com">bryanlaforme@newcreditfirstnation.com</a>

The information upon which the above comments are based is subject to change. First Nation or Métis communities can make claims at any time, and other developments can occur that could result in additional communities being affected by or interested in your undertaking.

Through Aboriginal Affairs and Northern Development (AANDC), the Government of Canada sometimes receives claims that Ontario does not receive, or with which Ontario does not become involved. AANDC's Consultation and Accommodation Unit (CAU) established a "single window" to respond to requests for baseline information held by AANDC on established or potential Aboriginal Treaty and rights. To request information from the Ontario Subject Matter Expert send an email to: [UCA-CAU@aadnc-aandc.gc.ca](mailto:UCA-CAU@aadnc-aandc.gc.ca)

Additional details about your project or changes to it that suggest impacts beyond what you have provided to date may necessitate further consideration of which Aboriginal communities may be affected by or interested in your undertaking. If you think that further consideration may be required, please bring your inquiry to whatever government body

oversees the regulatory process for your project. MAA does not wish to be kept informed of the progress of the project; please be sure to remove MAA from the mailing list.

Yours truly,

A handwritten signature in black ink, appearing to read 'C Troje'. The signature is fluid and cursive, with a large initial 'C' and a stylized 'Troje'.

Corwin Troje  
Manager, Ministry Partnerships Unit  
Aboriginal Relations and Ministry Partnerships Branch







REPLY FORM (Please print)

Rec'd July 13, 2015

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date: July 7, 2015

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Monday, July 20, 2015 10:55 AM  
**To:**  
**Cc:** Stephen Keen; Sonya Kapusin; Hubjer, Selma  
**Subject:** RE: Transport Canada grade crossing standards - Portage Parkway extension

Thanks for attending and your follow up below to last week's meeting.

Looking forward...  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Monday, July 20, 2015 10:40 AM  
**To:** Roias, Marta  
**Cc:**  
**Subject:** Transport Canada grade crossing standards - Portage Parkway extension

Marta,

This is a link to the Transport Canada (TC) setback standard I mentioned last week:  
<https://www.tc.gc.ca/eng/railsafety/grade-crossings-standards-317.htm>

This link leads to the TC webpage which contains the new 2015 crossing regulations:  
<https://www.tc.gc.ca/eng/railsafety/grade-crossings-standards.htm>

Any proposal for a new grade crossing must comply with these standards, and the proposal must be reviewed by the appropriate departments at ; it is important to note that each new grade crossing constitutes a new safety risk, and as such does not support the creation of new crossings, unless the intent is to close an existing crossing to create a safer one. As I mentioned at last week's meeting, additional constraints may preclude a new crossing of active industrial lead on the west side of MacMillan Yard, linking the future extension of Portage Parkway to Creditstone Road.

Furthermore, wishes to reiterate 2 other major concerns:

- 1) While we note that the intent of the project is create a human-scale roadway that will facilitate transit-oriented development in the Vaughan Metropolitan Centre (VMC) all while ensuring that roadway can serve as vital truck route, we do not understand how both functions can coexist in a safe and coherent manner;

- 2) Linking Portage Parkway to Creditstone will encourage further rezoning and land conversion beyond the limits of the VMC. The City of Vaughan has not provided explanations on how employment lands on the west side of CN's MacMillan Yard will be preserved, in conformity with the Provincial Policy Statement (1.3.2). As such, sensitive land uses may be permitted at insufficient distances from MacMillan Yard and the surrounding industrial uses, and may result in a number of issues stemming from this land use incompatibility.

In light of these major concerns and limitations, and in consideration for the safety and well-being of future occupants, is respectfully requesting that the issues raised above be adequately addressed. Please keep us apprised of the developments in the EA and in the Portage Parkway extension in general.

Regards,

\*\*\*\*\*  
*Le contenu du présent courriel est privilégié, confidentiel et soumis à des droits d'auteur. Il est interdit de l'utiliser ou de le divulguer sans autorisation.*

*This e-mail message is privileged, confidential and subject to copyright. Any unauthorized use or disclosure is prohibited.*  
\*\*\*\*\*

This e-mail, including any attachment(s), may be confidential and is intended solely for the attention and information of the named addressee(s). If you are not the intended recipient or have received this message in error, please notify me immediately by return e-mail and permanently delete the original transmission from your computer, including any attachment(s). Any unauthorized distribution, disclosure or copying of this message and attachment(s) by anyone other than the recipient is strictly prohibited.

## Maram Miri

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**Sent:** Tuesday, July 21, 2015 3:19 PM  
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**Cc:** Stephen Keen; Sonya Kapusin  
**Subject:** RE: CoV Portage Parkway

Greetings

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Regards,  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
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ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD**

Date: July 23 2015  
Name: \_\_\_\_\_  
Affiliation (if applicable): \_\_\_\_\_  
Address: \_\_\_\_\_  
Postal Code: \_\_\_\_\_  
Phone: \_\_\_\_\_ Fax: \_\_\_\_\_  
Email: \_\_\_\_\_

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes       No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes       No

Part of interest (Circle Part A or Part B or Both):

Part A       Part B       Both (Part A and Part B)

Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
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Fax: (289) 288-0285  
Email: [sonya.kapusin@cima.ca](mailto:sonya.kapusin@cima.ca)

## Sonya Kapusin

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**To:** Sonya Kapusin  
**Cc:** marta.roias@vaughan.ca  
**Subject:** FW: Vaughan - Portage Pkwy EA - TAC and Stakeholders Group Meeting No. 1

Hi Sonya,  
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**From:** Sonya Kapusin [<mailto:Sonya.Kapusin@cima.ca>]  
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Best regards,

---

**Sonya Kapusin, MCIP, RPP**  
Project Manager  
Environmental Planning

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## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Monday, August 10, 2015 4:02 PM  
**To:**  
**Cc:** Stephen Keen; Sonya Kapusin,  
**Subject:** City of Vaughan Portage Parkway EA Study - Link to City Webpage

Following up to your phone inquiry this afternoon...

- Information on the Environmental Assessment Study for Portage Parkway can be accessed by visiting : [Portage Parkway Environmental Assessment Study](#). Please provide your contact and full mailing address information, if you wish to be added to the study mailing list to receive future notices.
- With respect to any inquiries regarding the Vaughan Metropolitan Centre Secondary, please contact the Planning Department.

Regards,  
Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



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## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Monday, August 17, 2015 2:40 PM  
**To:** Sonya Kapusin  
**Subject:** FW: City of Vaughan EA/Portage Parkway Extension/ Phase A and B  
**Attachments:** Untitled.pdf

---

**From:**  
**Sent:** Wednesday, June 24, 2015 12:24 PM  
**To:** Sonya Kapusin  
**Subject:** City of Vaughan EA/Portage Parkway Extension/ Phase A and B

Hello Sonya,  
I would like to be kept informed, as noted in the attached reply form.  
Regards,

-----  
-----  
This message contains confidential information and is intended only for the individual named. If you are not the named addressee you should not disseminate, distribute or copy this e-mail. Please notify the sender immediately by e-mail if you have received this e-mail by mistake and delete this e-mail from your system. E-mail transmission cannot be guaranteed to be secure or error-free as information could be intercepted, corrupted, lost, destroyed, arrive late or incomplete, or contain viruses. The sender therefore does not accept liability for any errors or omissions in the contents of this message, which arise as a result of e-mail transmission.

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REPLY FORM (Please print)

RE: CITY OF VAUGHAN  
ENVIRONMENTAL ASSESSMENT (EA) FOR:  
PORTAGE PARKWAY WIDENING, APPLEWOOD CRESCENT TO JANE STREET  
PORTAGE PARKWAY EXTENSION, JANE STREET TO CREDITSTONE ROAD

Date:

Name:

Affiliation (if applicable):

Address:

Postal Code:

Phone:

Fax:

Email:

Do you wish to be kept informed of the Study? (Circle Yes or No)

Yes

No

Do you wish to participate as a member of the Stakeholders Group? (Circle Yes or No)

Yes

No

Part of interest (Circle Part A or Part B or Both):

Part A

Part B

Both (Part A and Part B)

Comments: The Parkway Road extension must  
eventually extend over to Keele Street,  
over the CN property, to achieve traffic  
relief from Hwy 7. Has the City of Vaughan  
or the Region of York considered studying  
the extension of Langstaff Road from  
Creditstone to Keele Street, rejoining the existing  
Langstaff Road?

Please return this form to the contact below by June 26, 2015:

Sonya Kapusin, MCIP, RPP  
EA Planning  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7

Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: sonya.kapusin@cima.ca

## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Tuesday, August 18, 2015 5:19 PM  
**To:**  
**Cc:** Sonya Kapusin; Stephen Keen; Hubjer, Selma  
**Subject:** FW: City of Vaughan EA/Portage Parkway Extension/ Phase A and B  
**Attachments:** Untitled.pdf; REVISED Nov 24 2014 VOP 2010 - Oct.21.2014 Consolidation - Schedule 9 .pdf

Greetings

Thank you for your reply ([Untitled.pdf](#)) and apology for delay in follow up to your question noted under comments re: *Has the City of Vaughan or York Region considered studying the extension of Langstaff Road from Creditstone Road to Keele Street, rejoining the existing Langstaff Road?*

- Network improvements including the completion of incomplete grid connections such as Langstaff Road from Creditstone Road to Keele Street (policy 4.2.1.6) from part of the [City of Vaughan Official Plan 2010](#) - Planned Future Transportation Network. (Schedule 9 attached for reference.)
- For further information on consideration of study, please contact York Region - Stephen Collins at 905 830-4444 ext 75949.

Looking forward to your continued interest...

Marta

Marta Roias , RPP  
Transportation Project Manager  
Development Engineering and Infrastructure Planning Services  
City of Vaughan | 2141 Major Mackenzie Drive  
Vaughan, ON L6A 1T1  
Bus: 905-832-8585 Ext. 8026 | Fax: 905-832-6145  
[www.vaughan.ca](http://www.vaughan.ca)



---

**From:**  
**Sent:** Wednesday, June 24, 2015 12:24 PM  
**To:** Sonya Kapusin  
**Subject:** City of Vaughan EA/Portage Parkway Extension/ Phase A and B

Hello Sonya,  
I would like to be kept informed, as noted in the attached reply form.  
Regards,

-----

## Maram Miri

---

**From:**  
**Sent:** Thursday, October 29, 2015 5:21 PM  
**To:** Sonya Kapusin  
**Cc:**  
**Subject:** FW: B000541\_Vaughan - Portage Pkwy EA\_Stakeholders Group Meeting No. 2

**Sonya,**

has reviewed the material provide and we have no further comments at this time.

Thank you

---

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---

**From:** Sonya Kapusin [<mailto:Sonya.Kapusin@cima.ca>]  
**Sent:** Friday, October 23, 2015 1:05 PM  
**To:** Sonya Kapusin  
**Subject:** B000541\_Vaughan - Portage Pkwy EA\_Stakeholders Group Meeting No. 2

Good afternoon,

Please find attached agenda and presentation from the Stakeholders Group meeting held on October 20 for the Portage Parkway Class EA. We welcome your comments on the draft Public Information Centre materials by November 3, 2015.

Regards,

---

**Sonya Kapusin, MCIP, RPP**  
Project Manager  
Environmental Planning

### **CIMA+**

**Partners in excellence**  
3027 Harvester Road, Suite 400  
Burlington Ontario L7N 3G7  
CANADA

Tel. : 289 288-0287 # 6812  
Cell. : 905 630-6667  
Fax : 289 288-0285

---

**Ministry of Tourism,  
Culture and Sport**

Culture Services Unit  
Programs and Services Branch  
401 Bay Street, Suite 1700  
Toronto ON M7A 0A7  
Tel: 416 314 7147  
Fax: 416 212 1802

**Ministère du Tourisme,  
de la Culture et du Sport**

Unité des services culturels  
Direction des programmes et des services  
401, rue Bay, Bureau 1700  
Toronto ON M7A 0A7  
Tél: 416 314 7147  
Télééc: 416 212 1802



February 4, 2016 (EMAIL ONLY)

Henry Cary, PhD, RPA  
Archaeologist/Built Heritage Specialist  
Golder Associates  
33 Alderney Drive, Suite 460  
Dartmouth, NS B2& 2N4  
E: Henry\_Cary@golder.com

**RE: MTCS file #: 0003035**  
**Proponent: City of Vaughan**  
**Subject: Heritage Impact Assessment**  
**Portage Parkway Widening and Extension**  
**Location: City of Vaughan, Ontario**

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Dear Dr. Cary:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the Heritage Impact Assessment for your project. The report concludes that there is no potential for impact to built heritage resources or cultural heritage landscapes resulting from this project, and as such recommends no mitigation measures. MTCS is satisfied with these recommendations.

Thank you for consulting MTCS on this project: please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Dan Minkin  
Heritage Planner  
Dan.Minkin@Ontario.ca

Copied to: Marta Roias, RPP  
City of Vaughan



The current scope of work does not include conditions 8, 11 and 12 in The Guidelines. These refer to detailed modelling, an assessment of Climate Change, and a regional impact assessment. For similar Municipal Class EAs, this level of assessment has not been required and a qualitative approach, as described above, was considered to be sufficient.

We trust this letter has addressed your comments. Please do not hesitate to contact the undersigned at (289) 288-0287 ext. 6834 or [Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca) if you have any questions or need clarification.

Yours sincerely,

Stephen Keen, P.Eng.  
Consultant Project Manager

cc: Marta Roias, City of Vaughan



**PUBLIC INFORMATION CENTRE No. 1**

**COMMENT FORM (Please print)**

**RE: Environmental Assessment Study  
Part A: Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road  
City of Vaughan**

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

**Do you wish to be kept informed of the study? (Circle yes or no)**

Yes       No

**Comments (Please use the back of this sheet or an extra sheet if you need additional space):**

Thanks for keeping us updated!  
\_\_\_\_\_  
\_\_\_\_\_  
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\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Please return this form to the contact below by December 11, 2015:**

**Stephen Keen, P.Eng.,  
CIMA Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: stephen.keen@cima.ca**



With the exception of personal information, all comments will become part of the public record.

**PUBLIC INFORMATION CENTRE No. 1****COMMENT FORM (Please print)**

**RE: Environmental Assessment Study  
Part A: Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road  
City of Vaughan**

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

**Do you wish to be kept informed of the study? (Circle yes or no)**

Yes       No

**Comments (Please use the back of this sheet or an extra sheet if you need additional space):**

*Please add to stakeholder group.*

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**Please return this form to the contact below by December 11, 2015:**

**Stephen Keen, P.Eng.,  
CIMA Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: [stephen.keen@cima.ca](mailto:stephen.keen@cima.ca)**



**PUBLIC INFORMATION CENTRE No. 1**

**COMMENT FORM (Please print)**

**RE: Environmental Assessment Study  
Part A: Portage Parkway Widening from Applewood Crescent to Jane Street, and  
Part B: Portage Parkway Extension from Jane Street to Creditstone Road  
City of Vaughan**

Name: \_\_\_\_\_

Affiliation (if applicable): \_\_\_\_\_

Address: \_\_\_\_\_

Postal Code: \_\_\_\_\_

Phone: \_\_\_\_\_ Fax: \_\_\_\_\_

Email: \_\_\_\_\_

**Do you wish to be kept informed of the study? (Circle yes or no)**

**Yes**       **No**

**Comments (Please use the back of this sheet or an extra sheet if you need additional space):**

\_\_\_\_\_  
\_\_\_\_\_  
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**Please return this form to the contact below by December 11, 2015:**

**Stephen Keen, P.Eng.,  
CIMA Project Manager  
3027 Harvester Road, Suite 400  
Burlington, ON L7N 3G7**

**Phone: (289) 288-0287  
Fax: (289) 288-0285  
Email: stephen.keen@cima.ca**



With the exception of personal information, all comments will become part of the public record.



## Maram Miri

---

**From:**  
**Sent:** Tuesday, March 29, 2016 10:07 AM  
**To:** Marta Roias  
**Cc:** Stephen Keen; Hubjer, Selma; Sonya Kapusin,  
Jennifer.Logullo@vaughan.ca;  
(gerardo.paezalonso@vaughan.ca); ; Pearce, Andrew;  
**Subject:** FW: CoV Portage Parkway FW: B541 - Vaughan Preliminary Preferred Design PIC 2

Good morning Marta,

We thank you for providing us with the drawing files for the latest preliminary preferred design. We are pleased to provide our comments with respect to the latest preliminary preferred design as presented at the Public Information Centre 2 for the Portage Parkway Environmental Assessment Study. The preliminary preferred design is appropriate and satisfactory as it relates to our development

We have worked closely and cooperatively with the City, its consultant CIMA and other stakeholders through this iterative EA process to arrive at this preliminary preferred design. As you note below, the preliminary preferred design is subject to the study process, therefore we note that any deviation from this preliminary preferred design is of potential concern to the viability of our development. As such, we request notification of future stages of the study process to its completion.

Thank you

---

**From:** Roias, Marta [<mailto:Marta.Roias@vaughan.ca>]  
**Sent:** Monday, March 14, 2016 4:06 PM  
**To:**  
**Cc:** 'Stephen Keen ([Stephen.Keen@cima.ca](mailto:Stephen.Keen@cima.ca))'; Hubier, Selma; 'Sonya Kapusin ([Sonya.Kapusin@cima.ca](mailto:Sonya.Kapusin@cima.ca))'; ; Cappola-Logullo, Jennifer; ; Pearce, Andrew;  
**Subject:** CoV Portage Parkway FW: B541 - Vaughan Preliminary Preferred Design PIC 2

Following up to your request on March 11, 2016... CAD drawing files for the latest Preliminary Preferred Design presented at Public Information Centre 2 for the Portage Parkway Environmental Assessment Study are available from the **file transfer site** link below (courtesy of CIMA). Please note that the Preliminary Preferred Design is subject to study process.

Looking forward...  
Marta

**Marta Roias, RPP**  
**Transportation Project Manager**  
905-832-8585, ext. 8026 [marta.roias@vaughan.ca](mailto:marta.roias@vaughan.ca)

**City of Vaughan | Development Engineering and Infrastructure Planning**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:** [hongtao.gao@cima.ca](mailto:hongtao.gao@cima.ca) [<mailto:hongtao.gao@cima.ca>]  
**Sent:** Thursday, March 10, 2016 9:47 AM  
**To:** Roias, Marta  
**Subject:** CIMA+'s file transfer web site - Project : Portage Design File

Hi [marta.roias@vaughan.ca](mailto:marta.roias@vaughan.ca),

This e-mail has been sent to you to give you access to file transfer Website for the project 'Portage Design File'.

### **Access the file transfer site**

#### **Access the file transfer site in Advanced Mode\***

Access to the file transfer site will be automatically deactivated after **60 consecutive days** of inactivity. The file transfer site allows you to receive, as well as send files (Advanced Mode is recommended for sending files). Refer to [online help](#) for further information. Powered by [SoleWeb](#).

**If the link doesn't work, please Copy / Paste the following link in your browser:**

\* This mode lets you send multiple files or directories simultaneously by using ActiveX.

[hongtao.gao@cima.ca](mailto:hongtao.gao@cima.ca)

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## Maram Miri

---

**From:** Sonya Kapusin  
**Sent:** Tuesday, April 26, 2016 8:20 AM  
**To:** Maram Miri  
**Subject:** FW: Portage Parkway Class EA

### Sonya Kapusin

Tel: 289-288-0287 x 6812  
Fax: 289-288-0285

**From:**  
**Sent:** Friday, February 05, 2016 8:50 AM  
**To:** Sonya Kapusin  
**Subject:** RE: Portage Parkway Class EA

Thank you for the presentation.

Please keep us informed of future meetings..

Regards,

**From:** Sonya Kapusin [<mailto:Sonya.Kapusi@portageparkway.ca>]  
**Sent:** Wednesday, February 03, 2016 3:26 PM  
**To:** Sonya Kapusin

**Subject:** Portage Parkway Class EA

Good afternoon,

Please find attached agenda and presentation for the Portage Parkway Class EA. Your comments on the presentation by **February 16, 2016** would be appreciated.

The presentation can be downloaded from our ftp site by clicking on the following link:  
[Access the file transfer site](#)

Best Regards,

---

**Sonya Kapusin, MCIP, RPP**  
Project Manager, Environmental Planning



3027 Harvester Road, Suite 400  
Burlington Ontario L7N 3G7  
CANADA  
Tel: 289-288-0287 ext. 6812 / Fax: 289-288-0285



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## Maram Miri

---

**From:** Sonya Kapusin  
**Sent:** Tuesday, April 26, 2016 8:19 AM  
**To:** Maram Miri  
**Subject:** FW: Extension of Portage Parkway  
**Attachments:**

### Sonya Kapusin

Tel: 289-288-0287 x 6812  
Fax: 289-288-0285

---

**From:**  
**Sent:** Tuesday, February 23, 2016 12:58 PM  
**To:** Stephen Keen  
**Subject:** Re: Extension of Portage Parkway

Stephen,

In follow up to our meeting on February 4, 2016, please see attached my letter setting out my client's concerns about the impact of the proposed extension of Portage Parkway.

Regards,

February 23, 2016

CIMA Partners in Excellence  
3027 Harvester Road  
Suite 400  
Burlington, Ontario L7N 3G7

Attention: Stephen Keen - Director Transporting Planning

Dear Stephen:

Re:

Expansion of Portgace Parkway

At our site visit on February 4, 2016, you asked that the owners of the property, submit in writing their concerns about the impact of the proposed extension of Portgace Parkway, running east from Jane Street. concerns are as follows:

1. By closing off the present driveway access to the property from Jane Street, parking spaces will be lost along the southern portion of the property.
2. The closing of the Jane Street access will greatly diminish accessibility to and therefore the attractiveness of the property for current and future tenants. is therefore looking for a full access driveway entrance from Jane Street at the north end of the property.
3. The construction of Portgace Parkway as contemplated will require acquisition by the municipality of a strip of land at the south end of property. This will greatly diminish truck access to the shipping door and shipping area for the industrial unit that is located at the south end of the property, adjacent to the proposed new road.
4. The proposed access driveway to the property from Portgace Parkway does not appear to line up directly with the proposed access driveway to the condominium development proposed for the lands to the south. is concerned that the lack of coordination of these access points to the two properties will create difficulties for vehicles entering and exiting both properties, in particular large trucks

5. \_\_\_\_\_ is asking for confirmation that any amendments made to the site  
plan \_\_\_\_\_ such as reduction in the available parking, will not  
put \_\_\_\_\_ in breach of any by-law.

I thank you for your time and attention, and look forward to hearing from you  
in addressing \_\_\_\_\_ concerns.

Yours truly,

## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Wednesday, April 27, 2016 2:50 PM  
**To:** Stephen Keen; Sonya Kapusin; Hubjer, Selma  
**Cc:** Maram Miri  
**Subject:** FW: Portage Parkway Open House - City of Vaughan

---

**From:** .  
**Sent:** Wednesday, April 27, 2016 2:46 PM  
**To:** Maram Miri  
**Cc:** Roias, Marta;  
**Subject:** RE : Portage Parkway Open House - City of Vaughan

Thank you Maram.

will not be available to attend, but does not have additional comments at this time.

Please keep us informed as the process progresses.

Best regards.

---

**De :** Maram Miri [Maram.Miri@cima.ca]  
**Envoyé :** 25 avril 2016 16:38  
**Cc :** Roias, Marta; Hubjer, Selma; Pearce, Andrew; 'Woo, Danny'; 'Macchiusi, Dino'; 'Cappola-Logullo, Jennifer'; 'Roots, Amy'; 'Paez Alonso, Gerardo'; Simmonds, Tim; 'Facchini, Frank'; 'Lue, Stephen'; 'Dixon, Steven'; 'Yousaf, Saad'; [Michael.Frieri@vaughan.ca](mailto:Michael.Frieri@vaughan.ca); [Margie.Chung@vaughan.ca](mailto:Margie.Chung@vaughan.ca); 'Paul.Salerno@vaughan.ca'; 'Daniel.Rende@vaughan.ca'; Stephen Keen; Sonya Kapusin; Maram Miri  
**Objet :** Portage Parkway Open House - City of Vaughan

Dear Stakeholders,

You are invited to attend an Open House on May 5, 2016, to review specific issues associated with the preliminary preferred design of Portage Parkway. The open house will be an opportunity for affected landowners on Portage Parkway to review the preliminary preferred design in the vicinity of their property. Please find attached letter with the invitation details.

Thank you,

---

**Maram Miri**  
EIT  
Traffic Engineering, Transportation





## Maram Miri

---

**From:** Roias, Marta <Marta.Roias@vaughan.ca>  
**Sent:** Friday, May 06, 2016 4:48 PM  
**To:**  
**Cc:** Stephen Keen; ; Hubjer, Selma; Sonya Kapusin;  
**Subject:** FW: EA Portage Parkway  
**Attachments:** City of Vaughan Portage Parkway EA Invite to May 5 2016 Open House.pdf

Thanks for the opportunity earlier this afternoon to discuss the Portage Parkway Environmental Assessment Study and arrangements for meeting with you , on site . on Thursday May 12, 2016 at 3:00 p.m. A calendar item is forthcoming in this regard.

As mentioned - the EA study initiated in June 2015 is nearing completion and identifies property requirements for the widening, extension and improvements. I highlight that the acquisition and securing of lands from properties for the Preliminary Preferred Design is the subject of a separate and subsequent process(es). I will endeavor to have City Real Estate attend the meeting to address questions.

Looking forward...  
Marta

**Marta Roias, RPP**  
**Transportation Project Manager**  
905-832-8585, ext. 8026 [marta.roias@vaughan.ca](mailto:marta.roias@vaughan.ca)

**City of Vaughan | Development Engineering and Infrastructure Planning**  
2141 Major Mackenzie Dr., Vaughan, ON L6A 1T1  
[vaughan.ca](http://vaughan.ca)



---

**From:**  
**Sent:** Wednesday, May 04, 2016 6:57 PM  
**To:** stephen.keen@cima.ca; Roias, Marta  
**Cc:**  
**Subject:** EA Portage Parkway

Good evening Stephen and Marta.

I trust that my email finds you well.

I am writing to you at this time that we have been retained by

located at



## Maram Miri

---

**From:** >  
**Sent:** Thursday, June 30, 2016 2:23 PM  
**To:** Roias, Marta  
**Cc:** Stephen Keen; Sonya Kapusin; Maram Miri; Hubjer, Selma; Pearce, Andrew;  
**Subject:** RE: City of Vaughan Portage Parkway Environmental Assessment Study - Draft Environmental Study Reports Part A and Part B - Stakeholders Group Request for Review and Comment

Good afternoon Marta,

We thank you for providing us with the Draft Environmental Study Report indicating the recommended design for the Part A future Portage Parkway road alignment, located in the vicinity of our development, which is located of Jane Street and the future Portage Parkway extension. We are pleased to provide our comments with respect to the Draft Environmental Study Report for Part A, which you have advised recommends a road alignment that maintains the same road alignment as in the preferred design presented at the Public Information Centre 2 in March 2016. Our comments are similar to those we provided on March 29, 2016 following Public Information Centre 2.

The Draft Environmental Study Report and the recommended alignment for Part A is appropriate and satisfactory as it relates to our development located at . We have worked closely and cooperatively with the City, its consultant CIMA and other stakeholders through this iterative EA process to arrive at the alignment recommended in the Draft Environmental Study Report. We note that any deviation from the alignment recommended in the Draft Environmental Study Report for Part A is of potential concern to the viability of our development at .

As such, we request notification of the filing of the Notice of Completion of the Environmental Study Report for Part A, through to its conclusion.

Thank you

---

**From:** Roias, Marta [mailto:Marta.Roias@vaughan.ca]  
**Sent:** Tuesday, June 14, 2016 5:31 PM  
**To:** ,

**Cc:** Stephen Keen (Stephen.Keen@cima.ca); Sonya Kapusin (Sonya.Kapusin@cima.ca); Maram.Miri@cima.ca; Hubjer, Selma; Pearce, Andrew; Roias, Marta

**Subject:** City of Vaughan Portage Parkway Environmental Assessment Study - Draft Environmental Study Reports Part A and Part B - Stakeholders Group Request for Review and Comment

Greetings,

- Following Public Information Centre 2 on March 9, 2016 and the Open House on May 5, 2016, the Portage Parkway Environmental Study Assessment is progressing to completion in accordance with the Municipal Class EA for the following Schedule C projects:
  - **Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)**
  - **Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)**
- At this time ...
  - **Draft Environmental Study Reports (ESRs)** documenting the Environmental Assessment study (planning and design) process for the projects are being provided for your review and comment. Draft ESRs and supporting technical appendices are available for download from CIMA's file transfer site [here](#) (and through attached outlook item) as follows:
    - Draft ESR - Portage Parkway Widening and Easterly Extension to West of Black Creek ( Part A) B000541 Draft ESR Part A e02.pdf
    - Draft ESR - Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B) B000541 Draft ESR Part B e02.pdf
    - Appendix A (Draft Technical Reports) B000541 Appendix A All Reports e02.pdf
    - Appendix C (Preliminary Preferred Drawings) B000541 Appendix C Preferred Drawing e01-Plate1-4.pdf
  - **Please provide written comments by June 30, 2016.** Note that the formal minimum 30 day review period in accordance with the Municipal Class EA will follow at the time of issuing Notices of Completion. Notices and communication in this regard forthcoming.
- A recommendation report with respect to issuing Notices of Completion and including next steps in finalizing Environmental Study Reports is an item on the agenda at City of Vaughan Committee of the Whole meeting on June 21, 2016. The report is available [here](#).

Please advise of any difficulties with accessing the files/documents.

On behalf of the Portage Parkway EA project team, Thank You for your continued participation and input.

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**CITY OF VAUGHAN**

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, Report No. 27, of the Committee of the Whole, which was adopted without amendment by the Council of the City of Vaughan on June 28, 2016.

**14 CLASS ENVIRONMENTAL ASSESSMENT STUDY NOTICES OF COMPLETION  
PORTAGE PARKWAY WIDENING AND EASTERLY EXTENSION TO WEST  
OF BLACK CREEK (PART A); AND PORTAGE PARKWAY EXTENSION FROM WEST  
OF BLACK CREEK TO CREDITSTONE ROAD (PART B)  
WARD 4 - VAUGHAN METROPOLITAN CENTRE**

**The Committee of the Whole recommends approval of the recommendation contained in the following report of the Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning, dated June 21, 2016:**

**Recommendation**

The Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning recommend:

1. THAT staff issue Notices of Completion and place the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process for :
  - a) Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
  - b) Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

**Contribution to Sustainability**

Consistent with the Goals and Objectives of the City's Community Sustainability and Environmental Master Plan (Green Directions Vaughan), the Environmental Assessment Study advances and completes the planning and design process concurrently for the Portage Parkway Widening and Easterly Extension to West of Black Creek and Portage Parkway Extension from West of Black Creek to Creditstone Road, the projects will contribute to:

- transformation of the transportation network and system supporting intensification, sustainable growth and development of the Vaughan Metropolitan Centre (VMC)
- establishing a green and complete street network of sidewalks, paths and trails that support all modes of non-vehicular transportation
- developing a network of primary roads that support efficient and accessible public and private transit
- reducing single occupant vehicle trips by supporting active transportation, carpooling and public transit

**Economic Impact**

The Class Environmental Assessment Study for the Portage Parkway Widening and Easterly Extension to Creditstone Road is being funded from approved Capital Projects DT-7073-11 and DT-7071-11 with funding from City-wide Development Charges.

The estimated project cost associated with the recommended Preliminary Preferred Design for the Portage Parkway widening, extension and improvements, including engineering, construction, utility relocations and other project costs is approximately \$15.4 million as detailed below:

- \$9.2 million for Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 2

- \$6.2 million for Portage Parkway Extension from West of Black Creek to Creditstone Road including \$3.2 million for the crossing of the Black Creek channel (Part B)

Estimated project costs for the recommended Preliminary Preferred Design do not include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

The Portage Parkway improvements are included as a project in the 2013 Development Charges Background Study at an estimated cost of \$14.34 million. Accordingly, the increase in the estimated project cost will be captured in the next update of the Development Charge Background Study, which is scheduled for 2018.

Funding for the completion of detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street is programmed under Capital Project DT-7114-16 from City-wide Development Charges.

#### **Communication**

Key to study communication is the implementation of a comprehensive and responsive Consultation Plan, to ensure meaningful consultation with internal and external stakeholders while meeting mandatory points of contact in accordance with Municipal Class EA for Schedule C projects. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/ regulatory agencies at study milestones. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones. Draft Executives Summaries, attachments 1 and 2 respectively, provide further details.

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. Integral to the Consultation Plan was internal stakeholder communication that included coordination with the VMC Project Management team with respect to ongoing and emerging projects and initiatives in the VMC.

A Technical Agencies Committee (TAC) comprised of interested representatives of regulatory and review agencies was established upon project commencement. The TAC met at key study milestones and ahead of Public Information Centres. The Consultation Plan provided for separate and focused technical meetings with approving authorities as required to review project impacts, mitigation measures and approval requirements.

A Stakeholder Group (SG) comprised of property owners/ representatives of property owners and development applicants generally within 200 metres of the Portage Parkway corridor were invited to participate. The SG met at key study milestones and ahead of Public Information Centres. The Consultation Plan provided for focused one- on-one meetings with affected property owners as required. The Portage Parkway EA project team and City staff continues to consult and actively dialogue with stakeholders and affected property owners with respect to minimizing and mitigating impacts while finalizing the Environmental Study Reports.

Notices were communicated to the public through direct mailing to a broader notice area bounded by Langstaff Road (north), Highway 407 (south), CN MacMillan Yard (east) and Weston Road (west) and by publication in the Vaughan Citizen and Thornhill Liberal.

#### **Purpose**

This report summarizes and highlights the findings and recommendations of the Class Environmental Assessment Study so that a Notice of Completion can be issued in accordance with Municipal Class Environmental Assessment process for:

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

Item 14, CW Report No. 27 – Page 3

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

#### **Background - Analysis and Options**

#### **The City of Vaughan Transportation Master Plan (TMP) – A New Path - identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre**

With subway service to the Vaughan Metropolitan Centre Subway Station set to begin in late 2017 and VIVAnext Highway 7 / West Vaughan's first phase of fully dedicated Rapidway from Edgeley Boulevard to Bowes Road near completion, the timely implementation of the planned supporting street network that addresses travel demand, capacity and mobility needs of all users, with priority to transit and non-auto based modes of travel, is a key element of the transformation and building of the Vaughan Metropolitan Centre.

Council's adopted Official Plan (2010) and companion Transportation Master Plan (TMP) A New Path (2012) and focused area transportation studies, in support of the Vaughan Metropolitan Centre Secondary Plan, identified strategic road improvements to support regional initiatives and future development areas in accordance with the sustainable strategy for growth.

The TMP recommended improvements to Portage Parkway, a major collector road, as part of the Vaughan Metropolitan Centre grid network supporting intensification and transit-oriented development in Vaughan's centre and future Downtown. The easterly extension to Creditstone will provide a direct link from the industrial area east of the VMC to the planned relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

#### **The TMP recommends the completion of the Municipal Class EA for the Portage Parkway Widening and Easterly Extension to Creditstone Road**

The TMP A New Path, having addressed Phases 1 and 2, recommended completing the planning and design phase (Phases 3 and 4) in accordance with the Municipal Class Environmental Assessment.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 - a strategic connection in the City's transportation network and system that opened in late 2010.

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013)
- Vaughan Metropolitan Centre Municipal Servicing Class Environmental Assessment Master Plan (November 2012)
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015), City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings



CITY OF VAUGHAN

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, CW Report No. 27 – Page 4

**In May 2015, CIMA+ was retained to complete the Environmental Assessment Study, for two interrelated parts, the project limits of which were subject to refinement through the study process**

In May 2015, the City retained CIMA+ to complete the Class Environmental Assessment Study Parkway Widening and from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) - as two interrelated parts.

The EA Study has proceeded in accordance with the Municipal Class Environmental Assessment 2000 as amended in 2007 & 2011 and is now substantially complete.

**CIMA and City staff, over the course of the year has substantially completed the Class EA Study and refined project limits for:**

- **Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)**
- **Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)**

The total distance of the EA Study area corridor is approximately 1.5 km. The Study and recommended Preliminary Preferred Design is supported by multi-disciplinary and comprehensive technical review and study of transportation, natural, socio-economic and built environment of the area corridor.

As the study progressed part limits were refined with Part A, which was initially planned to Jane Street, extended to west of Black Creek to include the Jane Street intersection and to provide for staging of the extension to an interim terminus of Portage Parkway at Black Creek. It is noted that for design and evaluation purposes, both Part A and Part B needed to consider the alignment immediately east of Jane Street.

Draft Environmental Study Reports for the respective Schedule C projects have been prepared documenting the planning and design process and have been provided to the Technical Agencies Committee and Stakeholders Group for review and comment. An executive summary of each study report are included as Attachments 1 and 2 to this report.

**The EA Study corroborates the need to improve and extend Portage Parkway as a multi-modal street**

The Portage Parkway widening, extension and associated improvements for a 4-lane major collector from Applewood Crescent to Creditstone Road designed as a multi modal street will:

- address travel demand and mobility needs of all users with priority to non –auto based modes of travel
- improve connections to the Vaughan Metropolitan Centre subway station (regional) and York Region Rapid Transit Bus Terminal (local) transit infrastructure
- advance the implementation of an alternate route to Highway 7 and Jane Street facilitating another route for trucks, which is a key stakeholder concern
- strengthen the grid network in support of the VMC and surrounding employment area development.

**Part A Portage Parkway Widening and Easterly Extension to West of Black Creek**

The recommended Preliminary Preferred Design for the Portage Parkway Widening and Easterly Extension to West of Black Creek is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 1)

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 5

Alternative design options were generated based on the existing street centre line as a fixed point, constraints presented by the existing stable built form on the north side and consideration to the emerging transformation of the VMC. Five alternative design concepts (including Do-Nothing) for a widening to 33 metres were generated with horizontal alignment varying amongst options.

Widening the right-of-way asymmetrically to the south, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input. Key findings are as follows:

- avoids and minimizes impacts to existing on-site private parking on the north side
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway in step with near term transformation of the VMC
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- widen and improve Portage Parkway to a 33 metre right-of-way from comprised of the following functional elements:
  - active transportation facilitates – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks in keeping with policies of the VMC
  - auxiliary centre/left turn lane facilitating vehicular travel demand, movements and operations
- improvements to existing and future signalized intersections including Applewood Crescent, Edgeley Boulevard and as coordinated with the realigned Millway Avenue.
- new municipal local road right-of way facilitating in the near term the extension from Jane Street to an interim terminus at a cul-de-sac west of the Black Creek. (It is noted that implementation of this functional segment is concurrently being advanced in the context of planning application OP.15.003 and Z.15.003 located at the south east quadrant of Jane Street and Portage Parkway)

#### **Part B Portage Parkway Extension from West of Black Creek to Creditstone Road**

The recommended Preliminary Preferred Design for easterly extension to Creditstone Road is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 2)

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options (for the section east of Jane Street to west of Black Creek) and were carried forward for evaluation. It is noted the extension east from Jane Street to west of the Black Creek was under Part B for the purposes of generating alternative design concepts and evaluation.

Challenges and constraints in generating alternative design concepts for a new road right-of-way included:

- Transport Canada's Grade Crossing Standards (July 2014) with respect to restrictions on the proximity of intersections and driveways to public grade crossings (all design concepts that did not meet this restriction were screened and not carried forward)
- existing stable built form/ uses and associated access and operations principally at the north east corner of Jane Street and Portage Parkway
- imminent proposed development in the VMC at the south east quadrant of Jane Street and Portage Parkway

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

#### Item 14, CW Report No. 27 – Page 6

An option, extending east from the Jane Street intersection narrowing gradually from 33 metres to 25.7m at the Black Creek crossing and gradually widening to 33 metres easterly on the approaches to the future Maplecrete Road extension and Creditstone Road intersections, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input.

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA Study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority (TRCA) with respect to the crossing of the Black Creek channel that is a TRCA regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages consistent with Council's approved Term of Council Priorities.

Key findings for the preferred alternative are as follows:

- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with the longer term transformation of the VMC
- provides for seamlessly advancing near term implementation in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- horizontal alignment that accommodates a new right-of-way ranging from approximately 25.7m - 33m provides for a future intersection at the future Maplecrete Road extension and curves south on the approach to the future signalized intersection at Creditstone Road
  - active transportation facilities – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks taking into account the policies of the VMC Secondary Plan
  - auxiliary centre/ left turn lane facilitating vehicular travel demand, movements and operations
  - provision of a standard streetscape consistent with the City wide Streetscape Implementation Manual and Financial Strategy and the VMC Streetscape and Open Space Plan
- crossing of the Black Creek - 12 m wide ConSpan structure with consideration and not precluding opportunity for future planning of north- south continuation of pedestrian and cycling linkages in the broader context of the VMC Secondary Plan Environmental Open Space

#### **The EA Study recommends advancing a staged implementation of the Parts**

The EA Study recommends advancing an Implementation Plan giving priority to Part A, particularly in the vicinity of the mobility hub/VMC subway station. The following highlights the key components of the Implementation Plan;

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street as programmed under Capital Project DT – 7114-16

## CITY OF VAUGHAN

### EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016

Item 14, CW Report No. 27 – Page 7

- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets
- obtain approvals(permits, etc.) and acquire property (where necessary) in a timely manner

In addition, staff have been working with the development proponents to ensure the Portage Parkway project is protected for in the design of projects within the VMC.

#### **Filing the Environmental Study Reports for agency and public review is the next step in the study process**

The next steps in the study are as follows:

- finalize the Environmental Reports based on comments from the review, regulatory agencies and stakeholders
- issue a Notice of Study Completion and place the Environmental Study Reports for the minimum 30-day public review period (Phase 4 of the Municipal Class Environmental Assessment process)

#### **Relationship to Term of Council Service Excellence Strategy Map (2014-2018)**

The completion of the EA Study for the two projects advances Council's priorities with respect to:

- Improve municipal road network
- Continue to develop transit, cycling and pedestrian options to get around the City
- Facilitate the development of the VMC

#### **Regional Implications**

Region of York staff have participated through all phases of the EA Study as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access to Jane Street as Jane Street is under the Region's jurisdiction. The EA study recommends, in minimizing and mitigating impacts, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. It is recognized that all access to Jane Street is subject to the findings of the York Region's future Rapid Transit Corridor Environmental Assessment.

#### **Conclusion**

The Class EA Studies for the Portage Parkway widening and extension to Creditstone Road have now been completed and submitted to the Technical Agencies Committee and Stakeholders Group for their final review and comment. Upon completion of the review, it is recommended that the Environmental Study Reports be finalized and associated Notices of Completion be issued for the following Schedule C projects:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

Issuing the Notice of Completion places the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process.

The City may proceed with the detailed design and construction of the road works after the Class EA studies have received final approval.



**CITY OF VAUGHAN**

**EXTRACT FROM COUNCIL MEETING MINUTES OF JUNE 28, 2016**

Item 14, CW Report No. 27 – Page 8

**Attachments**

1. Portage Parkway Widening and Easterly Extension from Applewood Crescent to West of Black Creek – Draft Environmental Study Report Executive Summary
2. Portage Parkway Extension from West of Black Creek to Creditstone Road – Draft Environmental Study Report Executive Summary

**Report prepared by:**

Selma Hubjer, Manager Transportation Planning, 8674  
Marta Roias, Transportation Project Manager, 8026

(A copy of the attachments referred to in the foregoing have been forwarded to each Member of Council and a copy thereof is also on file in the office of the City Clerk.)

**CLASS ENVIRONMENTAL ASSESSMENT STUDY NOTICES OF COMPLETION  
PORTAGE PARKWAY WIDENING AND EASTERLY EXTENSION TO WEST OF BLACK CREEK  
(PART A); AND  
PORTAGE PARKWAY EXTENSION FROM WEST OF BLACK CREEK TO CREDITSTONE ROAD  
(PART B)  
WARD 4 - VAUGHAN METROPOLITAN CENTRE**

**Recommendation**

The Deputy City Manager, Planning and Growth Management and the Director of Development Engineering and Infrastructure Planning recommend:

1. THAT staff issue Notices of Completion and place the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process for :
  - a) Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
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**Contribution to Sustainability**

Consistent with the Goals and Objectives of the City's Community Sustainability and Environmental Master Plan (Green Directions Vaughan), the Environmental Assessment Study advances and completes the planning and design process concurrently for the Portage Parkway Widening and Easterly Extension to West of Black Creek and Portage Parkway Extension from West of Black Creek to Creditstone Road, the projects will contribute to:

- transformation of the transportation network and system supporting intensification, sustainable growth and development of the Vaughan Metropolitan Centre (VMC)
- establishing a green and complete street network of sidewalks, paths and trails that support all modes of non-vehicular transportation
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- reducing single occupant vehicle trips by supporting active transportation, carpooling and public transit

**Economic Impact**

The Class Environmental Assessment Study for the Portage Parkway Widening and Easterly Extension to Creditstone Road is being funded from approved Capital Projects DT-7073-11 and DT-7071-11 with funding from City-wide Development Charges.

The estimated project cost associated with the recommended Preliminary Preferred Design for the Portage Parkway widening, extension and improvements, including engineering, construction, utility relocations and other project costs is approximately \$15.4 million as detailed below:

- \$9.2 million for Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- \$6.2 million for Portage Parkway Extension from West of Black Creek to Creditstone Road including \$3.2 million for the crossing of the Black Creek channel (Part B)

Estimated project costs for the recommended Preliminary Preferred Design do not include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

The Portage Parkway improvements are included as a project in the 2013 Development Charges Background Study at an estimated cost of \$14.34 million. Accordingly, the increase in the estimated project cost will be captured in the next update of the Development Charge Background Study, which is scheduled for 2018.

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### **Communication**

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Notices were communicated to the public through direct mailing to a broader notice area bounded by Langstaff Road (north), Highway 407 (south), CN MacMillan Yard (east) and Weston Road (west) and by publication in the Vaughan Citizen and Thornhill Liberal.

### **Purpose**

This report summarizes and highlights the findings and recommendations of the Class Environmental Assessment Study so that a Notice of Completion can be issued in accordance with Municipal Class Environmental Assessment process for:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

## **Background - Analysis and Options**

### **The City of Vaughan Transportation Master Plan (TMP) – A New Path - identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre**

With subway service to the Vaughan Metropolitan Centre Subway Station set to begin in late 2017 and VIVAnext Highway 7 / West Vaughan's first phase of fully dedicated Rapidway from Edgeley Boulevard to Bowes Road near completion, the timely implementation of the planned supporting street network that addresses travel demand, capacity and mobility needs of all users, with priority to transit and non-auto based modes of travel, is a key element of the transformation and building of the Vaughan Metropolitan Centre.

Council's adopted Official Plan (2010) and companion Transportation Master Plan (TMP) A New Path (2012) and focused area transportation studies, in support of the Vaughan Metropolitan Centre Secondary Plan, identified strategic road improvements to support regional initiatives and future development areas in accordance with the sustainable strategy for growth.

The TMP recommended improvements to Portage Parkway, a major collector road, as part of the Vaughan Metropolitan Centre grid network supporting intensification and transit-oriented development in Vaughan's centre and future Downtown. The easterly extension to Creditstone will provide a direct link from the industrial area east of the VMC to the planned relocated northbound on-ramp to Highway 400, thus facilitating the diversion of truck traffic from Highway 7.

### **The TMP recommends the completion of the Municipal Class EA for the Portage Parkway Widening and Easterly Extension to Creditstone Road**

The TMP A New Path, having addressed Phases 1 and 2, recommended completing the planning and design phase (Phases 3 and 4) in accordance with the Municipal Class Environmental Assessment.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 - a strategic connection in the City's transportation network and system that opened in late 2010.

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the EA (planning and design) Study process. These principally included:

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**In May 2015, CIMA+ was retained to complete the Environmental Assessment Study, for two interrelated parts, the project limits of which were subject to refinement through the study process**

In May 2015, the City retained CIMA+ to complete the Class Environmental Assessment Study Parkway Widening and from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) - as two interrelated parts.

The EA Study has proceeded in accordance with the Municipal Class Environmental Assessment 2000 as amended in 2007 & 2011 and is now substantially complete.

**CIMA and City staff, over the course of the year has substantially completed the Class EA Study and refined project limits for:**

- **Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)**
- **Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)**

The total distance of the EA Study area corridor is approximately 1.5 km. The Study and recommended Preliminary Preferred Design is supported by multi-disciplinary and comprehensive technical review and study of transportation, natural, socio-economic and built environment of the area corridor.

As the study progressed part limits were refined with Part A, which was initially planned to Jane Street, extended to west of Black Creek to include the Jane Street intersection and to provide for staging of the extension to an interim terminus of Portage Parkway at Black Creek. It is noted that for design and evaluation purposes, both Part A and Part B needed to consider the alignment immediately east of Jane Street.

Draft Environmental Study Reports for the respective Schedule C projects have been prepared documenting the planning and design process and have been provided to the Technical Agencies Committee and Stakeholders Group for review and comment. An executive summary of each study report are included as Attachments 1 and 2 to this report.

**The EA Study corroborates the need to improve and extend Portage Parkway as a multi-modal street**

The Portage Parkway widening, extension and associated improvements for a 4-lane major collector from Applewood Crescent to Creditstone Road designed as a multi modal street will:

- address travel demand and mobility needs of all users with priority to non –auto based modes of travel
- improve connections to the Vaughan Metropolitan Centre subway station (regional) and York Region Rapid Transit Bus Terminal (local) transit infrastructure
- advance the implementation of an alternate route to Highway 7 and Jane Street facilitating another route for trucks, which is a key stakeholder concern
- strengthen the grid network in support of the VMC and surrounding employment area development.

**Part A Portage Parkway Widening and Easterly Extension to West of Black Creek**

The recommended Preliminary Preferred Design for the Portage Parkway Widening and Easterly Extension to West of Black Creek is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 1)

Alternative design options were generated based on the existing street centre line as a fixed point, constraints presented by the existing stable built form on the north side and consideration to the emerging transformation of the VMC. Five alternative design concepts (including Do-Nothing) for a widening to 33 metres were generated with horizontal alignment varying amongst options.

Widening the right-of-way asymmetrically to the south, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input. Key findings are as follows:

- avoids and minimizes impacts to existing on-site private parking on the north side
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway in step with near term transformation of the VMC
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- widen and improve Portage Parkway to a 33 metre right-of-way from comprised of the following functional elements:
  - active transportation facilitates – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks in keeping with policies of the VMC
  - auxiliary centre/left turn lane facilitating vehicular travel demand, movements and operations
- improvements to existing and future signalized intersections including Applewood Crescent, Edgeley Boulevard and as coordinated with the realigned Millway Avenue.
- new municipal local road right-of way facilitating in the near term the extension from Jane Street to an interim terminus at a cul-de-sac west of the Black Creek. (It is noted that implementation of this functional segment is concurrently being advanced in the context of planning application OP.15.003 and Z.15.003 located at the south east quadrant of Jane Street and Portage Parkway)

## **Part B Portage Parkway Extension from West of Black Creek to Creditstone Road**

The recommended Preliminary Preferred Design for easterly extension to Creditstone Road is based on a reasonable range of alternatives, identification and consideration of effects on all aspects of the environment and a systematic evaluation. (See Attachment 2)

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options (for the section east of Jane Street to west of Black Creek) and were carried forward for evaluation. It is noted the extension east from Jane Street to west of the Black Creek was under Part B for the purposes of generating alternative design concepts and evaluation.

Challenges and constraints in generating alternative design concepts for a new road right-of-way included:

- Transport Canada's Grade Crossing Standards (July 2014) with respect to restrictions on the proximity of intersections and driveways to public grade crossings (all design concepts that did not meet this restriction were screened and not carried forward)
- existing stable built form/ uses and associated access and operations principally at the north east corner of Jane Street and Portage Parkway
- imminent proposed development in the VMC at the south east quadrant of Jane Street and Portage Parkway

An option, extending east from the Jane Street intersection narrowing gradually from 33 metres to 25.7m at the Black Creek crossing and gradually widening to 33 metres easterly on the approaches to the future Maplecrete Road extension and Creditstone Road intersections, emerged as the preferred design concept carried forward in consultation with the stakeholders, technical agencies and with public input.

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA Study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority (TRCA) with respect to the crossing of the Black Creek channel that is a TRCA regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages consistent with Council's approved Term of Council Priorities.

Key findings for the preferred alternative are as follows:

- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with the longer term transformation of the VMC
- provides for seamlessly advancing near term implementation in coordination with and in the context of the VMC planned street network

The recommended Preliminary Preferred Design is comprised of the following key and functional elements:

- horizontal alignment that accommodates a new right-of-way ranging from approximately 25.7m - 33m provides for a future intersection at the future Maplecrete Road extension and curves south on the approach to the future signalized intersection at Creditstone Road
  - active transportation facilities – including cycle tracks
  - 4 general purpose lanes (2 in each direction) designed with a wider curb lane for transit vehicles and trucks taking into account the policies of the VMC Secondary Plan
  - auxiliary centre/ left turn lane facilitating vehicular travel demand, movements and operations
  - provision of a standard streetscape consistent with the City wide Streetscape Implementation Manual and Financial Strategy and the VMC Streetscape and Open Space Plan
- crossing of the Black Creek - 12 m wide ConSpan structure with consideration and not precluding opportunity for future planning of north- south continuation of pedestrian and cycling linkages in the broader context of the VMC Secondary Plan Environmental Open Space

### **The EA Study recommends advancing a staged implementation of the Parts**

The EA Study recommends advancing an Implementation Plan giving priority to Part A, particularly in the vicinity of the mobility hub/VMC subway station. The following highlights the key components of the Implementation Plan;

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street as programmed under Capital Project DT – 7114-16

- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets
- obtain approvals(permits, etc.) and acquire property (where necessary) in a timely manner

In addition, staff have been working with the development proponents to ensure the Portage Parkway project is protected for in the design of projects within the VMC.

**Filing the Environmental Study Reports for agency and public review is the next step in the study process**

The next steps in the study are as follows:

- finalize the Environmental Reports based on comments from the review, regulatory agencies and stakeholders
- issue a Notice of Study Completion and place the Environmental Study Reports for the minimum 30-day public review period (Phase 4 of the Municipal Class Environmental Assessment process)

**Relationship to Term of Council Service Excellence Strategy Map (2014-2018)**

The completion of the EA Study for the two projects advances Council’s priorities with respect to:

- Improve municipal road network
- Continue to develop transit, cycling and pedestrian options to get around the City
- Facilitate the development of the VMC

**Regional Implications**

Region of York staff have participated through all phases of the EA Study as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access to Jane Street as Jane Street is under the Region’s jurisdiction. The EA study recommends, in minimizing and mitigating impacts, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. It is recognized that all access to Jane Street is subject to the findings of the York Region’s future Rapid Transit Corridor Environmental Assessment.

**Conclusion**

The Class EA Studies for the Portage Parkway widening and extension to Creditstone Road have now been completed and submitted to the Technical Agencies Committee and Stakeholders Group for their final review and comment. Upon completion of the review, it is recommended that the Environmental Study Reports be finalized and associated Notices of Completion be issued for the following Schedule C projects:

- Portage Parkway Widening and Easterly Extension to West of Black Creek (Part A)
- Portage Parkway Extension from West of Black Creek to Creditstone Road (Part B)

Issuing the Notice of Completion places the respective Environmental Study Reports on public record for the minimum 30 day review period in accordance with the Municipal Class Environmental Assessment process.

The City may proceed with the detailed design and construction of the road works after the Class EA studies have received final approval.



**Attachments**

1. Portage Parkway Widening and Easterly Extension from Applewood Crescent to West of Black Creek – Draft Environmental Study Report Executive Summary
2. Portage Parkway Extension from West of Black Creek to Creditstone Road – Draft Environmental Study Report Executive Summary

**Report prepared by:**

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Respectfully submitted,

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**Executive Summary of the Draft Environmental Study Report  
Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## 0. Executive Summary

The City of Vaughan Transportation Master Plan (TMP), *A New Path* identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre and recommends completion of the Municipal Class Environmental Assessment.

This Environmental Assessment Study advances implementation of the Portage Parkway Widening and Easterly Extension to Creditstone Road in accordance with Schedule C of the Municipal Class Environmental Assessment (October 2000, as amended in 2007 & 2011).

The City of Vaughan retained CIMA+ in May 2015 to complete an Environmental Assessment (EA) study for the Portage Parkway Widening from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) as two interrelated parts; the project limits of which were subject to refinement through the study process.

**Accordingly this Environmental Study Report documents the planning and design process, in accordance with the Municipal Class EA for:**

**Portage Parkway Widening and Easterly Extension to West of Black Creek.** Part A was initially defined as the widening and improvements from Applewood Crescent to Jane Street. As the study progressed, the project limits were extended easterly to west of Black Creek in order to facilitate the required staged reconstruction of the extension/ east leg of the Jane Street intersection and easterly extension of a local road to an interim terminus at cul-de-sac west of Black Creek.

**Documentation with respect to Part B is contained in a separate Environmental Study Report for Portage Parkway Extension from West of Black Creek to Creditstone Road.**

### 0.1 Background and Study Context

The City's Transportation Master Plan, *A New Path* (2013), Vaughan Metropolitan Centre (VMC) Secondary Plan and supporting focused area transportation plans and studies identified the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic network improvement to support development within the VMC.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The current right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built to 2 lane urban cross section to the terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400; a strategic connection in the City's transportation network and system that opened in late 2010.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the study EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013)
- Municipal Servicing Class Environmental Assessment Master Plan (November 2012)
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015) City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings

The VMC Secondary Plan provides the following broader transportation planning and policy context for advancing and completing the planning and design of Portage Parkway:

- + Encourage the use of Portage Parkway for through movements linking to Highway 400.
- + Support the extent of the right-of-way for the Spadina Subway Line to Portage Parkway to protect for future subway extension beyond the VMC.
- + Link Creditstone Road to Portage Parkway and link both roads to Highway 7 and Jane Street to establish a bypass (i.e., Creditstone-Portage-Applewood Bypass) to assist with mitigating through movements on Highway 7 and Jane Street. This bypass would require the southerly extension of
- + Applewood Crescent from Portage Parkway.
- + Cooperate with York Region to implement rapid transit services on Jane Street.
- + Acquire right-of-way for an extension of Portage Parkway from Jane Street to Creditstone Road.

### 0.1.1 Study Area

**Figure 1** illustrates the study area for Part A and Part B. The total distance of the study area corridor is approximately 1.5 km. As the project progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore, both Parts A and B considered implications of the alignment immediately east of Jane Street.

The EA study and recommended Preliminary Preferred Design is supported by a multi-disciplinary and comprehensive technical review and study of transportation, natural, socio- economic and built environment of the area corridor.

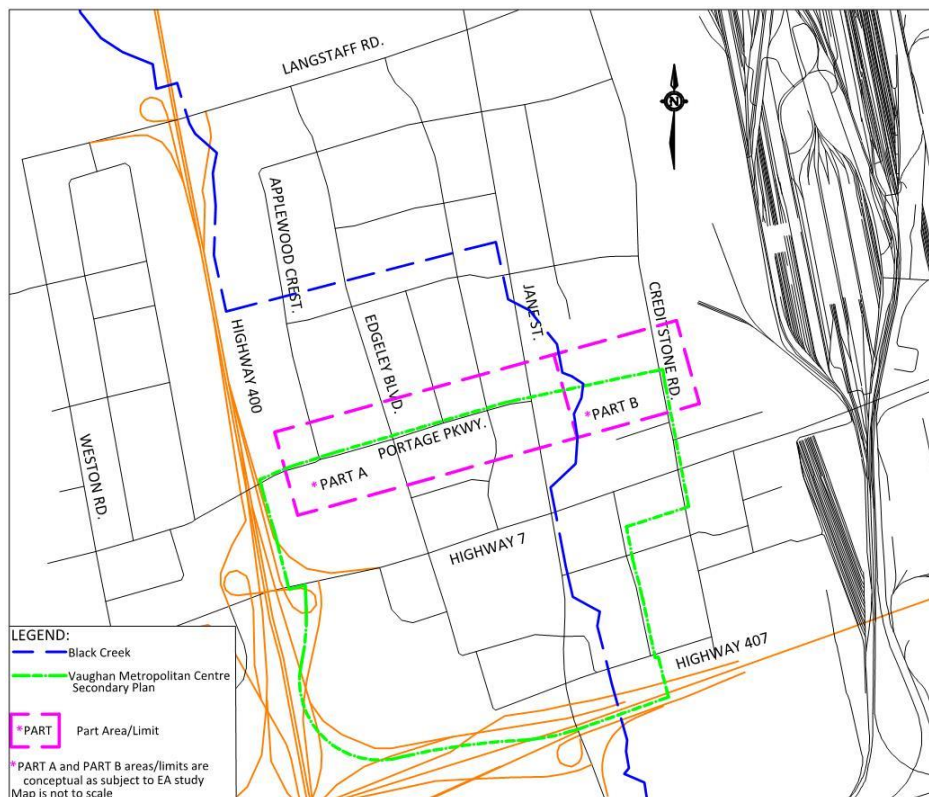
### 0.1.2 Class EA Process

Municipal projects that affect the purpose, capacity or function of a roadway, or propose new roadways, are subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011).

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The TMP *A New Path* (Appendix I), having addressed Phases 1 and 2, recommended completing the planning and design process in accordance with the Municipal Class EA. This EA Study revisits Phases 1 and 2 and completes Phases 3 and 4 for the Portage Parkway Widening and Easterly Extension to West of Black Creek as a **Schedule C** project under the Municipal Class EA. Schedule C projects must follow Phases 1 through 5 of the Class EA process:

- Phase 1** Identify the problem or opportunity.
- Phase 2** Identify alternative solutions to address the problem or opportunity.
- Phase 3** Examine alternative methods of implementing the preferred solution.
- Phase 4** Document the rationale for the Preferred Solution and Design Concept, and the planning, design and consultation process in an Environmental Study Report (ESR) for public and agency review.
- Phase 5** Complete contract drawings and documents, and proceed to construction, operation and environmental monitoring.



**Figure 1: Study Area for Part A and Part B**

Accordingly, separate Environmental Study Reports for Part A (and interrelated Part B) documenting Phases 1 through 4 will be placed on public record for the minimum 30 calendar days upon study completion for public and agency review. .



**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.1.3 Consultation Plan

An extensive Consultation Plan, was implemented to ensure meaningful consultation with internal and external stakeholders and reviewing agencies. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/regulatory agencies at study milestones

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones

A mailing list was developed to notify potentially interested parties of opportunities for review and comment. Four notices to the public and internal and external stakeholders were undertaken:

Notice of Commencement,	June 18 and 25, 2015
Notice of Public Information Centre No. 1,	November 10, 2015
Notice of Public Information Centre No. 2,	February 23, 2016
Notice of Completion.	Pending, 2016

To inform the general public of the study, each of the notices were advertised in two separate issues of the Vaughan Citizen and The Thornhill Liberal, and mailed to approximately 1,500 stakeholders and 93 agency representatives on the study mailing list. The Notices were also posted to the City's webpage at: [http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx).

The Public Information Centres (PICs) were held at the Homewood Suites Hotel near Portage Parkway on:

- + November 25, 2015 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phases 1 and 2 of the project and asked for public input on the findings – 19 members of the public attended.
- + March 9, 2016 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phase 3 of the project, presenting alternative designs and the preliminary preferred design, and requested public input on the findings - 14 members of the public attended.

Two advisory groups were formed and met three (3) times to provide input to the study following commencement and in advance of each PIC. Invitations to participate on the study's Technical Agencies Committee were mailed to approximately 93 agency representatives. Invitations to participate on the study's Stakeholders Group were mailed to approximately 201 property owners within 200 m north and south of Portage Parkway within the study limits.

A **Stakeholders Group** was established from interested owners of property within 200 m of the Portage Parkway study corridor, including representatives of active and proposed development in the area; and

A **Technical Agencies Committee** was established from interested representatives of regulatory and/or approving agencies on the study mailing list. Separate meetings were held with approving authorities as

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

required to review project impacts, mitigation measures and approval requirements.

One-on-one meetings were held with several land owners whose property was directly impacted by the proposed road improvements so that their concerns were considered during the evaluation of options and mitigation measures developed as appropriate. An Open House further reaching out and engaging affected property owners along the corridor was held on May 5, 2016.

The Ministry of Aboriginal Affairs advised the following First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation which may be affected by the project:

- + Chippewas of Georgina Island
- + Chippewas of Rama
- + Beausoleil First Nation
- + Mississaugas of the New Credit First Nation

## **0.2 Phase 1 – Problem and Opportunity**

The EA Study, building on the City's TMP and VMC Secondary Plan, corroborates the need to improve, and design Portage Parkway as a multi-modal street.

### **0.2.1 Needs and Justification**

A broader comprehensive traffic analysis was undertaken of the corridor that validates the need to widen Portage Parkway to 4 lanes from Applewood Crescent to Creditstone Road as a major collector road. The following highlights technical findings with respect to road way performance and operations including under a "Do-Nothing" planning scenario. :

- + Under existing traffic conditions, most intersections and turning movements operate satisfactorily during both the a.m. and p.m. peak hours;
- + Under future 2031 traffic conditions with a "Do Nothing" scenario (e.g. no widening on Portage Parkway), traffic operations along the corridor is expected to severely deteriorate, resulting in high levels of congestion; and
- + Under future 2031 traffic conditions with the proposed widening, all intersections and turning movements are expected to operate satisfactorily during the a.m. peak hour.

### **0.2.2 Problem and Opportunity Statement**

The following Problem and Opportunity Statement was developed and used to guide the remainder of the planning process:

*Transportation improvements are needed to support growth and to provide alternative truck routes to Highway 7 within the VMC.*

*Improving Portage Parkway from Applewood Crescent to Jane Street is an opportunity to:*

- + *Serve the VMC and surrounding employment area,*
- + *Create an alternative route for trucks to bypass the VMC core,*
- + *Improve connections to local and regional infrastructure,*

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- + *Support transit-oriented nodes and corridors, and*
- + *Enhance transit ridership, cycling and walking.*

*The extension of Portage Parkway from Jane Street to Creditstone Road is an opportunity to:*

- + *Provide a continuous route for all modes of transportation from Weston Road crossing Highway 400 and the Black Creek channel to Creditstone Road, and*
- + *Alleviate traffic congestion on Highway 7 within the VMC.*

### **0.3 Phase 2 – Alternative Planning Solutions**

Six (6) alternative planning solutions were identified and evaluated as part of Phase 1 and 2 of the Class EA process:

#### **Do Nothing**

This solution would leave Portage Parkway unmodified in an 'as is' state.

#### **Travel Demand Management Initiatives**

Travel demand management initiatives involve strategies and policies used to reduce travel demand or redistribute the demand spatially or temporally.

#### **Alternative Modes of Transportation**

Promoting and facilitating the use of alternative modes of transportation, such as transit and cycling, can reduce the demand on a roadway.

#### **Localized Intersection and Operational Improvements**

Operational improvements such as the retiming of traffic signals and installation of turning lanes, can improve the overall efficiency of a roadway (i.e. maximize throughput) and the surrounding network.

#### **Widening Portage Parkway from Applewood Crescent to Jane Street**

This solution would increase the capacity of the roadway between Applewood Crescent and Jane Street.

#### **Extending Portage Parkway from Jane Street to Creditstone Road**

This solution addresses the local areas network discontinuity by extending Portage Parkway from Jane Street to Creditstone Road.

An advantage/disadvantage evaluation process was used to compare the alternatives. Two (2) of the recommended solutions have previously been recommended as part of the Transportation Master Plan (TMP) and are assumed to continue, which are:

- + Travel Demand Management Initiatives – Identified in the TMP and will be implemented by the City as a separate strategy.
- + Alternative Modes of Transportation – Identified in the TMP, including the provision for continuous sidewalks, cycling systems, connectivity of the subway extension to Highway 7, and rapid transit of Jane Street.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The other three (3) recommended solutions are:

- + Localized Intersection and Operational Improvements;
- + Widening Portage Parkway from Applewood Crescent to Jane Street; and
- + Extending Portage Parkway from Jane Street to Creditstone Road.

The alternative planning solutions and evaluation were presented to the Advisory Groups (Technical and Stakeholder) and also at the first PIC. Minor comments were received during this consultation process, mostly concerning requests to be kept up to date as the project proceeds.

## **0.4 Phase 3 – Alternative Designs**

Development of the design concepts and alternative options, evaluated against technical and environmental based criteria, considered the three-dimensions of a road corridor; i.e. the cross-section, the horizontal alignment and vertical profile as follows.

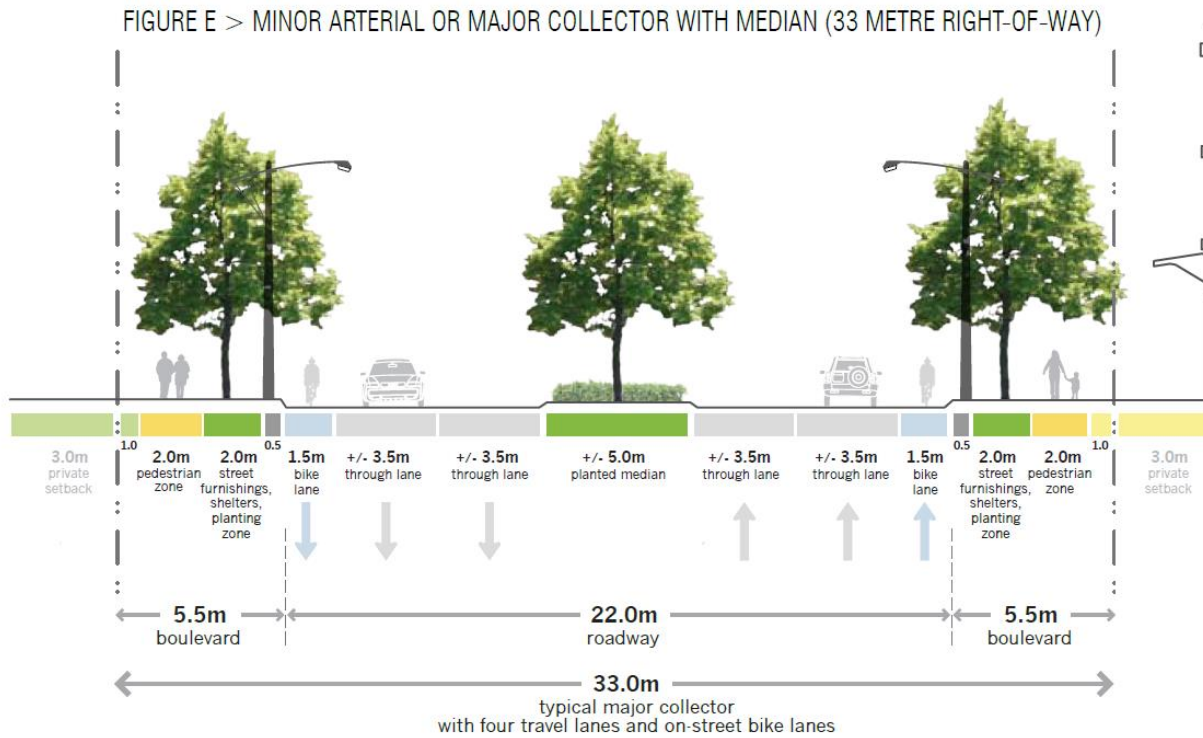
### Cross-Section

The VMC Secondary Plan (Figure below) and VMC Streetscape and Open Space Plan as coordinated with the City's broader City-wide Streetscape Implementation Manual and Financial Strategy and Design Criteria and Standard Drawings provided the planning and design context and framework for the exploration of alternative typical cross-sections.

The beginning point for the development of a typical cross-section was the street cross section from the VMC Secondary Plan (figure below) and organization of streetscape infrastructure as guided by the VMC Streetscape and Open Space Plan. The symmetrical street cross section in the VMC Secondary Plan accommodates four 3.5m travel lanes, two 1.5m on-street bicycle lanes and 5.5m boulevards within a 33 metre right of way. The VMC Streetscape and Open Space Plan incorporates cycling facilities into the street network to build a cohesive and permeable network - identifying a cycle track for Portage Parkway.



**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016



Recognizing high truck volumes associated in large part with the surrounding industrial and employment areas, the preferred typical cross-section, shown in figure 2, was refined to provide a wider ( relative to 3.3m inside lane) 3.5m curb lane, and wider raised off-road cycle track at 1.8m separated by a rolled curb facilitating trucks and cyclists respectively.

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.4.1 Technical and Environmental Criteria

Each of the alternative design options was assessed against the following technical and environmental criteria:

#### Technical Criteria

##### *Economic*

- + Capital costs

##### *Implementation*

- + Conformity with regulatory framework.
- + Construction staging and planning.
- + Impacts on existing municipal services and utilities.

##### *Infrastructure Planning*

- + Improved road safety.
- + Opportunities for other travel modes (walking, cycling, and public transit).
- + Improving road capacity and/or traffic flow.
- + Conformity with official/secondary plans and transportation master plans.

#### Environmental Criteria

##### *Natural*

- + Impacts on avian and wildlife.
- + Encroachment onto natural areas
- + Impacts on species at risk.
- + Impacts on aquatic and watercourses.
- + Impacts on vegetation.

##### *Cultural/Social*

- + Impacts on archaeology.
- + Impacts on built heritage and cultural landscapes.
- + Air quality.
- + Compatibility with emergency services requirements.
- + Property and parking access impacts.
- + Noise impacts (post construction).

### 0.4.2 Applewood Crescent to Jane Street Widening Alternatives

Horizontal alignment constraints were presented by the existing stable built form on the north side (including parking) and consideration of the emerging transformation of the VMC on the south side; these considerations led to five alternative design concepts (including Do-Nothing) being developed with the horizontal alignment varying for each option.

As previously noted, while Part A was initially identified as being west of Jane Street, as the EA Study progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing off-set driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore options were developed that considered both Parts A and B.

Four (4) design options to widen and improve Portage Parkway from Applewood Crescent to Jane Street were developed and carried forward for evaluation. The first option aligns the “widening equally on both sides”; once the business impacts (mainly loss of parking on the north side) of this option were apparent, further options were developed that varied the alignment to the south and north giving due consideration to constraints on both the north and south sides:

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

**'Do Nothing' Option**

No widening of Portage Parkway from Applewood Crescent to Jane Street – this was used for comparative purposes in order to measure the net impacts.

**Option 1: Widen Equally on Both Sides**

Widening equally about the centreline removes a row of parking from all properties on the North side.

**Option 2: Widen to the South with No Impact on North Side**

Widening to the South avoids **all** North side parking impacts, but adversely impacts parking and loading/unloading operations on the South side.

**Option 3: Widen to the South with impact on North Side**

Holding the right-of-way at its current position on the North side adversely impacts parking within the right-of-way, as well as parking and loading/unloading operations on the South side.

**Option 4: Hold Right-of-Way on Majority of Parking Curb Lines (Both Sides) - *preferred***

Holding the right-of-way to the edge of the curb line for parking on the North and South sides minimizes property impacts on the North side (affects one North-side property which is constructed partly in the City's right-of-way).

Widening the right-of-way asymmetrically to the south - holding the future right-of-way on the majority of parking curb lines on the north and south sides to the south (Option 4), - emerged as the preferred design option carried forward in consultation with the stakeholders, technical agencies and with public input.

Key features of the preferred design are as follows:

- minimizes impacts to existing on-site private parking on the north side;
- the option is compatible with the preferred option for Part B;
- provides flexibility for a logical and orderly staging of the widening and improvements to Portage Parkway facilitating and in step with imminent and nearer term transformation of the VMC; and
- provides for seamlessly staging and advancing near term implementation of improvements in coordination with and in the context of the VMC planned street network.

Meetings were held with the Stakeholders and Agencies as well as with property owners where there was a significant property impact concern. A second Public Information Centre was also held to review the design options. There was general support for the preferred design options and some minor modifications developed to further minimize impacts wherever possible.

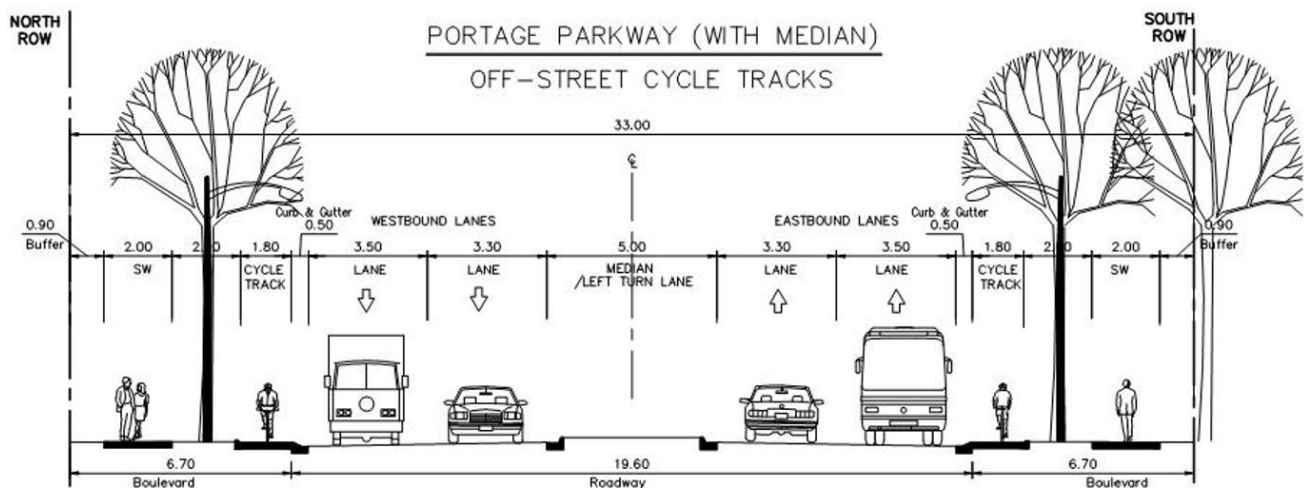
**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## 0.5 Preliminary Preferred Design

Based on the evaluation of alternative design concepts and consultation with the affected property owners, interested public and agencies, the recommended Preliminary Preferred Design (see Plates 1- 3) is to:

- ✦ Widen Portage Parkway from Applewood Crescent to Jane Street to 33 metre right-of –way. This preferred design minimizes impacts, improves access to several driveways through dedicated left turns, increases capacity of VMC sub-area network, improves level of service on Jane Street, and improves accommodation of active transportation.

The typical cross section for the preferred design are shown in **Figure 2**.



**Figure 2: Typical Cross-Section**

A 1.8m wide cycle track is accommodated on both sides of the road and separated from the travel lanes by a roll-over curb. A 2.0m sidewalk on each side is proposed just inside the property buffer. The south side (adjacent to the VMC) has proposed two lines of trees, one of which is on the adjacent development.

The existing vertical alignment is relatively flat. One section has a grade of less than 0.5%. The proposed vertical alignment is improved with a minimum grading of 0.5% for the entire study area.

### 0.5.1 Municipal Infrastructure/Utilities

The recommended Preliminary Preferred Design provides for the integrated and coordinated implementation of planned and approved municipal services (water and sanitary) as per the VMC Municipal Class Environmental Assessment Master Plan together with streetscape infrastructure as per VMC Streetscape and Open Space Plan and City wide Streetscape Implementation Manual and Financial Strategy

A number of utilities will require relocation to accommodate the recommended roadway design, such as:



**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

- + Underground Bell cable, Rogers buried fibre, Enbridge pipe, All Stream underground ducts and cable, PowerStream underground ducts and cable.

Based on the City Standards, the proposed locations of the relocated utilities are shown in **Figure 3**.

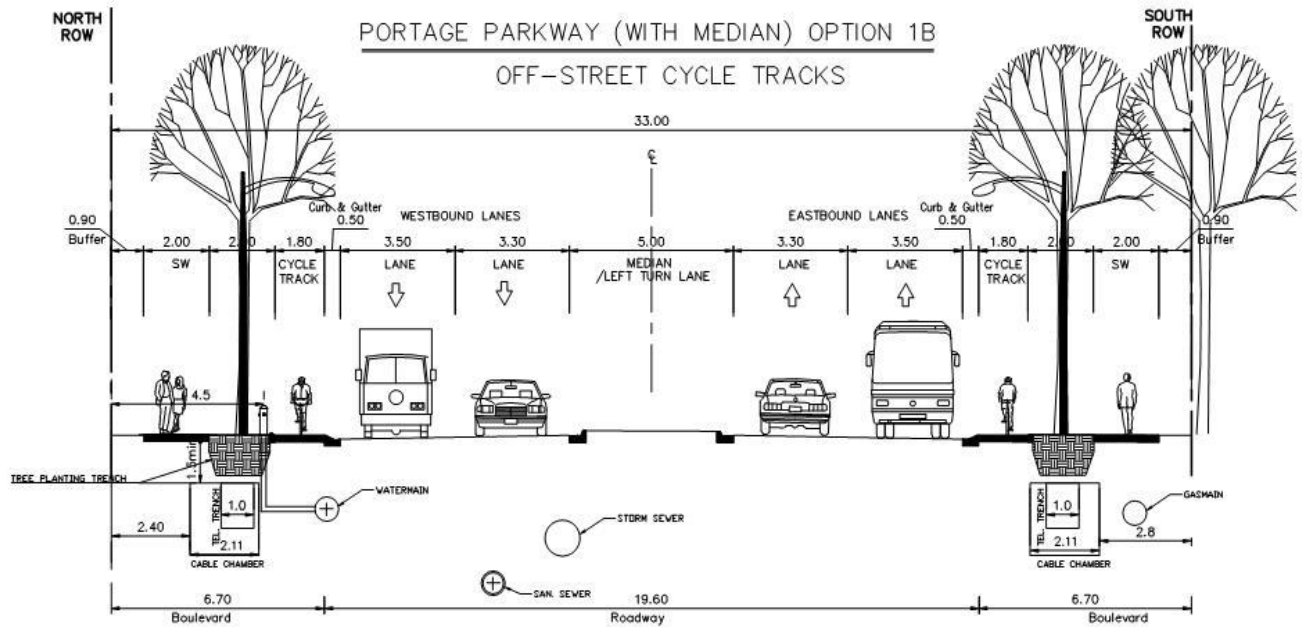


Figure 3: Typical Cross Section (with Utility Locations)

### 0.5.2 Traffic Signals

The EA Study and recommended Preliminary Preferred Design provides for staging improvements to the existing signalized intersections at Applewood Crescent and Edgeley Boulevard and implementation of a new signalized intersection at and in coordination with the realignment of Millway Avenue.

With respect to the Jane Street intersection - the EA Study considers the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension. In the regard it is noted that the EA study provided for exchange of information and ongoing dialogue with Region of York staff as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access as Jane Street is under the Region's jurisdiction. It is recognized that all access to Jane Street is subject to the findings of York Region's future Rapid Transit Corridor Environmental Assessment.

### 0.5.3 Implementation Plan

The EA Study recommends advancing staging improvements in step with the transformation of the VMC and in coordination with the VMC planned street network. The key components of the Implementation Plan follow:

**Executive Summary of the Draft Environmental Study Report**  
**Portage Parkway Widening and Easterly Extension to West of Black Creek**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- advance detailed design and engineering for the reconstruction of Portage Parkway from Applewood Crescent to Jane Street;
- facilitate near term improvements and extension east of Jane Street to an interim terminus at a cul-de-sac west of Black Creek;
- program and allocate funding accordingly for Part A dovetailing ( where possible) with the ongoing and emerging detailed design of intersecting north-south VMC streets; and
- obtain approvals(permits etc.) and acquire property (where necessary) in a timely manner

### **0.5.3.1 Capital Cost Estimate**

The estimated total project cost associated with the proposed improvements, including engineering, construction, utility relocations and other project costs is approximately \$9,200,000 for Part A , widening, extension and improvements for Portage Parkway from Applewood Crescent to west of Black Creek. It is noted that estimated preliminary capital cost estimates do include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.

### **0.5.3.2 Property Requirements**

Implementation of the recommended Preliminary Preferred Design for the widening, extension and improvements from Applewood Crescent to Jane Street will require lands from all properties on the north and south sides of Portage Parkway of up to approximately 7.7 m from the south and 3.4 m from the north. It is noted that the recommended Preliminary Preferred Design is subject to detailed design and engineering.

## **0.6 Phase 4 – Notice of Completion**

The Environmental Study Report will be placed on the public record for a minimum 30 calendar days. The Notice of Completion will announce where the report can be reviewed and will include contact information and a date for receiving comments. The Notice will further explain the process for resolving concerns. The public must contact the City of Vaughan within the 30-day review period to discuss and resolve any outstanding issues. If the issues cannot be resolved, the public may request for the Minister of Environment and Climate Change to order the City to comply with Part II of the Environmental Assessment Act, which addresses individual environmental assessments. Part II Order requests must be made to the Minister of Environment and Climate Change within the review period. Contact information for the Minister will be included in the Notice.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## 0. Executive Summary

The City of Vaughan Transportation Master Plan (TMP), *A New Path* identified Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic improvement and key element in support of the Vaughan Metropolitan Centre and recommends completion of the Municipal Class Environmental Assessment (EA).

This EA Study advances implementation of the Portage Parkway Widening and its Easterly Extension to Creditstone Road in accordance with Schedule C of the Municipal Class EA (October 2000, as amended in 2007 & 2011).

The City of Vaughan retained CIMA+ in May 2015 to complete an EA study for the Portage Parkway Widening from Applewood Crescent to Jane Street (Part A) and Portage Parkway Extension from Jane Street to Creditstone Road (Part B) as two interrelated parts. The project limits of which were subject to refinement through the study process.

**Accordingly this Environmental Study Report documents the planning and design process, in accordance with the Municipal Class EA for:**

**Portage Parkway Extension from West of Black Creek to Creditstone Road.**

Part B is for the extension of Portage Parkway from west of the Black Creek, crossing the Black Creek to Creditstone Road.

**Documentation with respect to Part A is contained in a separate Environmental Study Report for Portage Parkway Widening and Easterly Extension to West of Black Creek<sup>1</sup>**

### 0.1 Background and Study Context

The City's Transportation Master Plan, *A New Path* (2013), Vaughan Metropolitan Centre (VMC) Secondary Plan and supporting focused area transportation plans and studies identified the Portage Parkway Widening and Easterly Extension to Creditstone Road as a strategic network improvement to support development within the VMC.

Portage Parkway is an east-west road from its westerly connection at Chrislea Road and is currently built to a 4-lane urban cross section (2 lanes in each direction) from Applewood Crescent to Edgeley Boulevard. The current right-of-way through this section is approximately 26 metres. East of Edgeley Boulevard to Jane Street, Portage Parkway is currently built as a 2 lane urban cross section to its terminus at the signalized intersection with Jane Street. The right-of-way through this section is approximately 23 metres. Portage Parkway west to Chrislea Road features an overpass structure crossing Highway 400 – a strategic connection in the City's transportation network that opened in late 2010.

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<sup>1</sup> As the study progressed, the study area for Part A was extended easterly to the Black Creek in order to accommodate the required reconstruction of the east leg of the Jane Street intersection.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The VMC Secondary Plan and focused area supporting plans and studies, in the context of broader area, provided a robust planning and design framework for advancing and completing the study EA (planning and design) Study process. These principally included:

- Vaughan Metropolitan Centre Transportation Plan (June 2012) and VMC and Surrounding Areas Transportation Study (March 2013);
- Municipal Servicing Class Environmental Assessment Master Plan (November 2012);
- Black Creek Stormwater Optimization Study Municipal Class Environmental Assessment Master Plan (February 2012) and ongoing Black Creek Renewal EA
- VMC Streetscape and Open Space Plan (November 2015) City wide Streetscape Implementation Manual and Financial Strategy (November 2014) and Design Criteria and Standard Drawings

The VMC Secondary Plan provides the following broader transportation planning and policy context for advancing and completing the planning and design of Portage Parkway:

- + Encourage the use of Portage Parkway for through movements linking to Highway 400.
- + Support the extent of the right-of-way for the Spadina Subway Line to Portage Parkway to protect for future subway extension beyond the VMC.
- + Link Creditstone Road to Portage Parkway and link both roads to Highway 7 and Jane Street to establish a bypass (i.e., Creditstone-Portage-Applewood Bypass) to assist with mitigating through movements on Highway 7 and Jane Street. This bypass would require the southerly extension of
- + Applewood Crescent from Portage Parkway.
- + Cooperate with York Region to implement rapid transit services on Jane Street
- + Acquire right-of-way for an extension of Portage Parkway from Jane Street to Creditstone Road.

### 0.1.1 Study Area

**Figure 1** illustrates the study area for Part A and Part B. The total distance of the study area corridor is approximately 1.5 km. As the project progressed, it became clear that Part A widening had implications immediately east of Jane Street because of an existing driveway on the east side of the Jane Street/Portage Parkway intersection that would have to be relocated. Therefore, both Parts A and B considered implications of the alignment immediately east of Jane Street.

The EA study and recommended Preliminary Preferred Design is supported by a multi-disciplinary and comprehensive technical review and study of transportation, natural, socio- economic and built environment of the area corridor.

### 0.1.2 Class EA Process

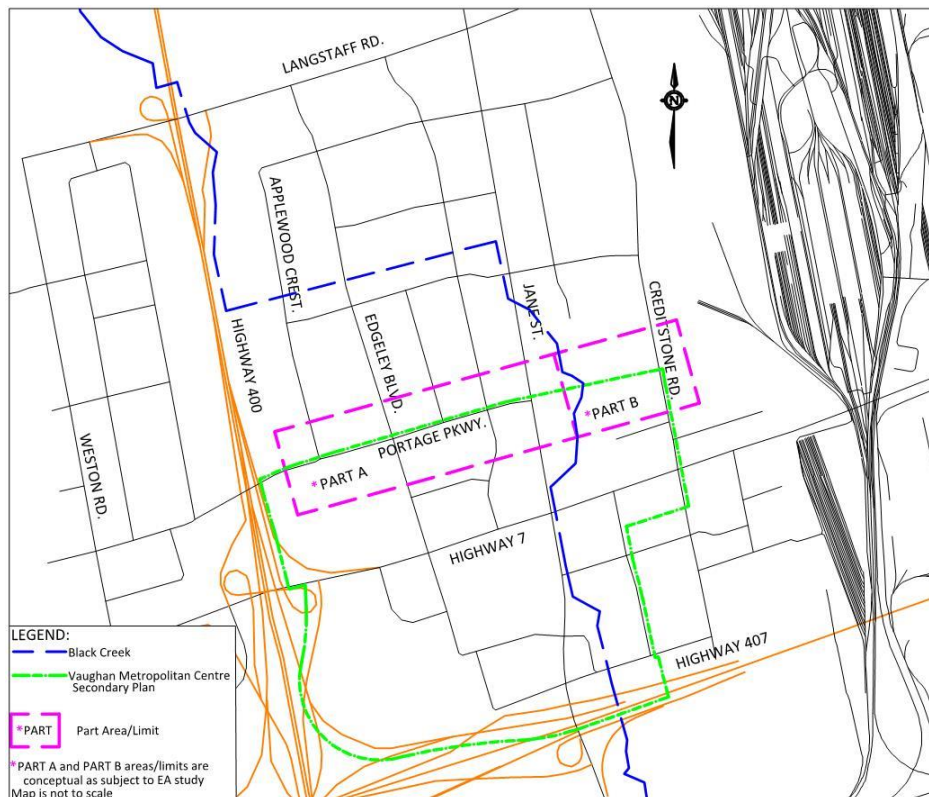
Municipal projects that affect the purpose, capacity or function of a roadway, or propose new roadways, are subject to the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2007 and 2011).



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

The TMP, *New Path* (Appendix I), having addressed Phases 1 and 2, recommended completing the planning and design process in accordance with the Municipal Class EA. This EA Study revisits Phases 1 and 2 and completes Phases 3 and 4 for the Portage Parkway Widening and Easterly Extension to West of Black Creek as a **Schedule C** project under the Municipal Class EA. As noted above, Schedule C projects must follow Phases 1 through 5 of the Class EA process:

- Phase 1** Identify the problem or opportunity.
- Phase 2** Identify alternative solutions to address the problem or opportunity.
- Phase 3** Examine alternative methods of implementing the preferred solution.
- Phase 4** Document the rationale for the Preferred Solution and Design Concept, and the planning, design and consultation process in an Environmental Study Report (ESR) for public and agency review.
- Phase 5** Complete contract drawings and documents, and proceed to construction, operation and environmental monitoring.



**Figure 1: Study Area for Part A and Part B**

Accordingly, separate Environmental Study Reports for Part B (and interrelated Part A) documenting Phases 1 through 4 will be placed on public record for the minimum 30 calendar days upon study completion for public and agency review.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.1.3 Consultation Plan

An extensive Consultation Plan, was implemented to ensure meaningful consultation with internal and external stakeholders and reviewing agencies. The Consultation Plan, organized around study phases, included public information centres, stakeholder engagement and participation of technical review/regulatory agencies at study milestones

The Consultation Plan was led by the Portage Parkway EA project team comprised of CIMA+ and City staff. The City's website, printed media and correspondence provide information on the Study's progress and notice of key Study milestones.

A mailing list was developed to notify potentially interested parties of opportunities for review and comment. Four notices to the public and internal and external stakeholders were undertaken:

Notice of Commencement	June 18 and 25, 2015
Notice of Public Information Centre No. 1	November 10, 2015
Notice of Public Information Centre No. 2	February 23, 2015
Notice of Completion	Pending, 2016

To inform the general public of the study, each of the notices were advertised in two separate issues of the Vaughan Citizen and The Thornhill Liberal, and mailed to approximately 1,500 stakeholders and 93 agency representatives on the study mailing list. The Notices were also posted to the City's webpage at: [http://www.vaughan.ca/projects/projects\\_and\\_studies/environmental\\_assessment\\_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx](http://www.vaughan.ca/projects/projects_and_studies/environmental_assessment_studies/Pages/Class-EA-Study-for-Portage-Parkway.aspx).

The Public Information Centres (PICs) were held at the Homewood Suites Hotel near Portage Parkway on:

- + November 25, 2015 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phases 1 and 2 of the project and asked for public input on the findings – 19 members of the public attended.
- + March 9, 2016 from 5:00 pm to 8:00 pm  
This PIC presented findings from Phase 3 of the project, presenting alternative designs and the preliminary preferred design, and requested public input on the findings - 14 members of the public attended.

Two advisory groups were formed and met three (3) times to provide input to the study following commencement and in advance of each PIC. Invitations to participate on the study's Technical Agencies Committee were mailed to approximately 93 agency representatives. Invitations to participate on the study's Stakeholders Group were mailed to approximately 201 property owners within 200 m north and south of Portage Parkway within the study limits.

A **Stakeholders Group** was established from interested owners of property within 200 m of the Portage Parkway study corridor, including representatives of active and proposed development in the area; and

A **Technical Agencies Committee** was established from interested representatives of regulatory and/or approving agencies on the study mailing list. Separate meetings were held with approving authorities as

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

required to review project impacts, mitigation measures and approval requirements.

One-on-one meetings were held with several land owners whose property was directly impacted by the proposed road improvements so that their concerns were considered during the evaluation of options and mitigation measures developed as appropriate. An Open House further reaching out and engaging affected property owners along the corridor was held on May 5, 2016.

The Ministry of Aboriginal Affairs advised the following First Nations may have existing or asserted rights or claims in Ontario's land claims process or litigation which may be affected by the project:

- + Chippewas of Georgina Island
- + Chippewas of Rama
- + Beausoleil First Nation
- + Mississaugas of the New Credit First Nation

## **0.2 Phase 1 – Problem and Opportunity**

The EA Study, building on the City's TMP and VMC Secondary Plan, corroborates the need to improve, and design Portage Parkway as a multi-modal street.

### **0.2.1 Needs and Justification**

A broader comprehensive traffic analysis of the corridor was undertaken that validates the need to widen Portage Parkway to 4 lanes from Applewood Crescent to Creditstone Road as a major collector road.

- + Under existing traffic conditions, most intersections and turning movements operate satisfactorily during both the a.m. and p.m. peak hours;
- + Under future 2031 traffic conditions with a "Do Nothing" scenario (e.g. no widening on Portage Parkway), traffic operations along the corridor is expected to severely deteriorate, resulting in high levels of congestion; and
- + Under future 2031 traffic conditions with the proposed widening and road extension, all intersections and turning movements are expected to operate satisfactorily during the a.m. peak hour.

### **0.2.2 Problem and Opportunity Statement**

The following Problem and Opportunity Statement was developed and used to guide the remainder of the planning process:

*Transportation improvements are needed to support growth and to provide alternative truck routes to Highway 7 within the VMC.*

*Improving Portage Parkway from Applewood Crescent to Jane Street is an opportunity to:*

- + *Serve the VMC and surrounding employment area,*
- + *Create an alternative route for trucks to bypass the VMC core,*

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

- + *Improve connections to local and regional infrastructure,*
- + *Support transit-oriented nodes and corridors, and*
- + *Enhance transit ridership, cycling and walking.*

*The extension of Portage Parkway from Jane Street to Creditstone Road is an opportunity to:*

- + *Provide a continuous route for all modes of transportation from Weston Road crossing Highway 400 and the Black Creek channel to Creditstone Road, and*
- + *Alleviate traffic congestion on Highway 7 within the VMC.*

### **0.3 Phase 2 – Alternative Planning Solutions**

+

Six (6) alternative planning solutions were identified and evaluated as part of Phase 1 and 2 of the Class EA process, namely:

#### **Do Nothing**

This solution would leave Portage Parkway unmodified in an 'as is' state.

#### **Travel Demand Management Initiatives**

Travel demand management initiatives involve strategies and policies used to reduce travel demand or redistribute the demand spatially or temporally.

#### **Alternative Modes of Transportation**

Promoting and facilitating the use of alternative modes of transportation, such as transit and cycling, can reduce the demand on a roadway.

#### **Localized Intersection and Operational Improvements**

Operational improvements such as the retiming of traffic signals and installation of turning lanes, can improve the overall efficiency of a roadway (i.e. maximize throughput) and the surrounding network.

#### **Widening Portage Parkway from Applewood Crescent to Jane Street**

This solution would increase the capacity of the roadway between Applewood Crescent and Jane Street.

#### **Extending Portage Parkway from Jane Street to Creditstone Road**

This solution addresses the local areas network discontinuity by extending Portage Parkway from Jane Street to Creditstone Road.

An advantage/disadvantage evaluation process was used to evaluate the appropriateness of the above alternatives. The evaluation process yielded a combination of the aforementioned alternatives as the recommended solution for Portage Parkway. Two (2) of the recommended solutions have also been recommended as part of the Transportation Master Plan (TMP), which are:

- + Travel Demand Management Initiatives – Identified in the TMP and will be implemented by the City as a separate strategy.
- + Alternative Modes of Transportation – Identified in the TMP, including the provision for continuous



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

sidewalks, cycling systems, connectivity of the subway extension to Highway 7, and rapid transit of Jane Street.

The other three (3) recommended solutions are:

- + Localized Intersection and Operational Improvements;
- + Widening Portage Parkway from Applewood Crescent to Jane Street; and
- + Extending Portage Parkway from Jane Street to Creditstone Road.

The alternative planning solutions and evaluation were presented to the Advisory Groups (Technical and Stakeholder) and also at the first PIC. Minor comments were received during this consultation process, mostly concerning requests to be kept up to date as the project proceeds.

## **0.4 Phase 3 – Alternative Designs**

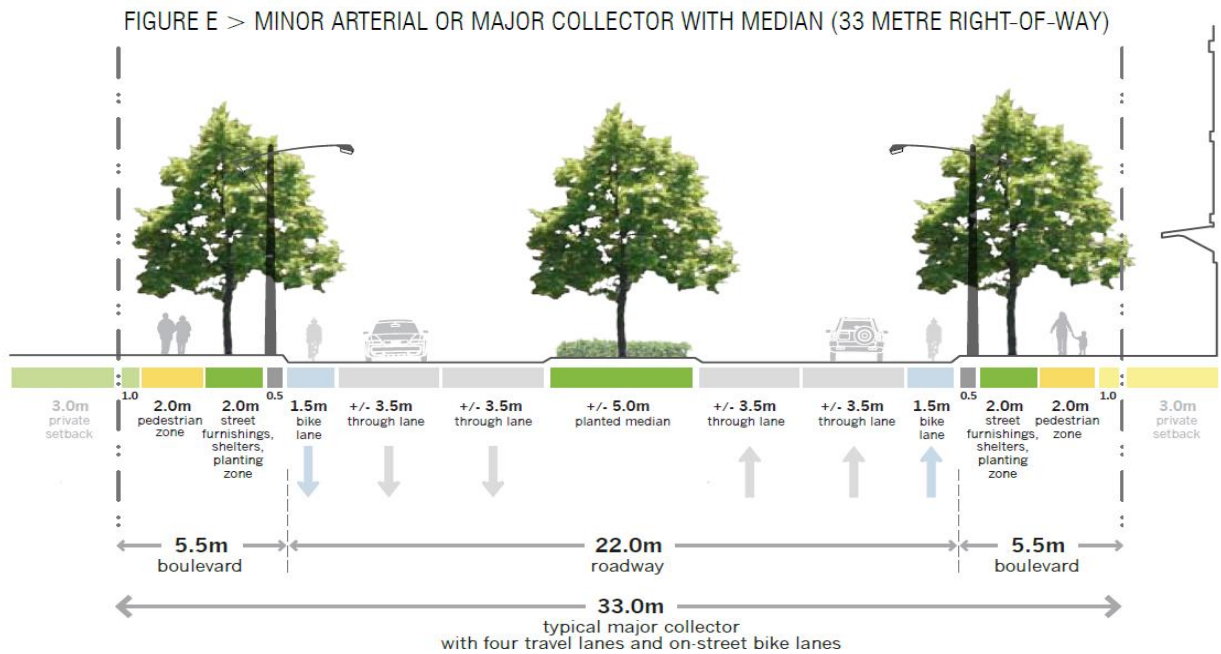
Development of the design concepts and alternative options, evaluated against technical and environmental criteria considered the three-dimensions of a road corridor; i.e. the cross-section, the horizontal alignment and vertical profile as follows.

### Cross-Section

The VMC Secondary Plan (Figure below) and VMC Streetscape and Open Space Plan as coordinated with the City's broader City-wide Streetscape Implementation Manual and Financial Strategy and Design Criteria and Standard Drawings provided the planning and design context and framework for the exploration of alternative typical cross-sections.

The beginning point for development of a typical cross-section was the street cross -sections from the VMC Secondary Plan (figure below) and organization of streetscape infrastructure as guided by the VMC Streetscape and Open Space Plan. The symmetrical street cross section in the VMC Secondary Plan accommodates four 3.5m travel lanes, two 1.5m on-street bicycle lanes and 5.5m boulevards within a 33 metre right of way. The VMC Streetscape and Open Space Plan incorporates cycling facilities into the street network to build a cohesive and permeable network, - identifying a cycle track for Portage Parkway.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016



Recognizing high truck volumes associated, in large part, with the surrounding industrial and employment areas, the preferred typical cross-section, shown in figure 2, was refined to provide a wider (relative to 3.3m inside lane) 3.5m curb lane and wider raised off-road cycle track at 1.8m separated by a rolled curb facilitating trucks and cyclists respectively. The preferred typical cross section for the crossing of Black Creek, provides for continuation of 4 general purpose lanes (2 in each direction) and a raised median within an approximate 25.7 metre right of way maintaining the wider curb lane at 3.5m and off-road cycle track at 1.8 m.

The ROW width tapered from the 25.7m width to 33m width as it approached Jane Street and Creditstone Road.

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### 0.4.1 Technical and Environmental Criteria

Each of the alternative design options was assessed against the following technical and environmental criteria:

#### Technical Criteria

##### *Economic*

- + Capital costs

##### *Implementation*

- + Conformity with regulatory framework.
- + Construction staging and planning.
- + Impacts on existing municipal services and utilities.

##### *Infrastructure Planning*

- + Improved road safety.
- + Opportunities for other travel modes (walking, cycling, and public transit).
- + Improving road capacity and/or traffic flow.
- + Conformity with official/secondary plans and transportation master plans.

#### Environmental Criteria

##### *Natural*

- + Impacts on avian and wildlife.
- + Encroachment onto natural areas
- + Impacts on species at risk.
- + Impacts on aquatic and watercourses.
- + Impacts on vegetation.

##### *Cultural/Social*

- + Impacts on archaeology.
- + Impacts on built heritage and cultural landscapes.
- + Air quality.
- + Compatibility with emergency services requirements.
- + Property and parking access impacts.
- + Noise impacts (post construction).

### 0.4.2 Jane Street to Creditstone Road Alternatives

Alternative design concepts were generated with consideration to challenges and constraints and iteratively short listed to a reasonable range of context sensitive alternative design options and were carried forward for evaluation.

It is noted that for the extension east from Jane Street to west of the Black Creek, key challenges and constraints in generating alternative design concepts for a new road right-of-way were principally centered on:

1. An existing commercial property on the northeast corner of Jane Street and Portage Parkway and the need to relocate its driveway, potential property requirements and impacts on on-site operations;
2. A proposed development on the southeast corner of Jane Street and Portage Parkway;
3. A large property (Iron Mountain) on the northeast corner of the crossing of Black Creek;
4. Existing buildings on the southeast side of the Black Creek crossing, recognizing that they are in the VMC;
5. A CN Rail spur line from the MacMillan Yard crossing Creditstone Road and Transport Canada's

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

Grade Separated restriction with respect at least 30 metres away from this line with any new road;  
and

6. Intersection with Creditstone Road and proximity of adjacent existing driveways

In short-listing options design options, these challenges and constraints were dealt with in the following manner:

- + Item 1 and 2 above were considered in the evaluation of Part A options.
- + Item 3 was addressed by developing options that had varying degrees of impact on the Iron Mountain building.
- + Item 4, all alternatives had an impact on the buildings on the southeast side of the creek. These buildings are shown as being redeveloped in the VMC Secondary Plan. These buildings will remain until the time that the owners decide to redevelop their land.
- + Item 5 was addressed by ensuring all options were shown at least 30 metres away from the CN Rail spur.
- + Item 6 was addressed by ensuring that adjacent driveways could be accommodated in any of the designs.

Four (4) alternatives along with Do-Nothing were carried forward for evaluation and examined for the Portage Parkway road extension. The development of options proceeded in an iterative manner in avoiding and minimizing impacts, recognizing stable built form/uses and associated parking and operation:

**'Do Nothing' Option**

No extension of Portage Parkway from Jane Street to Creditstone Road – this was used for comparative purposes in order to measure the net impacts.

**Option A: Southerly Shift of alignment east of Jane Street**

This context sensitive alignment does not facilitate the proposed development on the southeast corner of the Jane Street/Portage Parkway intersection;

**Option B: Northerly Shift of alignment east of Jane Street**

This context sensitive alignment impacts property on the northeast corner of the Jane Street/Portage Parkway intersection;

**Option C: Alignment east of Jane Street with Centre Boulevard**

This context sensitive alignment impacts property on the northeast corner of the Jane Street/Portage Parkway intersection while facilitating the proposed development on the southeast corner;

**Option D: Alignment east of Jane Street without Centre Boulevard (*Preferred*)**

This context sensitive alignment minimizes impacts on the property on the northeast corner of the Jane Street/Portage Parkway intersection and facilitates proposed development on the southeast corner. This was achieved by exploring opportunities for gradually narrowing the boulevard on the north side immediately west of Black Creek.



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

Under all design options, the unavoidable need to relocate the existing off-set driveway access at the Jane Street intersection to the property on the north east quadrant of Jane Street and the Portage Parkway extension was considered. In the regard it is noted that the EA study provided for exchange of information and ongoing dialogue with Region of York staff as part of the formal Technical Agencies Committee and specifically as it relates to intersection design and access as Jane Street is under the Region's jurisdiction. It is recognized that all access to Jane Street is subject to the findings of York Region's future Rapid Transit Corridor Environmental Assessment.

### **0.4.3 Black Creek Channel Crossing**

The crossing of the Black Creek was the subject of a separate set of alternative design options and evaluation for a preferred structure crossing of the Black Creek. It is noted that the EA study provided for ongoing consultation, exchange of information and focused technical meetings with the Toronto Region Conservation Authority with respect to the crossing of the Black Creek channel as under their regulated area. Five preliminary design concepts were considered for the crossing grouped under culverts (2) and bridges (3). Culvert options not meeting flood criteria were dismissed from further consideration.

The following provides an overview of the preliminary design concepts and key considerations that emerged through the evaluation

Five (5) preliminary design concepts were considered for the crossing of the Black Creek Channel:

**Option 1: 10.6 m wide Box Culvert with two 4.5m wide culverts for multiuse trails on both sides of the creek;**

**Option 2: Single Span 35m Bridge with multiuse trails accommodated underneath the bridge;**

**Option 3: 2 Span (60m) Bridge with multiuse trails accommodated underneath the bridge;**

**Option 4: 3 Span (60m) Bridge with multiuse trails accommodated underneath the bridge;**

**Option 5: 12m wide ConSpan Culvert with two 4.5m wide culverts for multiuse trails on both sides of the creek (*Preferred*)**

As previously noted a key stakeholder for the crossing was the Toronto and Region Conservation Authority (TRCA) with various flood accommodation criteria needing to be met – two meetings were held with TRCA staff to discuss the crossing options. Option 1 for a culvert did not meet the TRCA flood criteria and was not short-listed while the other options met or exceeded the criteria.

For each of the bridge options, there is in the order of approximately a \$6 million premium relative to costs for culvert design options.

Notwithstanding that there are no current plans for active transportation facilities extending north from the VMC Secondary Plan area along the Black Creek channel, all alternative design options at the crossing consider and do not preclude the opportunity for north-south continuous pedestrian and cycling linkages.

### **0.4.4 Meetings with Stakeholders**

Meetings were held with the Stakeholders and Agencies as well as with property owners where there was a significant property impact concern. A second Public Information Centre was also held to review the

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

design options. There was general support for the preferred design options and some minor modifications developed to further minimize impacts wherever possible.

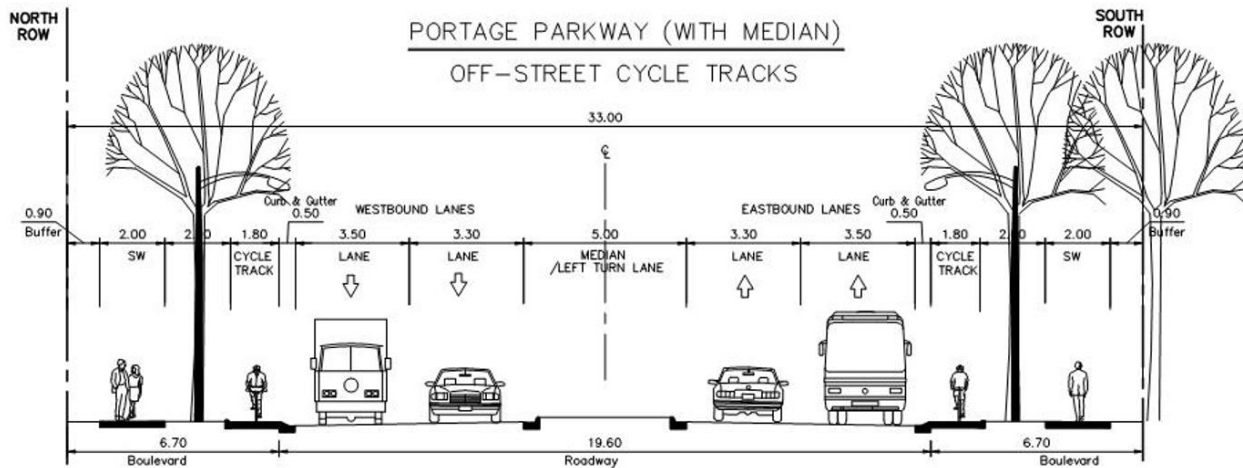
## 0.5 Preliminary Preferred Design

Based on the evaluation of alternative design concepts and consultation with the affected property owners, interested public and agencies, the recommended Preliminary Preferred Design (See Plate 1) is to:

- + Extend Portage Parkway from West of Black Creek to Creditstone Road, as per the Preliminary Preferred Design/ via an alignment as documented in the Portage Parkway Widening and Easterly Extension to West of Black Creek .
- + Accommodate the crossing of Black Creek by implementing a 12m wide ConSpan structure and provide and protect for opportunities for active transportation in the form of two separate 4.5m wide culverts.

The typical cross sections for the preferred design are shown in

**Figure 2.**



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
 Class Environmental Assessment  
 City of Vaughan  
 May 27, 2016

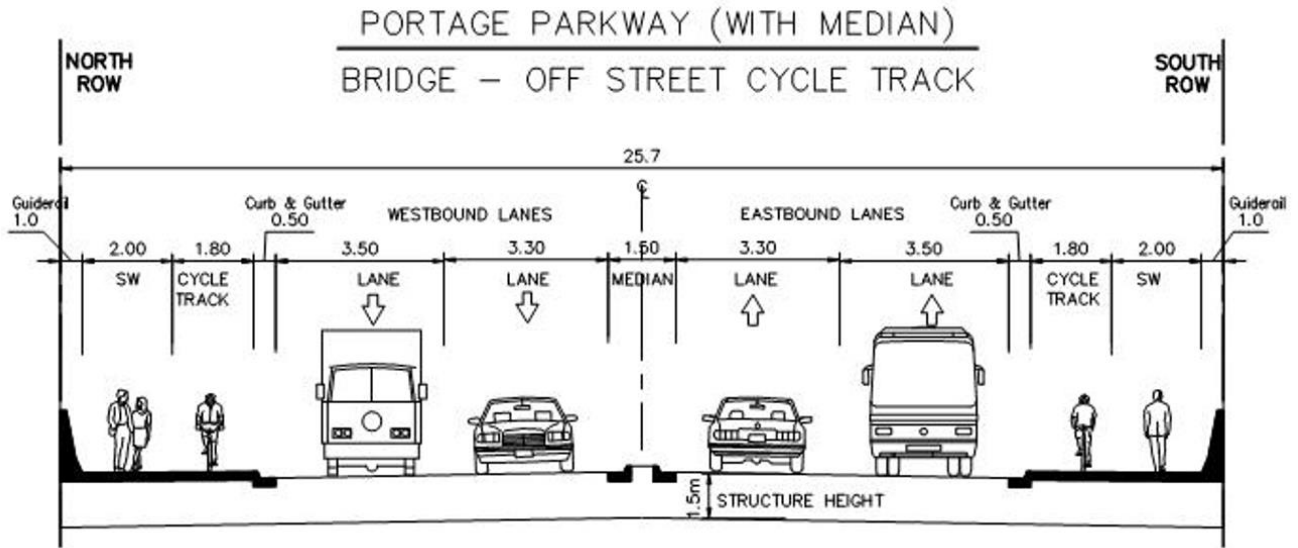


Figure 2: Typical Cross Sections

A 1.8m wide cycle track and 2.0m sidewalk are accommodated on both sides of the road and separated from the travel lanes by a rolled curb.

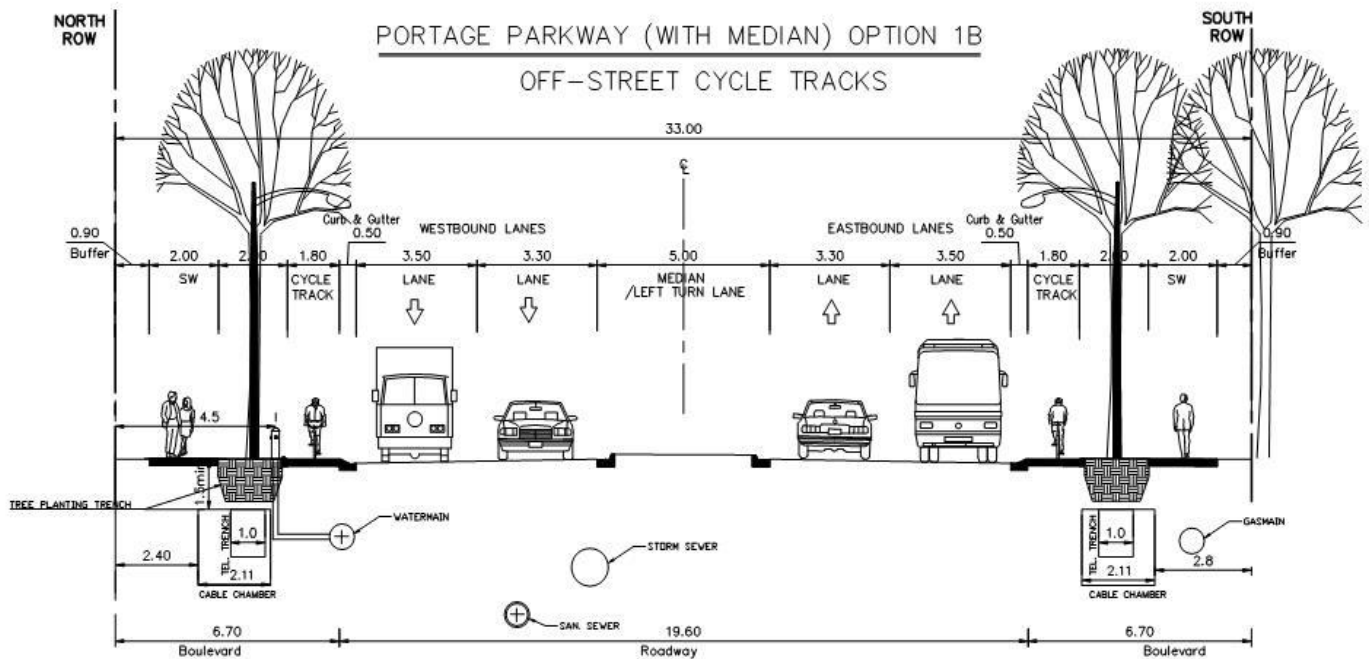


Figure 3: Typical Cross Section (with Utility Locations)

**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

### **0.5.1 Municipal Infrastructure/Utilities**

The recommended Preliminary Preferred Design provides for the integrated and coordinated implementation of planned and approved municipal services (water and sanitary ) as per the VMC Municipal Class Environmental Assessment Master Plan together with streetscape infrastructure as per VMC Streetscape and Open Space Plan and City wide Streetscape Implementation Manual and Financial Strategy

The profile had to match the existing profiles at Jane Street and Creditstone Road while in between those terminal points, the need for fill in the valley was minimized by using a minimum -0.55% slope approach from Jane Street.

Based on the City Standards, the proposed locations of the relocated utilities are shown in **Figure 3** (see above).

### **0.5.2 Traffic Signals**

The EA Study and recommended Preliminary Preferred Design provides for signalized intersections at the future Maplecrete Road extension and at Creditstone Road.

### **0.5.3 Implementation Plan**

The EA Study recommends advancing an Implementation Plan staging improvements in step with the transformation of the VMC and in coordination with the VMC planned street network. The following highlights the key components of the Implementation Plan;

- program and allocate funding for completion of detailed design and engineering for the construction of the crossing of the Black Creek and extension from Creditstone Road.
- obtain approvals (permits – Black Creek Crossing, etc.) and acquire property (where necessary) in a timely manner.

### **0.5.4 Property Requirements**

Implementation of the Portage Parkway, will require the acquisition of lands from several properties including two (2) properties on the south side of Portage Parkway in the VMC Secondary Plan area.

### **0.5.5 Capital Cost Estimate**

The estimated total project cost associated with the proposed improvements, including engineering, construction, utility relocations and other project costs is approximately:

- ✦ \$6,200,000 for Part B, extending Portage Parkway from West of Black Creek to Creditstone Road (including \$3,200,000 for the Black Creek Crossing; a 12 m ConSpan culvert and provide and two separate 4.5m wide culverts on either side of the main culvert protecting and providing for opportunities for active transportation).

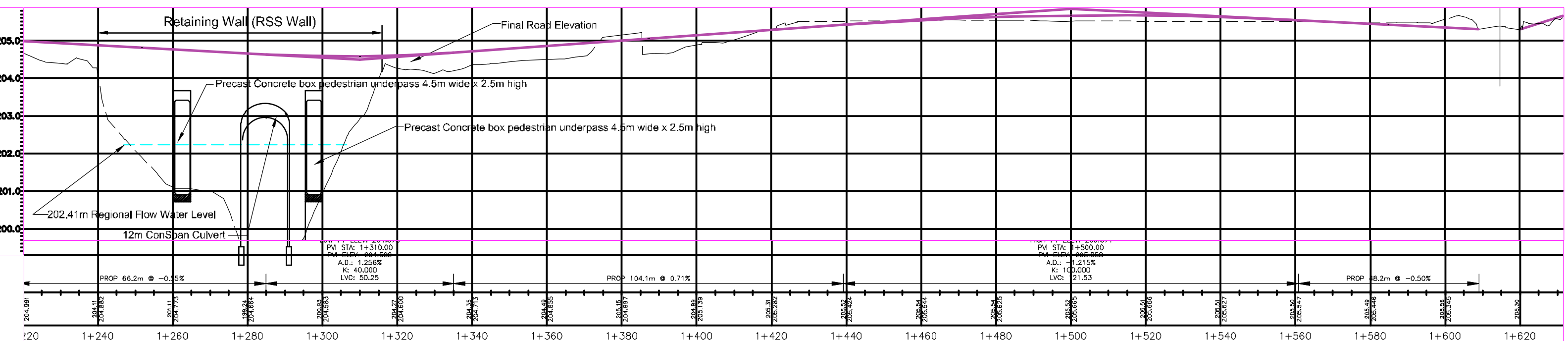
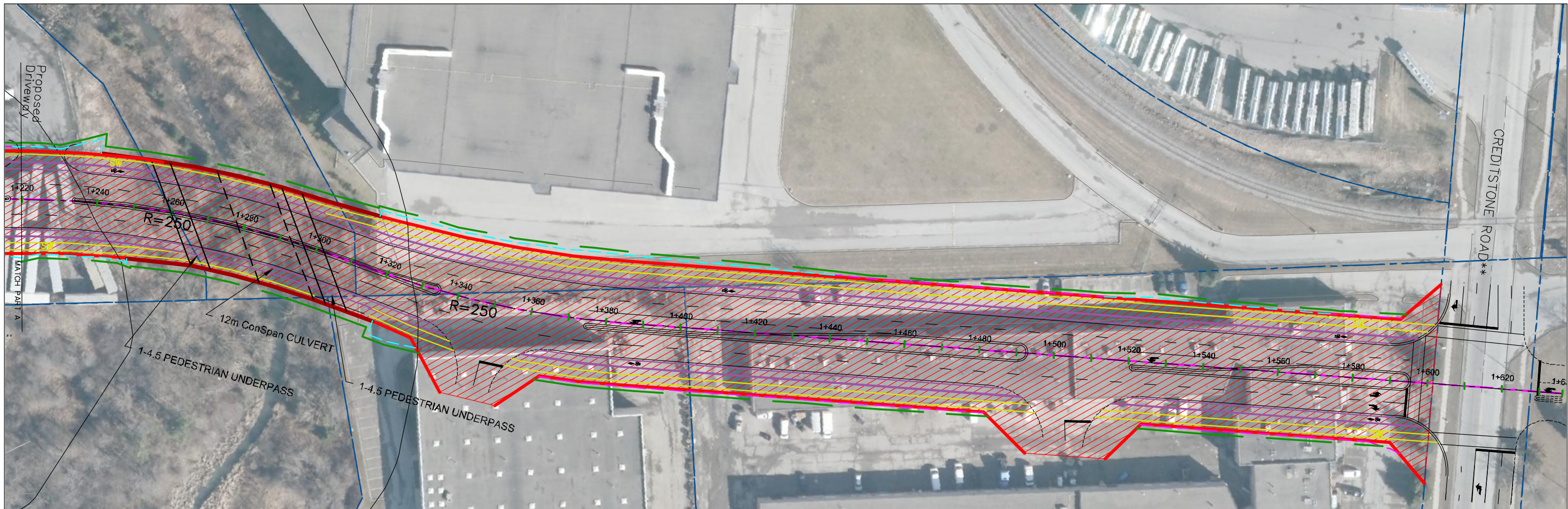
It is noted that estimated preliminary capital cost estimates do include property acquisition and are subject to adjustment and refinement through the completion of detailed design and engineering.



**Executive Summary of Draft Environmental Study Report**  
**Portage Parkway Extension from West of Black Creek to Creditstone Road**  
Class Environmental Assessment  
City of Vaughan  
May 27, 2016

## **0.6 Phase 4 – Notice of Completion**

The Environmental Study Report will be placed on the public record for a minimum 30 calendar days. The Notice of Completion will announce where the report can be reviewed and will include contact information and a date for receiving comments. The Notice will further explain the process for resolving concerns. The public must contact the City of Vaughan within the 30-day review period to discuss and resolve any outstanding issues. If the issues cannot be resolved, the public may request for the Minister of Environment and Climate Change to order the City to comply with Part II of the Environmental Assessment Act, which addresses individual environmental assessments. Part II Order requests must be made to the Minister of Environment and Climate Change within the review period. Contact information for the Minister will be included in the Notice.



Portage Parkway Class EA  
Preliminary Preferred Plan with Profile  
Plate 1 of 1

SCALE: 1:1000

NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

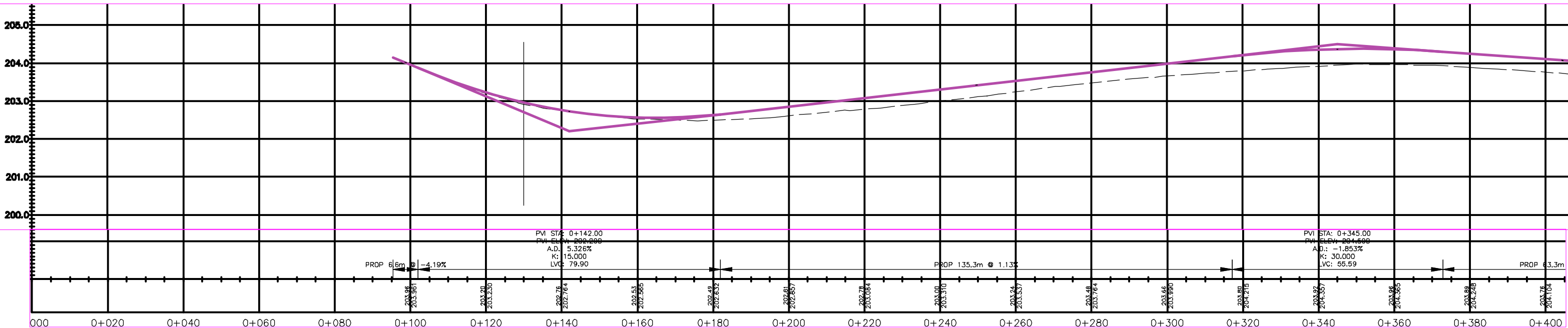
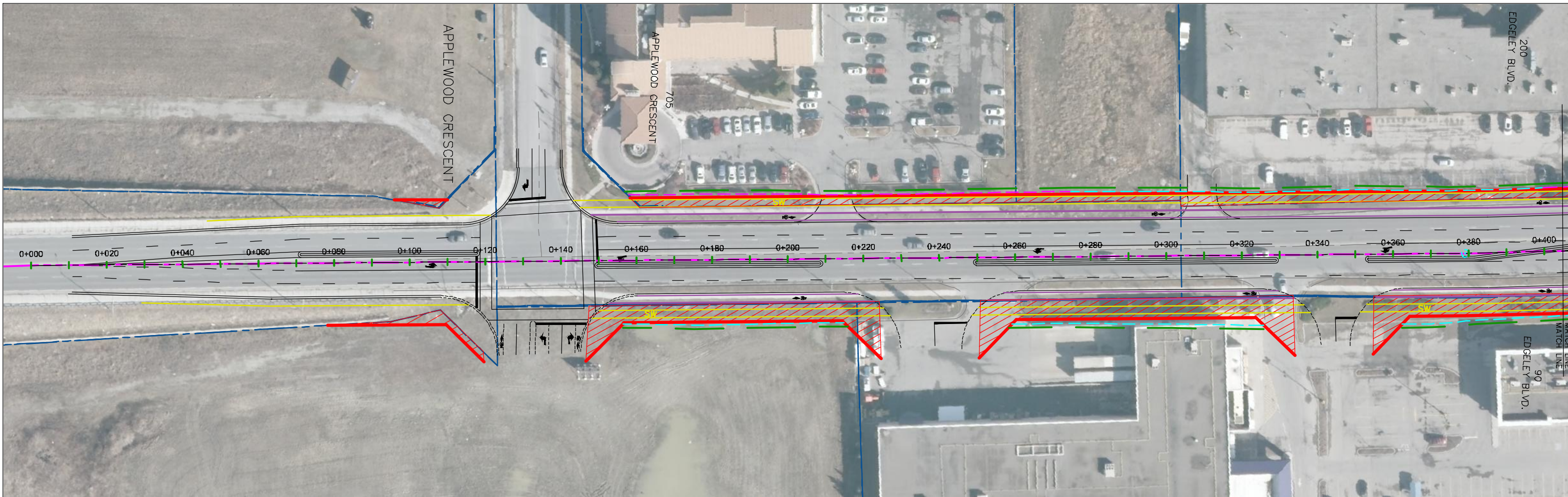
LEGEND:

	PROPOSED SIDEWALK		GRADING LIMIT - FILL
	PROPOSED CYCLE TRACK		RETAINING WALL
	PROPOSED CURB/GUTTER		CONSTRUCTION EASEMENT LIMIT
	PROPOSED PROPERTY LINE		Relocated Signal Head
	EXISTING PROPERTY LINE		Relocated Signal Pole
	PROPERTY REQUIREMENTS		
	GRADING LIMIT - CUT		

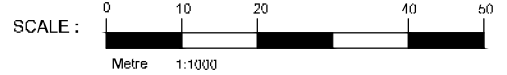
JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study  
CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study







Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 1 of 3

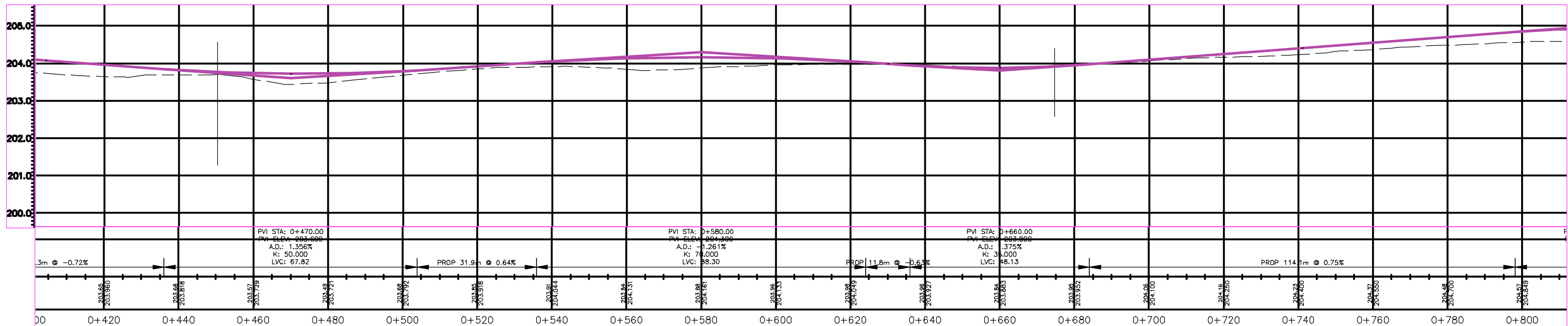
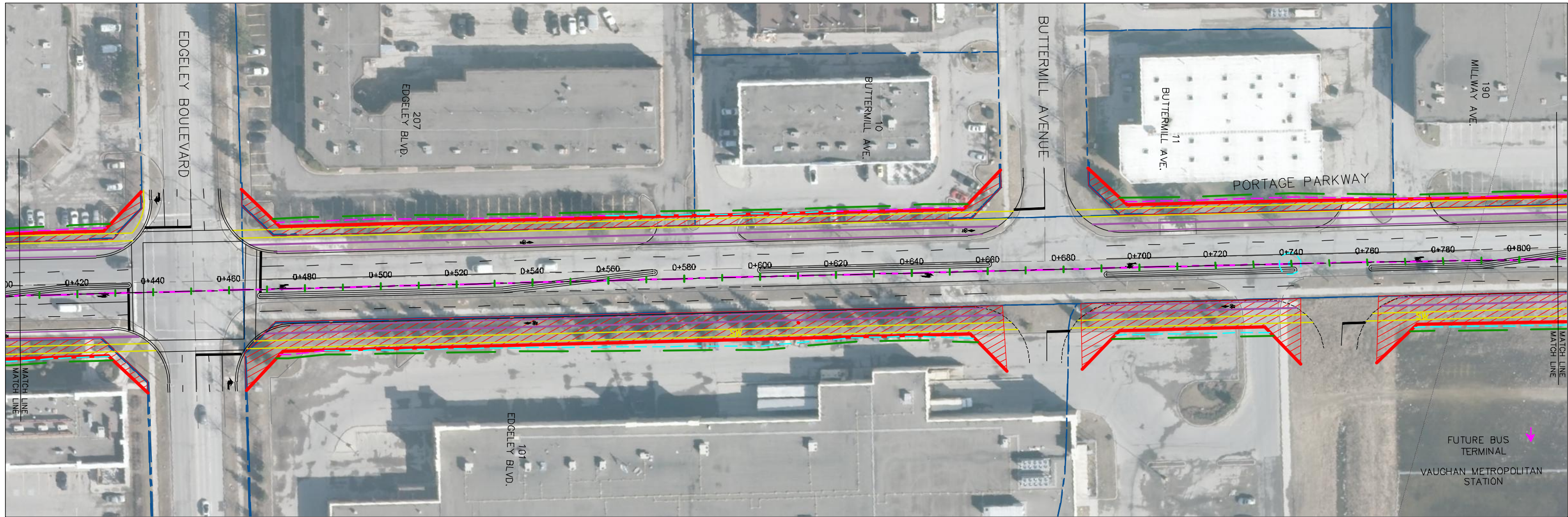


NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

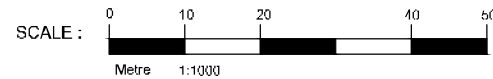
- LEGEND:
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  - PROPOSED CYCLE TRACK
  - PROPOSED CURB/GUTTER
  - PROPOSED PROPERTY LINE
  - - - - EXISTING PROPERTY LINE
  - ▨▨▨▨ PROPERTY REQUIREMENTS
  - - - - GRADING LIMIT - CUT
  - - - - GRADING LIMIT - FILL
  - ▬▬▬▬ RETAINING WALL
  - - - - CONSTRUCTION EASEMENT LIMIT
  - ◀ Relocated Signal Head
  - Relocated Signal Pole
- JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study  
 CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study







Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 2 of 3

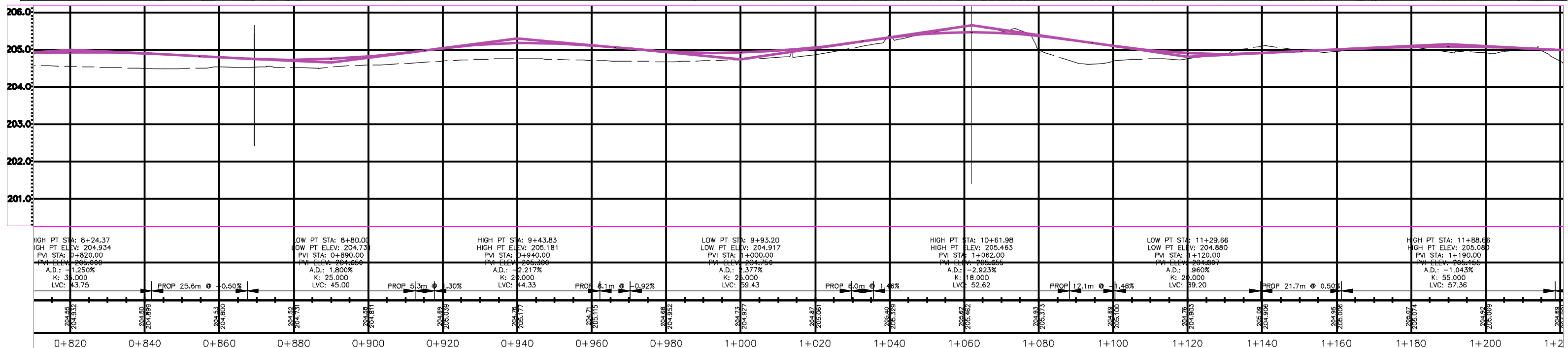
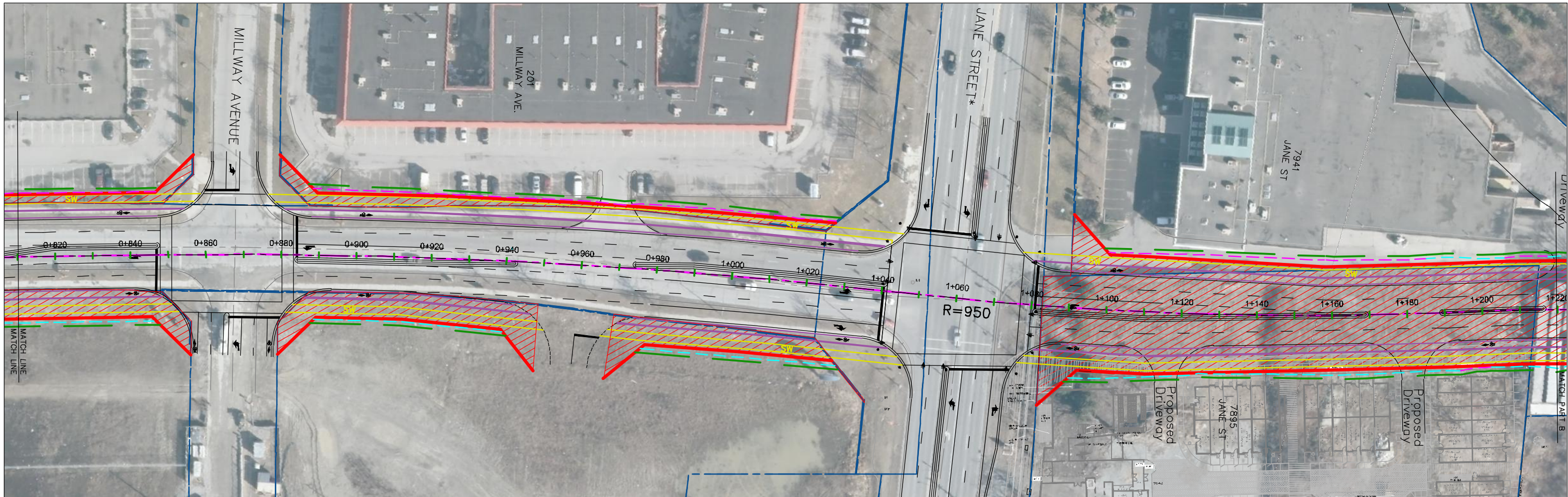


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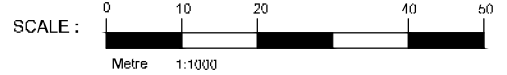
- LEGEND:
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  - PROPOSED CYCLE TRACK
  - PROPOSED CURB/GUTTER
  - PROPOSED PROPERTY LINE
  - EXISTING PROPERTY LINE
  - PROPERTY REQUIREMENTS
  - GRADING LIMIT - CU1
  - GRADING LIMIT - FILL
  - RETAINING WALL
  - CONSTRUCTION CASSEMENT LIMIT
  - Relocated Signal Head
  - Relocated Signal Pole
  - JANE STREET\* - Subject to future York Region Rapid Transit Corridor FA Study
  - CREDITSTONE ROAD\*\* - Subject to future City of Vaughan EA Study







Portage Parkway Class EA  
 Preliminary Preferred Plan with Profile  
 Plate 3 of 3



NOTE: SUBJECT TO DETAILED DESIGN AND ENGINEERING

- LEGEND:
- PROPOSED SIDEWALK
  - PROPOSED CYCLE TRACK
  - PROPOSED CURB/GUTTER
  - PROPOSED PROPERTY LINE
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  - ▨▨▨▨ PROPERTY REQUIREMENTS
  - - - - GRADING LIMIT - CUT
  - - - - GRADING LIMIT - FILL
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